



VILLAGE OF
PORT CHESTER

222 Grace Church Street, Port Chester, New York 10573

PRES -01
 BOT 3-7-2016

AGENDA MEMO

Department: Office of the Village Manager

BOT Meeting Date: 3/7/2016

Item Type: Presentation

Sponsor's Name: Christopher Ameigh, Assistant to the Village Manager

Description	Yes	No	Description	Yes	No
Fiscal Impact	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Public Hearing Required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Funding Source:			BID #		
Account #:			Strategic Plan Priority Area		
	Yes	No	Infrastructure and Sustainability		
Agreement	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Manager Priorities		
Strategic Plan Related	<input checked="" type="checkbox"/>	<input type="checkbox"/>	5 Year Capital Plan		

Agenda Heading Title
(Will appear as indicated below on Agenda)

Presentation from Lumen Light Solutions regarding LED Street Lighting

Summary

On Monday Feb 29, Staff met with representatives from Lumen Light Solutions. Lumen offers complete street light conversion services from auditing of systems to procurement, installation, maintenance and financing. The proposed savings as stated by Lumen could be substantial. As such they have been invited to speak to the BOT. Lumen's complete proposal is contained herein.

Attachments

Lumen Light Solution's Proposal

March 3, 2016

City of Port Chester– Proposal to Piggyback

Energy Savings Estimate, Project Cost & Lease/Financing

Lumen Light Solutions is pleased to offer an extension of the terms included in the City of New Rochelle / Village of Elmsford Bid dated August 13, 2014 under the terms of the General Municipal Law Section 103. By virtue of including the Village of Elmsford as well as the appropriate “piggyback” language in the bid document, Lumen is pleased to be able to extend the same unit pricing and terms.

ENERGY SAVINGS CALCULATION

Based on the ConEd Westchester Streetlight Database provided, Lumen Light Solutions has performed an analysis for converting the entire streetlight system to Philips LED Roadway Luminaires. These fixtures were selected by the City of New Rochelle after arduous research and testing.

Municipalities are billed by NYPA based on a production and delivery charge for Westchester Street Lighting under Service Classification 66. A rate of 23-cents per watt was specified to be used, and is reflective of the current 2014 and 2015 base rates for combined ConEd Delivery and NYPA Production.

CURRENT SYSTEM						
	Count	ABH	KWH	Energy Rate	Annual Cost	
Various HID Fixtures per WSLDB	1856	4270	1,333,831.00	\$ 0.23	\$ 306,781	
NEW						
Fixture	LED Watts	Quantity	ABH	KWH	Energy Rate	Annual Cost
RFS 35W	38	1060	4270	171,995.60		
RFS 54W	54	209	4270	48,191.22		
RFM 72W	73	41	4270	12,780.11		
RFM 108W	106	465	4270	210,468.30		
RFL 180W	174	81	4270	60,181.38		
				503,616.61	\$ 0.23	\$ 115,832
SAVINGS (annually)			830,214.39 kwh	=	\$	190,949
			62%			62%

As the energy rate increases going forward so will the energy savings; however this proposal does not attempt to forecast the future price of energy nor the rate charged to municipalities by ConEd for delivery or NYPA for provision of energy. Therefore, the proposal is based on today’s known rate.

Likewise, savings does not include any maintenance savings realized nor refunds due to database corrections.

COST OF PROJECT CALCULATION

The project cost for auditing the system and reviewing the developed replacement program with the City, furnishing and installing the fixtures is based on the piggy-backing provision of the New Rochelle bid, and is shown in the table below:

	Philips LED Equivalent	Quantity	Unit Price *	Total
HPS 70W equivalent	RFS 35W 1LED 4K	1060	336.15	\$ 356,319.00
HPS 100W equivalent	RFS 54W 16LED 4K	209	341.25	\$ 71,321.25
HPS 150W equivalent	RFM 72W 32LED 4K	41	377.35	\$ 15,471.35
HPS 250W equivalent	RFM 108W 48LED 4K	465	420.15	\$ 195,369.75
HPS 400W equivalent	RFL 180W 80LED 4K	81	564.5	\$ 45,724.50
Photocells (extra life)		1856	24.11	\$ 44,748.16
<i>Decorative fixtures can be accommodated also</i>				
MPT		1856	16.79	\$ 31,162.24
TOTAL PROJECT COST				\$ 760,116.25

The Unit Price is being extended from the New Rochelle contract pricing.

Scope of Services – included in Project Cost

“Furnishing, Installation and Financing of Energy Efficient Street Lighting” – a full-service, turn-key approach:

- 1. Perform Audit & Develop Audit Report with Replacement Plan**
 – key to effective conversion and matching existing lighting levels is knowing what is really in the field. The Audit Report will: detail field conditions compared to the inventory; assist development of a detailed replacement plan; facilitate the system mapping to expedite installation; and confirm the energy savings.

 Actual lighting levels will be mapped providing the opportunity to see what actually exists in the field and make adjustments based on the desired lighting plan.
- 2. Furnish of LED Cobraheads** (and decorative fixtures, if desired)
 – order the fixtures in a manner consistent with the inventory required to accomplish the installation plan, receive delivery and store fixtures until installed.
- 3. Install fixtures** – in a one-for-one replacement in accordance with the Conversion/Replacement Plan.
- 4. Effectuate Savings / Update ConEd Database** – To develop the Audit Report and Replacement Plan, Lumen painstakingly matches the records in the ConEd database to the lights in the field, converts each fixture and then modifies the appropriate record to realize energy savings.
- 5. Provide a Streetlight Database** that provides a mapping and tracking tool for the LED inventory in both a GIS format compatible with ArcView, if desired, and in a GoogleEarth mapping for ease of use by DPW crews.
- 6. Payments from Energy Savings** using the NYS Energy Law that allows energy efficiency measures to be leased, 5-year and 7-year options allow the Project Cost to be paid from the energy savings realized.

NO PAYMENTS UNTIL CONVERSION COMPLETE

BENEFITS OF THE LEASE/FINANCE OPTION

- ✓ **Not a penny is paid until all streetlights are installed.**
- ✓ **Uses Energy Savings to Pay for the Conversion** – energy savings are accumulated –and will be realized– as each light is converted under Lumen’s approach.
- ✓ **Allows bonding or borrowing capacity to be used for other capital improvements.** Using the NYS Energy Law financing is handled via a “lease” to the City for the financing term and thus does not affect bonding or borrowing capacity.
- ✓ **Better value and a more streamlined process** than other available financing options.

Worth noting is that Lumen’s financing option compares favorably with NYPA financing when including all of NYPAs costs and performing a cost vs. benefit analysis, and is comparable with most municipal bonding rates when considering all costs associated with bonding.

- ✓ **Fixture Cost for Inventory Replenishment Fixed at current Volume Cost.**
- ✓ **At the end of the Lease period, the fixtures are yours.**

SIMPLE LUMP SUM ANNUAL PAYMENTS

Lumen’s proposal is simple: Payments will be made annually, **in fixed lump-sum** amounts. The first invoice will be submitted upon Substantial Completion of installation and payment due within 30-days. Thereafter, payments are made on an annual basis on the anniversary of the first payment. This method was established to enable the streetlight conversions to be paid for from the energy savings generated and therefore not impact the capital program or operating budgets.

ENERGY SAVINGS (<i>see pg 1</i>)	\$ 190,949	\$ 190,949
ANNUAL PAYMENT	\$ 176,500	\$ 136,000
Net - Remainder to Use	\$ 14,449	\$ 54,949

Energy Savings Pays for the Project Cost

Without the financing option, Lumen payments will need to be made to purchase a supply of fixtures to have on hand for installation, and payment for the Audit upon submission of the Audit. Invoices will then be submitted and require payment each week of installation. The City will realize savings based on the date each light is converted; however funds will have to be set aside for progress payments.

Note –during the Audit/Field Verification phase, the actual quantity of light in the field will be determined. The Project Cost and payments will be adjusted to account for differences in quantities.

The financed and non-financed payments are compared in the Table 1 below. Again, under the financing option, the City pays nothing until the streetlights are installed.

Table 1			
PAYMENT COMPARISON – LEASE OPTION VS NON-LEASE OPTION			
Timeline	Anticipated Timeframe	Payments With Financing	Payments Without financing
Award			
NTP (Notice to Proceed)	Within 1 mo after award	NONE	Initial Fixture Order Material Payment
Audit / Verification of Inventory (Matching Field to CE DB)	3 months after NTP	NONE	Payment for Audit upon Submission of Audit
Installation Begins	Upon Audit Acceptance	NONE	<i>Upon receipt of fixtures</i> Weekly Invoices with payments due in 10 days – reflecting installations each week.
Substantial Completion	4 months after NTP	Invoice will be submitted upon Substantial Completion.	Final payment.
PAYMENT	BEGIN PAYMENTS 60-days after Substantial Completion	Lump Sum Fixed Annual Payment based on financing term	Fully paid.

Lease/Finance Option Payment Schedule – Payments under the lease/financing option are fixed annual lump sum payments This allows the payments to be made from the energy savings generated by the conversion of the streetlight system avoiding impact to the capital and operating budgets.

Table 2		
FINANCING OPTION PAYMENTS BASED ON FINANCING TERM		
	5-year Financing	7-year Financing
At Substantial Completion	\$176,500	\$136,000
Year 2– SC+1	\$176,500	\$136,000
Year 3 – SC+2	\$176,500	\$136,000
Year 4 – SC+3	\$176,500	\$136,000
Year 5 – SC+4	\$176,500	\$136,000
Year 6 – SC+5		\$136,000
Year 7 – SC+6		\$136,000
Total Cost (Payments x Term)	\$882,500	\$952,000

VALUE-ADDED TURN-KEY SOLUTION
... In Summary:

- Lumen Manages the Entire Process** and assist with PR needs (handouts/maps/etc)
- Assures ALL lights are Converted**
- Effectuates Changes in ConEd Database to Realize Energy Savings**
- Finance with Payments out of Energy Savings** with monitoring of billings during lease period, and coordination of any warranty issues & inventory

Interest rate is based on the bank cost of funds borrowing rates as of 8/11/14 as provided for in the New Rochelle bid documents.

The Lease Period also includes cost of continued consulting services and energy monitoring throughout term of lease, and an **ENERGY SAVINGS GUARANTEE** is available.

Value-Added Benefits of Exercising the Lease/Finance Option

– During the finance period, Lumen provides continued services:

1. **Coordination of any warranty issues or fixture questions** that may arise during the finance period.
2. **Fixture Pricing Held through Lease Period - Provision of an inventory and inventory replenishment of that inventory at known fixture unit prices throughout the financing term.**
Inventory can be included in the initial quantities to have cobra heads on hand in the event of damaged and require replacement. With financing, additional or replenishment inventory can be purchased throughout the financing term at designated unit prices that extend the volume-discount rate for the fixtures from the time of bid throughout the lease period. Lumen will order the inventory, and deliver with an invoice upon delivery.
3. **Monitoring & Coordination of ConEd streetlight database and review of NYPA billings during the financing period.** Lumen can assure that the savings is realized by continuing to check the ConEd database and NYPA billings throughout the financing/lease term and handling any adjustments.
4. **Guaranteed Savings** – As part of the financing solution, Lumen is willing to guarantee the energy savings will cover the payments for the project cost and that ZERO CAPITAL DOLLARS are needed.

STREETLIGHT CONVERSION – VALUE OF LUMEN’S APPROACH

<p>Furnish, Install, Finance and Manage the entire conversion process to minimize municipal staff time required for oversight and management and effectuate energy savings.</p>
<p>> Experienced personnel with a proven track-record of delivering start-to-finish will oversee and manage the entire process – planning, installation, inventory, execution and effectuate the savings and verify savings are realized (and manage issues along the way).</p> <p>> Trained personnel utilize an established and proven GIS-based system architecture to facilitate:</p> <ol style="list-style-type: none"> 1) mapping each fixture and its associated key data 2) developing and coordinating an efficient installation plan and fixture delivery schedule that integrates crew work schedules and manufacturer’s production capacity and plant schedule 3) managing, controlling and coordinating fixture inventory and installation work schedules 4) tracking installation and recording conversion dates via a web-based application 5) collecting any site constraints (ie no power to pole, loose arms, overgrown trees etc.) for timely resolution 6) producing weekly reports of converted fixtures and issues for handling along with tracking resolution 7) collecting and confirming GIS location of each fixture at the time of conversion for an accurate GIS and/or GoogleEarth-based mapping of the street system for ease of maintenance 8) facilitating entry of converted lights into the ConEd database by collecting the actual converted date as a database function (and the initial set-up and homework done in #1 above) 9) assuring all lights are converted and accurately recorded – and Lumen will monitor billings to assure accuracy and savings is realized 10) financing so that no payment is due until all lights are converted, with a payment schedule based upon the energy savings realized covering the annual payment to assure no capital funds are required for the project.
<p>NO PAYMENT UNTIL ALL LIGHTS ARE CONVERTED (with lease/financing option)</p> <p>Payment schedule set such that the annual energy cost savings cover the project payments</p>
<p>Lumen handles all aspects of converting the streetlight system AND effectuating the changes in the database so that savings are realized as soon as possible. Status Reports will keep municipal staff informed of progress; however, project risk for the elements under Lumen’s control are passed to Lumen vis a vis fixture delivery/installation, inventory management, schedule and cost management).</p>

From: [Heather Cuffel](#)
To: [Ameigh, Chris](#)
Cc: [Steers, Christopher](#); [Cerreto, Tony](#); [Douglas, Leonie](#); [Morabito, Rocky \(DPW\)](#)
Subject: RE: Inventory of Port Chester Street Lights
Date: Thursday, March 03, 2016 10:41:15 AM
Attachments: [City of Port Chester Piggyback Proposal.pdf](#)

Chris –

Attached is the Proposal to Piggyback. Please review and let me know if you have any questions.

Other information you requested:

Original Bid – advertised and let by the City of New Rochelle with the Village of Elmsford included

City of New Rochelle installs remain in progress
Village of Elmsford installs are completed.

Piggybacks to Date:

Village of Irvington (cobrahead installs completed)
Village of Ardsley (cobrahead installs completed)
Village of Hastings on Hudson (cobrahead installs completed)
Village of Tarrytown (cobrahead installs completed)
Village of Ossining (cobrahead installs completed)
Town of Ossining (cobrahead installs completed)
Village of Dobbs Ferry (cobrahead installs almost complete)
Village of Buchanan (cobrahead installs completed)

Village of Mamaroneck (audit just completed)
Town of Greenburgh (audit completing)
City of Peekskill (in audit phase)

There are a number of others that we are talking to, several of whom are interested in expediting the project and reducing their costs. Please speak with Marcus Serrano at the City of Rye, who was previously with Dobbs Ferry, and is now working to engage Lumen to complete conversions for the City of Rye.

The advantages of piggybacking are:

- Expedited project delivery – with turn-key execution by Lumen, conversion will be completed in under 6 months, with energy savings starting while conversion is in progress.
- Reduced Cost – Lumen’s approach minimizes cost through the turn-key solution, and fixture costs are reduced by the volume pricing from the original bid
- Management Minimized – Lumen provides you with updates and status reports minimizing oversight required from your staff.



VILLAGE OF
PORT CHESTER

222 Grace Church Street, Port Chester, New York 10573

AGENDA MEMO

Department: Office of the Village Manager

BOT Meeting Date: 3/7/2016

Item Type: Presentation

Sponsor's Name:	Christopher Steers – Village Manager
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Description	Yes	No	Description	Yes	No
Fiscal Impact	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Public Hearing Required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Funding Source:			BID #		
Account #:			Strategic Plan Priority Area		
			N/A		
	Yes	No			
Agreement	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Manager Priorities		
Strategic Plan Related	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Performance Management		

Agenda Heading Title <i>(Will appear as indicated below on Agenda)</i>
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GFOA Distinguished Budget Presentation Award

Summary

On February 16th, the Village was awarded its third subsequent Government Finance Officer's Association Distinguished Budget Presentation Award. This award is the highest form of recognition in governmental budgeting.

It reflects the commitment of the governing body and staff to meeting the highest principles of governmental budgeting. In order to receive the budget award, the entity had to satisfy nationally recognized guidelines for effective budget presentation. These guidelines are designed to assess how well an entity's budget serves as:

- a policy document
- a financial plan
- an operations guide
- a communications device

Budget documents must be rated "proficient" in all four categories, and the fourteen mandatory criteria within those categories, to receive the award.

Attachments
GFOA award announcement and award



Government Finance Officers Association
203 North LaSalle Street, Suite 2700
Chicago, Illinois 60601-1210
312.977.9700 fax: 312.977.4806

January 21, 2016

Christopher D. Steers
Village Manager
Village of Port Chester
222 Grace Church Street
Port Chester, NY 10573

VILLAGE OF PORT CHESTER

FEB 16 2016

RECEIVED *VM*

Dear Mr. Steers:

A Panel of independent reviewers has completed its examination of your budget document. We are pleased to inform you that the panel has voted to award your budget document the Distinguished Budget Presentation Award for the current fiscal period. This award is the highest form of recognition in governmental budgeting. Its attainment represents a significant achievement by your organization.

The Distinguished Budget Presentation Award is valid for one year. To continue your participation in the program, it will be necessary to submit your next annual budget document to GFOA within 90 days of the proposed budget's submission to the legislature or within 90 days of the budget's final adoption. Enclosed is an application form to facilitate a timely submission. This form should be submitted with four copies of your budget accompanied by the appropriate fee.

Each program participant is provided with confidential comments and suggestions for possible improvements to the budget document. Your comments are enclosed. We urge you to carefully consider the suggestions offered by our reviewers as you prepare your next budget.

When a Distinguished Budget Presentation Award is granted to an entity, a Certificate of Recognition for Budget presentation is also presented to the individual or department designated as being primarily responsible for its having achieved the award. Enclosed is a Certificate of Recognition for Budget Preparation for:

Christopher D. Steers, Village Manager

Continuing participants will find a certificate and brass medallion enclosed with these results. First-time recipients will receive an award plaque that will be mailed separately and should arrive within eight to ten weeks. Enclosed is a camera-ready reproduction of the award for inclusion in your next budget. If you reproduce the camera-ready in your next budget, it should be accompanied by a statement indicating continued compliance with program criteria.

The following standardized text should be used:

Christopher D. Steers

January 21, 2016

Page 2

The Government Finance Officers Association of the United States and Canada (GFOA) presented a Distinguished Budget Presentation Award to **Village of Port Chester, New York** for its annual budget for the fiscal year beginning **June 1, 2015**. In order to receive this award, a governmental unit must publish a budget document that meets program criteria as a policy document, as an operations guide, as a financial plan, and as a communications device.

This award is valid for a period of one year only. We believe our current budget continues to conform to program requirements, and we are submitting it to GFOA to determine its eligibility for another award.

A press release is enclosed.

The Government Finance Officers Association encourages you to make arrangements for a formal presentation of the award. If you would like the award presented by a member of your state or provincial finance officers association, we can provide the name of a contact person for that group.

We appreciate your participation in this program and we sincerely hope that your example will encourage others in their efforts to achieve and maintain excellence in governmental budgeting. The most current list of award recipients (with hyperlinks) can be found on GFOA's website at www.gfoa.org. If we can be of further assistance, please contact the Technical Services Center.

Sincerely,

A handwritten signature in cursive script that reads "Stephen J. Gauthier".

Stephen J. Gauthier, Director
Technical Services Center

Enclosure



Government Finance Officers Association
203 North LaSalle Street, Suite 2700
Chicago, Illinois 60601-1210
312.977.9700 fax: 312.977.4806

January 21, 2016

PRESS RELEASE

For Further Information Contact
Stephen J. Gauthier (312) 977-9700

Chicago--The Government Finance Officers Association of the United States and Canada (GFOA) is pleased to announce that **Village of Port Chester, New York** has received the GFOA's Distinguished Budget Presentation Award for its budget.

The award represents a significant achievement by the entity. It reflects the commitment of the governing body and staff to meeting the highest principles of governmental budgeting. In order to receive the budget award, the entity had to satisfy nationally recognized guidelines for effective budget presentation. These guidelines are designed to assess how well an entity's budget serves as:

- a policy document
- a financial plan
- an operations guide
- a communications device

Budget documents must be rated "proficient" in all four categories, and the fourteen mandatory criteria within those categories, to receive the award.

When a Distinguished Budget Presentation Award is granted to an entity, a Certificate of Recognition for Budget Presentation is also presented to the individual or department designated as being primarily responsible for its having achieved the award. This has been presented to **Christopher D. Steers, Village Manager**

For budgets beginning in 2014, 1,491 participants received the Award. Award recipients have pioneered efforts to improve the quality of budgeting and provide an excellent example for other governments throughout North America.

The Government Finance Officers Association is a major professional association servicing the needs of 18,300 appointed and elected local, state, and provincial-level government officials and other finance practitioners. It provides top quality publications, training programs, services, and products designed to enhance the skills and performance of those responsible for government finance policy and management. The association is headquartered in Chicago, Illinois, with offices in Washington D.C. The GFOA's Distinguished Budget Presentation Awards Program is the only national awards program in governmental budgeting.



**The Government Finance Officers Association
of the United States and Canada**

presents this

CERTIFICATE OF RECOGNITION FOR BUDGET PREPARATION

to

**Christopher D. Steers, Village Manager
Village of Port Chester, New York**



The Certificate of Recognition for Budget Preparation is presented by the Government Finance Officers Association to those individuals who have been instrumental in their government unit achieving a Distinguished Budget Presentation Award. The Distinguished Budget Presentation Award, which is the highest award in governmental budgeting, is presented to those government units whose budgets are judged to adhere to program standards.

Executive Director

Date

January 21, 2016



GOVERNMENT FINANCE OFFICERS ASSOCIATION

*Distinguished
Budget Presentation
Award*

PRESENTED TO

**Village of Port Chester
New York**

For the Fiscal Year Beginning

June 1, 2015

Executive Director



VILLAGE OF PORT CHESTER

OFFICE OF THE VILLAGE MANAGER

MEMORANDUM

TO: Mayor and Board of Trustees

FROM: Christopher W. Ameigh

THROUGH: Christopher D. Steers

DATE: March, 4 2016

RE: Edgewood Park Parking Lot Project Update

Staff has met with the Contractor awarded the bid for the 201 Grace Church Street house demolition. The asbestos abatement came in at \$10,200 bringing the total for the project to \$32,150. Or initial estimates were 30-35,000.

The contract for Abatement and Demolition has been executed. The contractor is currently waiting for state permits for asbestos abatement.

A consultation with the Village Building Inspector has confirmed all requirements for the demolition permit pending shut off notices from Con Ed and United Water. Both notices are pending.

Once abatement is complete PCFD and PCPD have requested to use the house for limited training purposes prior to demolition

Dolph has been asked to develop a plan for a 30 space parking lot with a bathroom on site as requested by the Parks Commission. He will provide cost estimates and design for consideration to the Parks Commission before bringing a project to the BOT to consider. There are some current hurdles relating to the grading that are being worked out in Dolph's Office.

Port Chester Station Outbound Elevator MNR Project M602-02-04
MNR Meeting With the Village Of Port Chester (February 24, 2016)
Agenda

1. Project Introduction:
 - a. Location: Westchester Avenue, between East Broadway and Main Street
 - b. Schedule:
Bid Advertisement: March 2016 Contract Award: July 2016 Contract Completion: July 2017
2. Maintenance & Protection of Traffic (MPT) Sequence – Drawing MPT-001:
 - a. East Broadway (Duration: 6 weeks):
 - Install concrete barrier for temporary ramp installation to provide outbound platform access during elevator construction and raised asphalt crosswalk to provide safe access across East Broadway.
 - PC Village: Possibly provide handicap parking in parking lot across the street to facilitate disabled individuals.
 - b. Westchester Avenue, between East Broadway and Main Street (Duration: 3 weeks):
 - Install temporary crosswalk in vicinity at corner of Westchester Avenue and East Broadway to facilitate pedestrians crossing Westchester Avenue due to sidewalk closure for elevator construction as well as to access the outbound platform temporary ramp on East Broadway.
 - Relocate bus stop north at the corner of Westchester Avenue and Broad Street. (Coordinate with Westchester Bee Line Bus)
 - Install concrete barrier 5 feet from the sidewalk curb onto the northbound lane and on sidewalk to establish construction work zone limits. Install plywood barrier on south side of work zone next to “One Station Plaza Restaurant”. Install movable barrier with chainlink fence on north side of work zone for construction equipment access. (Coordinate roadway barrier installation with Westchester County DPW & Transportation)
3. Major Construction Activities (Total Duration: 37 weeks):
 - a. Retaining wall, staircase and elevator foundations construction (Duration: 16 weeks):
 - Construct working platform for minipile drill rig (to be lifted in place via crane), install mini piles, tieback anchors and place concrete for retaining wall. (Drawings MPT-0001 & GT-101)
 - Construct elevator and staircase foundations.
 - b. New Staircase (Duration: 4 weeks):
 - Install new staircase and construct elevator machine room on underside.
 - c. New Elevator (Duration: 17 weeks):
 - Construct Elevator Tower (Duration: 5 weeks): Will involve a road closure between Main and Broad Streets starting at 10pm till 5am involving crane picks to erect tower sections and glass panels. Traffic to be detoured from Westchester Avenue to Broad Street to King Street to Adee Street to Main Street and vice versa.
 - Install elevator cab and machine room equipment (Duration: 12 weeks):
4. Village Of Port Chester Permits:
 - a. Waiver of Permit Fees



DESIGN PORT CHESTER ELEVATOR

CONTRACT NO. 1000060606

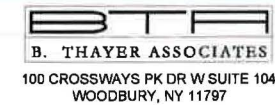


90% DESIGN SUBMISSION

January 25, 2016

Submitted By:

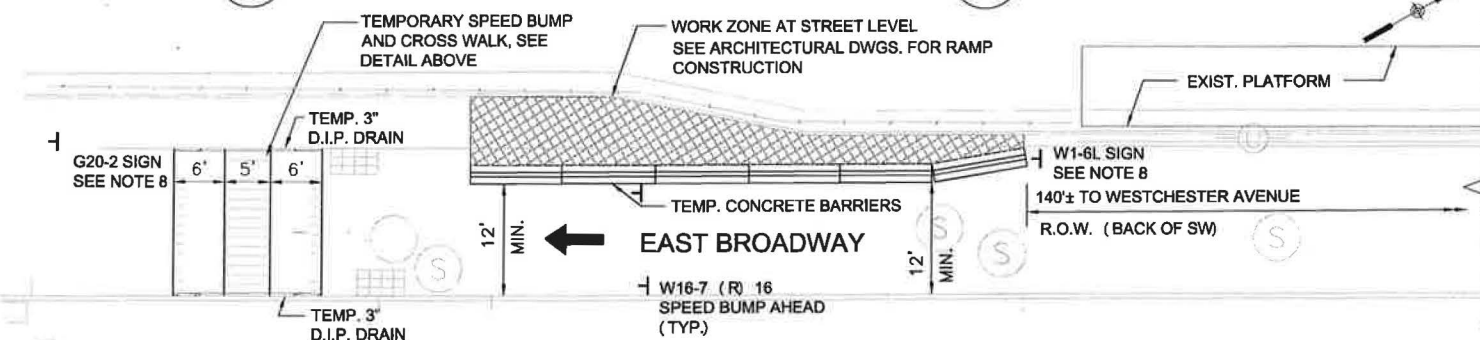
Prepared By:





5 SPEED BUMP DETAIL
MPT-001 N.T.S.

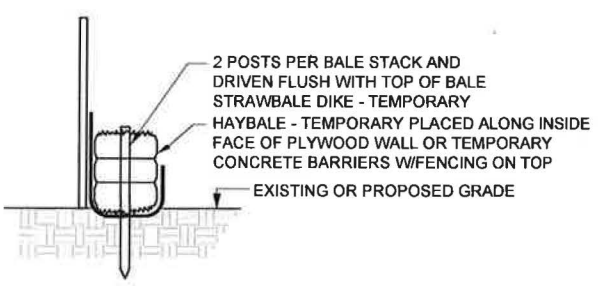
6 PEDESTRIAN RAMP
MPT-001 N.T.S.



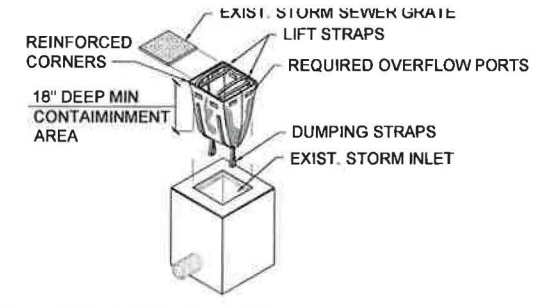
1 WORK ZONE TRAFFIC CONTROL PLAN- EAST BROADWAY
MPT-001 SCALE: 1" = 10'-0"

GENERAL NOTES:

- OBTAIN NECESSARY PERMITS FOR TEMPORARY CLOSURE OF SIDEWALK; VILLAGE OF PORT CHESTER PUBLIC WORKS FOR SIDEWALK OPENING AND COUNTY OF WEST CHESTER DPW AND TRANSPORTATION FOR STREET LANE CLOSURE (KEVIN ROSENMAN 914-995-4084) . CALL 753 FOR UTILITY MARK OUTS IN STREET AND SIDEWALK IS REQUIRED PRIOR TO ANY DRILLING OR EXCAVATION WORK. A MINIMUM TRAVEL WIDTH OF 10-FEET SHALL BZ W E MAINTAINED FOR NB WESTCHESTER AVENUE TRAFFIC.
- WORK SHALL BE COORDINATED WITH METRO NORTH AND THE WESTCHESTER BEE-LINE BUS (#13 BUS ROUTE) .
- SET TEMPORARY BARRIERS (WATER FILLED WITH 6' CHAIN LINK FENCING WITH SCREEN ON TOP SUCH AS "YODOCK" OR APPROVED EQUAL) IN STREET AS SHOWN AND DETOUR PEDESTRIANS TO ACROSS THE STREET WITH ADVANCE SIGNAGE. CONSTRUCTION SIGNS SHALL BE PLACED IN FRONT OF WORK ZONE IN BOTH DIRECTIONS IDENTIFYING "MEN WORKING" (W21-1) , "SIDEWALK CLOSED" (R9-9) AND ARROW SIGN (W16-5PR AND W16-5PL) POINTING TO ACROSS THE STREET. PLACE "ROAD WORK AHEAD" (W20-1) SIGN AT THE NWC OF WESTCHESTER AVENUE AND KING STREET SIMILARLY AS SHOWN ON NYSDOT STANDARD SHEET 619-50. PLACE "END ROAD WORK" (G20-2) SIGN AT THE NEC OF WESTCHESTER AVENUE AND BROAD STREET.
- MAINTAIN AND PROTECT EXISTING SIGNS WITHIN WORK ZONE. ANY DAMAGE TO SIGNS OR AMMENITIES IN WORK ZONE INCURRED BY THE WORK SHALL BE REMEDIATED TO THE SATISFACTION OF METRO NORTH AND THE VILLAGE OF PORT CHESTER. SIGNS REMOVED DURING COURSE OF WORK ARE TO BE REINSTALLED IN-KIND AT CONCLUSION OF WORK.
- ACCESS INTO AND OUT OF THE BUILDING (ONE STATION PLAZA RESTAURANT) MUST BE MAINTAINED AT ALL TIMES DURING WORK.
- THE APPROXIMATE LIMITS OF THE CONSTRUCTION PLATFORM TO SUPPORT A PILE RIG FOR DRIVING MINI-PILES IS SHOWN. SEE GEOTECHNICAL DRAWINGS SHOWING WORK. LOCATION OF TEMPORARY CONCRETE BARRIERS CAN BE SHIFTED IN ORDER TO LIFT EQUIPMENT ONTO THE PLATFORM AND/OR TO REMOVE CONSTRUCTION DEBRIS DURING OFF-PEAK HOURS OR DURING TIMES DENOTED IN THE GENERAL DRAWINGS. ALL TEMPORARY BARRIERS MUST BE PLACED BACK INTO POSITION DURING HOURS NOTED IN THE GENERAL DRAWINGS. REMOVE EXISTING CB, STAIRS CS-2 AND RETAINING WALL (PARTIAL) .
- CONSTRUCTION DEBRIS TO BE CONFINED WITHIN THE CONTRACTOR WORK AREA AND BE REMOVED AT THE END OF EACH DAY.
- PLACE "ROAD WORK AHEAD" (W20-1) SIGN AT THE SWC OF WESTCHESTER AVENUE AND EAST BROADWAY. PLACE A LEFT ARROW (W1-6L) SIGN AT THE BEGINNING OF THE TEMPORARY CONCRETE BARRIERS (FRONT OF TAPERED END SECTION) AND A "END ROAD WORK" (G20-2) SIGN AT THE NWC OF E. BROADWAY AND WILLIAM ST.



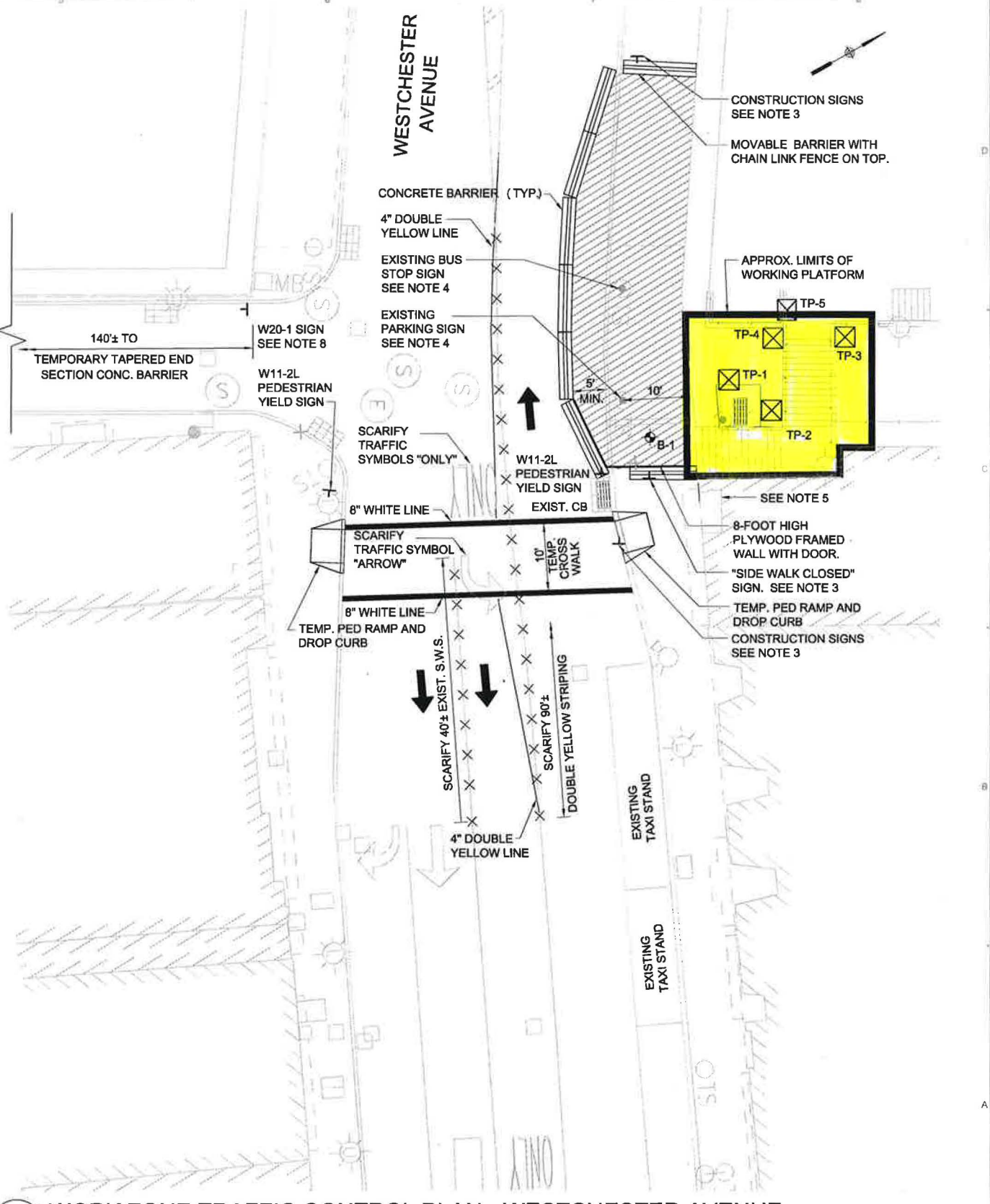
3 LINEAR PROTECTION
MPT-001 N.T.S.



4 TEMPORARY CATCH BASIN INSERT FOR DRAINAGE STRUCTURES
MPT-001 N.T.S.

LEGEND:

CONTRACTOR WORK AREA/ STAGING OF EQUIPMENT AND TOOLS



2 WORK ZONE TRAFFIC CONTROL PLAN - WESTCHESTER AVENUE
MPT-001 SCALE: 1" = 10'-0"

90% DESIGN SUBMISSION

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<p>NO. DATE MODIFICATION REVISIONS DRAWN CHKD APPRVD</p>					<p>SCALE: AS NOTED DATE: 01/25/2016</p>		<p>DRAWING NUMBER MPT-001</p>		<p>WORK ZONE TRAFFIC CONTROL PLAN-1 WESTCHESTER AVE AND E. BROADWAY</p>		<p>SHEET XX OF XX</p>			

GENERAL NOTES

1.0 SUPPORT OF EXCAVATION GENERAL NOTES

- 1.1 THE SUPPORT OF EXCAVATION (E.G. DRILLED MINIPILES (MINIPILES), LAGGING, TIEBACK ANCHORS) SHALL BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS ON THE CONTRACT DRAWINGS AND RELEVANT SECTIONS OF THE SPECIFICATIONS. THE WALLS ARE TO CONSIST OF DRILLED MINIPILES WITH TREATED TIMBER LAGGING INSTALLED BETWEEN THE MINIPILES AS EXCAVATION PROCEEDS IN FRONT OF THE WALLS.
- 1.2 LAYOUTS AND LIMITS OF THE SUPPORT OF EXCAVATION SYSTEM SHOWN HEREIN ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY ACTUAL LAYOUT AND LIMITS IN THE FIELD PRIOR TO CONSTRUCTION.
- 1.3 REPORT LOCATIONS AND ELEVATIONS OF UTILITIES, STRUCTURES, AND OBSTRUCTIONS WHICH CONFLICT WITH THE DESIGN LOCATION OF THE SUPPORT OF EXCAVATION SYSTEM.
- 1.4 TOLERANCES FOR INSTALLATION OF STRUCTURAL ELEMENTS ARE AS FOLLOWS:
 - VERTICALITY OF MINIPILES TO BE WITHIN TWO PERCENT OF PLUMB.
 - MINIPILES SHALL BE WITHIN 3 INCHES OF PLAN LOCATION.
 - WALERS AND TIEBACK ANCHORS SHALL BE WITHIN 6 INCHES OF DESIGN ELEVATION.
- 1.5 WELDING SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE AMERICAN WELDING SOCIETY STRUCTURAL WELDING CODE, AWS D1.1-08. WELDING ELECTRODES TO BE E7018.
- 1.6 LAGGING SHALL BE INSTALLED SUCH THAT GAPS BETWEEN HORIZONTAL LAGGING BOARDS ARE ONE-HALF INCH NOMINAL AND ONE-INCH MAXIMUM. VERTICAL LAGGING BOARDS SHALL BE INSTALLED WITHOUT A GAP BETWEEN BOARDS.
- 1.7 THE MAXIMUM EXPOSED HEIGHT OF SOIL SHALL NOT EXCEED FIFTEEN INCHES PRIOR TO INSTALLING LAGGING.

2.0 MATERIAL NOTES

- 2.1 MINIPILES SHALL CONSIST OF 10-3/4 INCH O.D. WALL CASING WITH A WALL THICKNESS OF 0.59 INCHES AND A MINIMUM YIELD STRESS OF 80 KSI. ALL MATERIAL SHALL BE NEW.
- 2.2 THE EXTERIOR OF THE MINIPILE CASING ABOVE TOP OF ROCK SHALL HAVE A FUSION BONDED EPOXY COATING, MINIMUM 18 MILS IN DRY THICKNESS.
- 2.3 GROUT USED TO FILL MINIPILES SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 PSI AT 28 DAYS.
- 2.4 STRUCTURAL SHAPES AND ANCHOR BEARING PLATES SHALL CONSIST OF ASTM A572 GRADE 50 STEEL. MISCELLANEOUS STEEL FOR LACING PLATES, ETC. SHALL BE MINIMUM ASTM A36. ALL STEEL SHALL BE NEW.
- 2.5 TIMBER LAGGING SHALL BE NEW, CONSTRUCTION GRADE, ROUGH CUT HARDWOOD WITH A MINIMUM ALLOWABLE BENDING STRESS OF NOT LESS THAN 1,300 PSI.
- 2.6 TIMBER LAGGING SHALL BE PRESSURE TREATED WITH ALKALINE COPPER QUATERNARY (ACQ) WITH RETENTION OF 0.4 LB PER CUBIC FOOT.

3.0 TIEBACK ANCHOR NOTES

- 3.1 TIEBACK ANCHOR TENDONS SHALL CONFORM TO REQUIREMENTS IN SPECIFICATION SECTION 31 51 00 - TIEBACK ANCHORS.
- 3.2 GROUT SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 5,000 PSI AT 28 DAYS.
- 3.3 CENTRALIZERS, SPACERS AND ANCHOR HEADS SHALL BE AS RECOMMENDED BY TIEBACK ANCHOR MANUFACTURER TO ACCOMMODATE GROUTING OPERATIONS.
- 3.4 TIEBACK ANCHOR INSTALLATION AND TESTING SHALL BE PERFORMED IN ACCORDANCE WITH POST-TENSIONING INSTITUTE RECOMMENDATIONS UNLESS SPECIFIED OTHERWISE IN SPECIFICATION SECTION 31 51 00 - TIEBACK ANCHORS.

4.0 TIEBACK ANCHOR INSTALLATION PROCEDURE NOTES

- 4.1 DRILL DIAMETER HOLE AS SPECIFIED ON TIEBACK ANCHOR DRAWINGS TO THE REQUIRED DEPTH WITH CASING, DRILL RODS AND APPROPRIATE BITS AS DRILLING CONDITIONS DICTATE.
- 4.2 INSTALL THE TIEBACK ANCHOR TENDON AS PER SPECIFICATION SECTION 31 51 00 - TIEBACK ANCHORS.
- 4.3 TOP OFF HOLE WITH GROUT DURING CASING REMOVAL AS REQUIRED.
- 4.4 INSTALL WALER SEAT, WALER, BEARING PLATE, AND ANCHOR HEAD.
- 4.5 TEST AND LOCK OFF TIEBACK ANCHORS ONCE GROUT HAS ACHIEVED A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI.

5.0 TIEBACK ANCHOR TESTING PROCEDURE NOTES

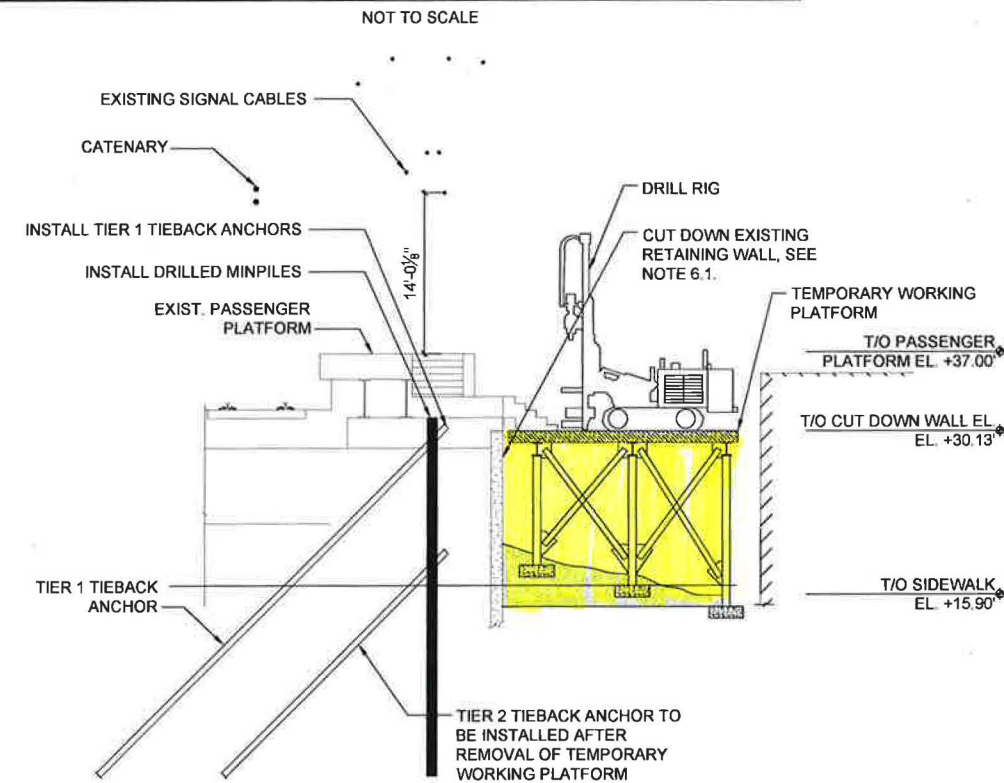
- 5.1 FOR TIEBACK ANCHOR TESTING PROCEDURES AND REQUIREMENTS, REFER TO SPECIFICATION SECTION 31 51 00 - TIEBACK ANCHORS. PERFORMANCE TEST A MINIMUM OF TWO TIEBACK ANCHORS. ANCHORS SHALL BE TESTED TO 133% OF DESIGN LOAD (TL = 1.33xDL). ALL OTHER ANCHORS SHALL BE PROOF TESTED.

6.0 SUGGESTED SUPPORT OF EXCAVATION CONSTRUCTION SEQUENCE

- 6.1 SAW-CUT AND REMOVE TOP 3 FEET OF THE EXISTING RETAINING WALL AND STAIR STRUCTURE. SLOPE SOIL RETAINED BY WALL AT 2 HORIZONTAL:1 VERTICAL TO MAINTAIN BEARING FOR HIGH-LEVEL PASSENGER PLATFORM PIER FOOTINGS.
- 6.2 CONSTRUCT TEMPORARY WORKING PLATFORM AT TOP ELEVATION OF CUT-DOWN RETAINING WALL TO PROVIDE ACCESS FOR LOW-HEADROOM DRILL RIG, TIEBACK ANCHOR DRILL RIG, AND ANY SUPPORTING EQUIPMENT. CONTRACTOR IS RESPONSIBLE FOR DESIGN OF THE TEMPORARY PLATFORM (TO BE PERFORMED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF NEW YORK).
- 6.3 LAYOUT LOCATION OF MINIPILES.
- 6.4 INSTALL MINIPILES.
- 6.5 FILL MINIPILES WITH GROUT TO TOP.
- 6.6 INSTALL TIER 1 TIEBACK ANCHORS. TEST AND LOCK OFF TIEBACK ANCHORS. REMOVE TEMPORARY WORKING PLATFORM.
- 6.7 REMOVE EXISTING RETAINING WALL AND STAIR STRUCTURE SEQUENTIALLY SO AS TO INSTALL LAGGING AS EXCAVATION PROCEEDS. EXCAVATE TO NO LOWER THAN 2 FEET BELOW THE SECOND TIER TIEBACKS.
- 6.8 INSTALL TIER 2 TIEBACK ANCHORS. TEST AND LOCK OFF TIEBACK ANCHORS. INSTALL LAGGING AS EXCAVATION PROCEEDS.
- 6.9 EXCAVATE AS REQUIRED FOR ELEVATOR PIT, MACHINE ROOM, AND PERMANENT CONCRETE WALL FOOTING INSTALLATION, INSTALL LAGGING AS EXCAVATION PROCEEDS.
- 6.10 INSTALL CAST-IN-PLACE CONCRETE WALL ON THE FACE OF THE MINIPILE WALL (SEE STRUCTURAL DRAWINGS).



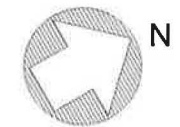
CONCEPTUAL TEMPORARY WORKING PLATFORM PLAN



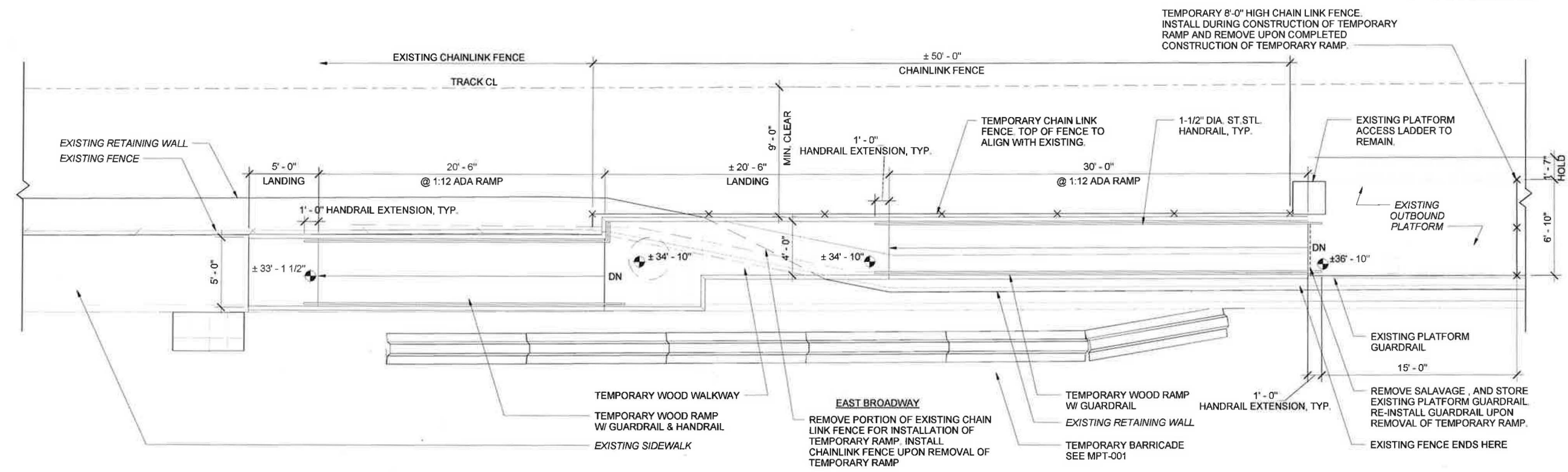
CONCEPTUAL TEMPORARY WORKING PLATFORM SECTION

90% DESIGN SUBMISSION

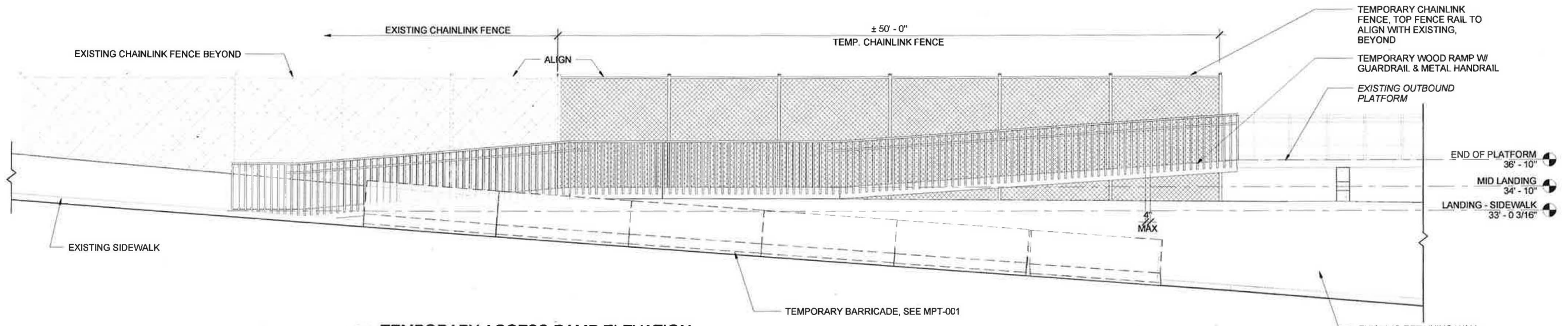
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NO.	DATE	MODIFICATION	DRAWN	CHKD.	APPRVD.								
REVISIONS													



NOTES:
 1. ALL WOOD SHALL BE PT FIRE RETARDANT TYPE
 2. RAMP SURFACE SHALL BE COATED WITH A SLIP RESISTANT EPOXY PAINT.
 3. CONTRACTOR IS RESPONSIBLE FOR PROVIDING THE DESIGN OF THE TEMPORARY RAMP. TEMPORARY RAMP SHALL BE DESIGNED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF NY.



2 TEMPORARY ACCESS RAMP PLAN
 AP-101 SCALE: 1/4" = 1'-0"

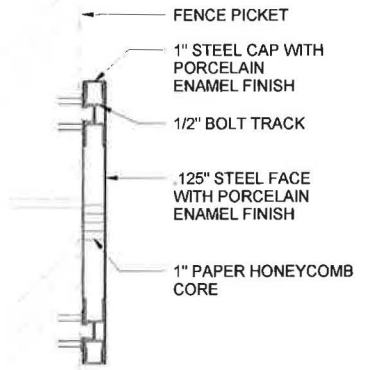


1 TEMPORARY ACCESS RAMP ELEVATION
 AP-101 SCALE: 1/4" = 1'-0"

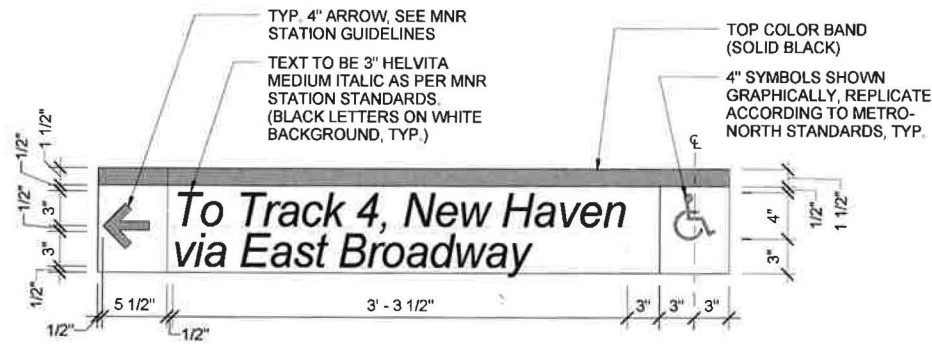
90% DESIGN SUBMISSION

NO. DATE MODIFICATION DRAWN CHKD. APPRVD. REVISIONS					DISCLAIMER IT IS A VIOLATION OF THE PROFESSIONAL LICENSE LAW FOR ANY PERSON TO ALTER THIS DRAWING IN ANY WAY, UNLESS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER/ARCHITECT AS APPLICABLE. THE ALTERNATION ENGINEER/ARCHITECT SHALL AFFIX HIS/HER SEAL AND THE NOTATION "ALTERED BY" FOLLOWED BY HIS/HER SIGNATURE AND DATE OF ALTERATION.		DESIGNED R. WITTE DRAWN K. SNYDER CHECKED L. JACOBITTI APPROVED R. SULLIVAN		people, building, and planet sowinskisullivan ARCHITECTS				DFSIGN PORT CHESTER ELEVATOR ARCHITECTURAL TEMP. RAMP AT WEST END		CONTRACT NUMBER 1000060606 AS NOTED DATE: 01/25/16 DRAWING NUMBER AP-101 SHEET OF	
--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--

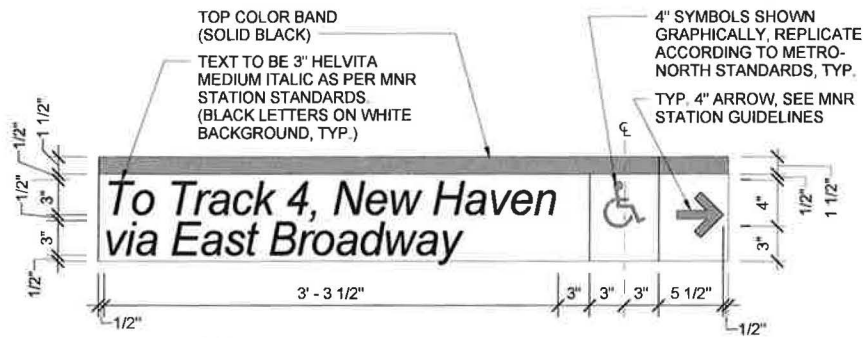
TEMPORARY SIGNAGE SCHEDULE													
SIGN TYPE	DESCRIPTION	MOUNTING	# SIDES	DIMENSIONS		FONT	TACTILE	BRAILLE	COLOR		SYMBOLS	DETAILS	QUANTITY
				WIDTH	HEIGHT				TEXT	BACKGROUND			
1	TO TRACK 4 - LEFT ARROW	WALL	1	4' - 6 1/2"	9"	HELVETICA MEDIUM ITALIC	NO	NO	BLACK	WHITE	YES		2
2	TO TRACK 4 - RIGHT ARROW	WALL	1	4' - 6 1/2"	9"	HELVETICA MEDIUM ITALIC	NO	NO	BLACK	WHITE	YES		2
3	EXIT	WALL	1	2' - 6"	8"	HELVETICA MEDIUM	NO	NO	WHITE	RED-PMS 186	YES		2



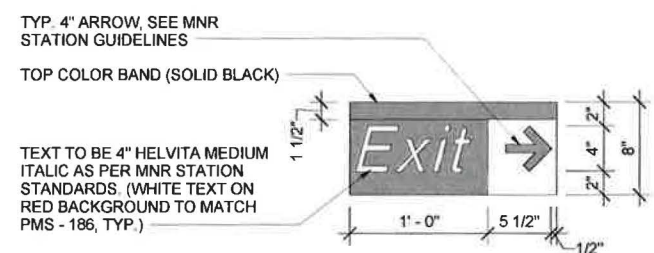
4 STONE MOUNTING DETAIL
SCALE: 3" = 1'-0"



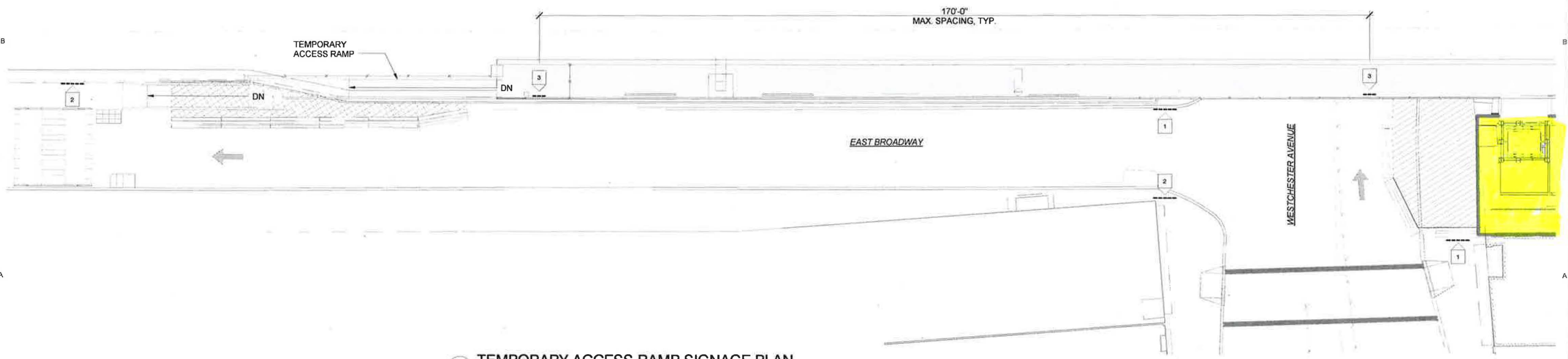
1 TEMPORARY SIGN TYPE 1
SCALE: 1 1/2" = 1'-0"



2 TEMPORARY SIGN TYPE 2
SCALE: 1 1/2" = 1'-0"



3 TEMPORARY SIGN TYPE 3
SCALE: 1 1/2" = 1'-0"



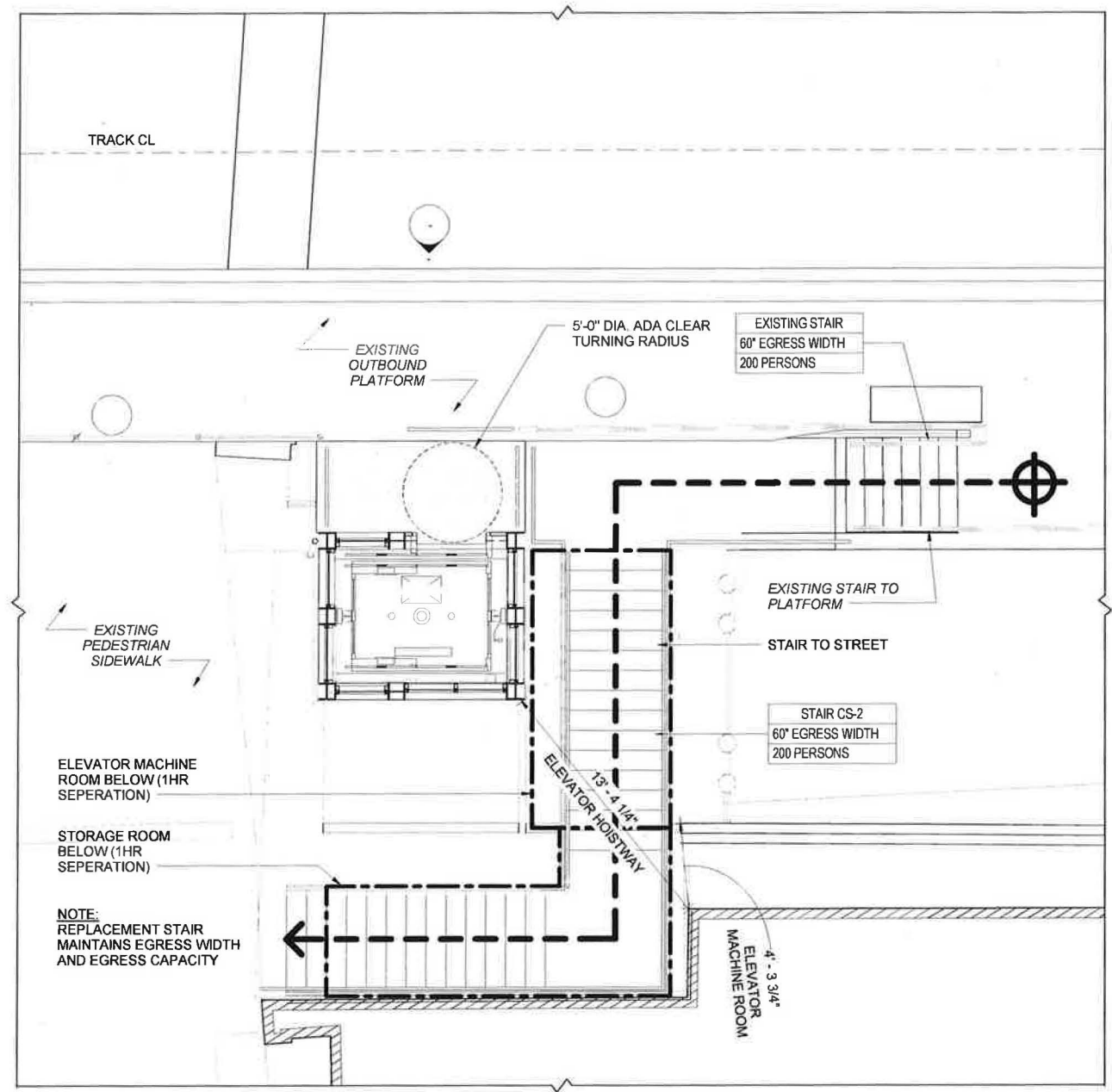
5 TEMPORARY ACCESS RAMP SIGNAGE PLAN
SCALE: 1" = 10'-0"

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NO. DATE MODIFICATION DRAWN CHKD. APPRVD. REVISIONS					DISCLAIMER <small>IT IS A VIOLATION OF THE PROFESSIONAL LICENSE LAW FOR ANY PERSON TO ALTER THIS DRAWING IN ANY WAY, UNLESS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER/ARCHITECT AS APPLICABLE. THE ALTERATION ENGINEER/ARCHITECT SHALL AFFIX HIS/HER SEAL AND THE NOTATION "ALTERED BY" FOLLOWED BY HIS/HER SIGNATURE AND DATE OF ALTERATION.</small>		DESIGNED R. WITTE DRAWN K. SNYDER CHECKED L. JACOBITTI APPROVED R. SULLIVAN		people, building, and planet sowinskisullivan ARCHITECTS				CONTRACT NUMBER 1000060606 SCALE: AS NOTED DATE: 01/25/16 DRAWING NUMBER AP-102 SHEET OF --	
									DFIGN PORT CHESTER ELEVATOR ARCHITECTURAL TEMP. SIGNAGE PLAN, SCHEDULE & DETAILS					

MNR - Port Chester Elevator	
Code Data	
Codes	
Building Code	2010 New York State Building Code
Fire/Life Safety Code	2010 New York State Fire Code (2006 IFC)
Accessibility Code	2010 New York State ADA 2010 ADA Standards for Accessible Design (ADAAG 2004)
Energy Code	2010 New York State Energy Conservation Code (2006 IECC)
Gas Code	2010 New York State Fuel Gas Code (2006 IFGC)
Electrical Code	NFPA 70
Elevator Code	ASME A17.1
Standards	
MNR Station Standards - Current Version	
MNR Signage Manual - Current Version	
Regulations	
Federal Railroad Administration (FRA) Regulations	
Occupational Safety and Health (OSHA) Regulations	

MNR - Port Chester Elevator				
Code Review - 2010 NYS Building Code				
Code Section	Section Title	Required	Provided	Remark
Chapter 3 - Use and occupancy Classification				
303	Use group	A-3	A-3	
Chapter 5 - General Building Heights and Areas				
Table 503	Height/Area	2 st. / 9,500 s.f.	2 st. / 160 s.f.	
Chapter 6 - Types of Construction				
Table 601	Construction Type	IIB	IIB	Non Combustible
Table 601	Fire resistance rating			
	primary structure	0	0	
	bearing walls exterior	0	0	
	Bearing walls interior	0	0	
	Non bearing exterior walls	See table 602		
	Non bearing interior walls	0	0	
	Floor construction	0	0	
	Roof construction	0	0	
Table 602	fire rating exterior, separation distance	See table 602		
	x < 5	1	1	Elevator Machine Room
	5 < x < 10	1	NA	
	10 < x < 30	0	0	Elevator Hoistway
	x > 30	0	NA	
Chapter 7 - Fire and Smoke Protection				
707.5	Shaft enclosure, fire rating			
	Less than 4 stories	1	NA	
707.6	Shaft enclosure, Exterior walls	Per Section 704	0	704.5 references table 601 & 602
Chapter 9 - Fire Protection Systems				
903.2.1.3	Automatic Sprinkler system - Group A-3			
	Area exceeds	12,000 s.f.	-	Not Required
	Occupant load exceeds	100 persons	-	Not Required
	Fire Area	Level other than discharge level	-	Not Required
903.3.1.1.1	Exempt locations	Areas that are of non combustible construction with wholly non combustible contents. An approved automatic fire detection system must be provided per 907.2		
907.2.1	Manual fire alarm boxes	Required		
Chapter 10 - Means of Egress				
1009.1	Stair width	44" min.	60"	
1009.2	Headroom at stair	80" min.	-	
1009.3.2	Stair treads and risers			
	Treads	11" min.	11"	
	Riser	7" max / 4" min	See details	
1009.4.4	Stair - Dimensional Uniformity tolerance	.375" per flight	-	
1009.4	Stair Landing			
	Width	Stair width	60"	
	Depth	Stair width	60"	Need not exceed 48"
1009.5.1	Stairway walking surface	1.48 max.	-	
1009.5.3	Enclosure under stairs	1 hour	1 hour	Elevator Machine Room
1009.6	Vertical rise (between levels)	12'-0" max.	9'-3" max.	
1009.1	Handrail	Each side of stair comply with 1012		
1012	Handrails			
	Height (Min. / Max.)	34" - 38"	36"	
	Graspability (Min. / Max.)	1 1/4" - 2"	1 1/2"	
	Continuity	continuous	continuous	
	Extensions (top/bottom)	12" tread depth	12"/12"	
Chapter 24 - Glass and Glazing				
2409.1	Glass in elevator enclosures	laminated glass conforming to ANSI Z97.1 or 16 CFR Part 1201		
Chapter 30 - Conveying Devices				
3002.1	Hoistway enclosure protection			
	See 707 for shaft enclosure	0	0	
3002.8	Glass in Elevator Enclosure	Comply with Section 2409.1		
3006.4	Machine rooms and machinery spaces			
	Fire Rating Not less than the hoistway	0	1	Required per 1009.5.3 and Table 602
3006.6	Plumbing Systems	Not permitted in machine rooms		



EGRESS PLAN
 SCALE: 1/4" = 1'-0"
 0 4' 8' 16'

EGRESS LEGEND

DESCRIPTION	EGRESS DESCRIPTION
[Line]	EGRESS WIDTH
[Circle]	MAX OCCUPANT CAPACITY
[Arrow]	PATH OF EGRESS
[Dashed Line]	1HR FIRE RATING

90% DESIGN SUBMISSION

NO	DATE	MODIFICATION	DRAWN	CHKD	APPRVD
REVISIONS					

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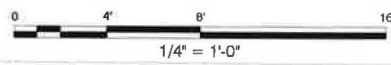
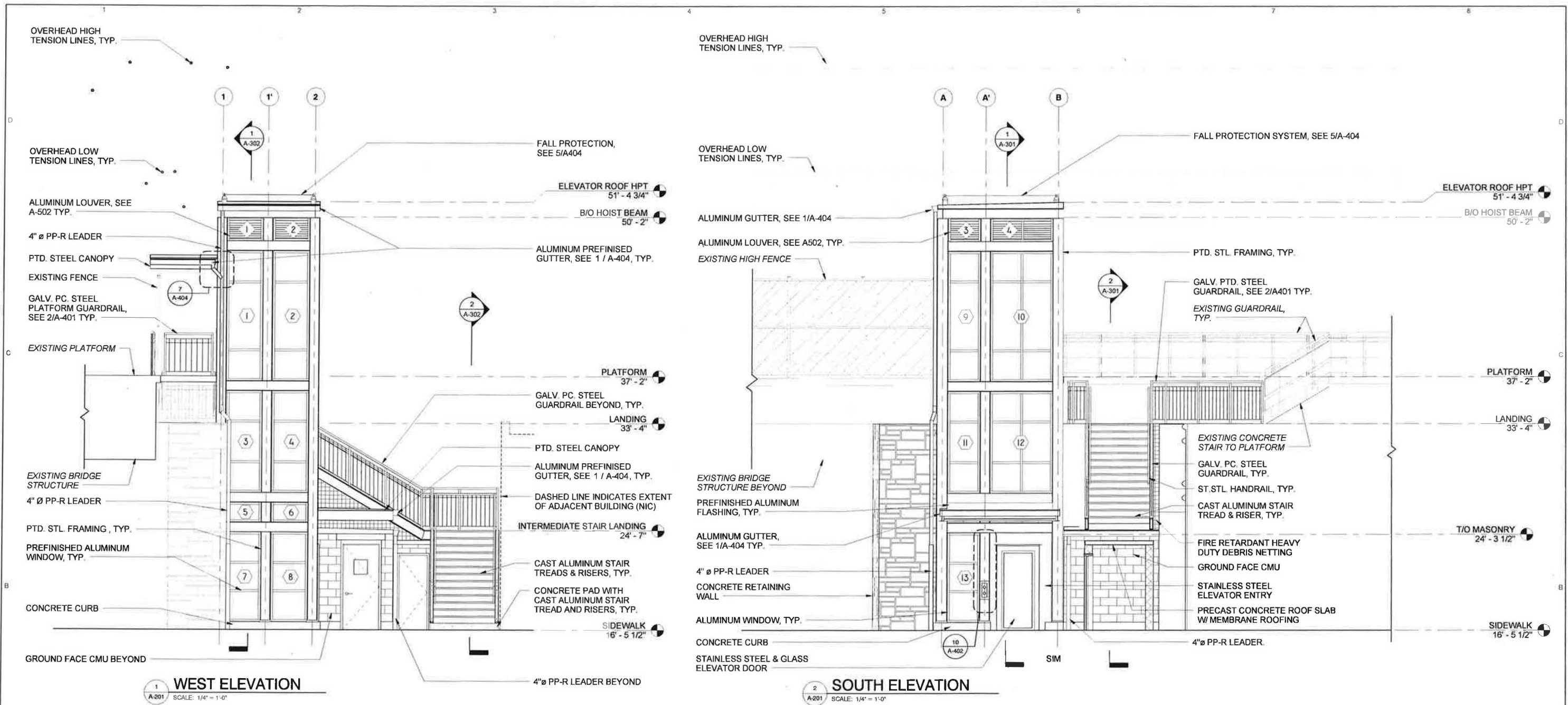
DESIGNED
 R. WITTE
DRAWN
 K. SNYDER
CHECKED
 L. JACOBITTI
APPROVED
 R. SULLIVAN

people, building, and planet
 sowinskisullivan
ARCHITECTS



DESIGN PORT CHESTER ELEVATOR
 ARCHITECTURAL
 CODE DATA & EGRESS PLAN

CONTRACT NUMBER	1000060606
SCALE:	AS NOTED
DATE:	01/25/16
DRAWING NUMBER	A-002
SHEET	OF



90% DESIGN SUBMISSION

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NO.	DATE	MODIFICATION	DRAWN	CHKD.	APPRVD.				
REVISIONS									



Typical Minipile Drill Rig





VILLAGE OF
PORT CHESTER
222 Grace Church Street, Port Chester, New York 10573

AGENDA MEMO

Department: Planning and Development Department
Department: Office of the Village Attorney

BOT Meeting Date: 3/7/2016

Item Type: Resolution

Sponsor's Name:	Anthony Cerreto, Village Attorney
Sponsor's Name:	Eric Zamft, Director of Planning & Economic Development

Description	Yes	No	Description	Yes	No
Fiscal Impact	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Public Hearing Required	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Funding Source:			BID #		
Account #:			Strategic Plan Priority Area		
	Yes	No	Transit Oriented Development		
Agreement	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Manager Priorities		
Strategic Plan Related	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Planning & Zoning		

Agenda Heading Title
(Will appear as indicated below on Agenda)

G&S Zoning Petition: LWRP Consistency, SEQRA Determination of Significance, and Zoning Text Amendments Approval

Summary

On October 1, 2015, G&S Port Chester LLC (the "Applicant") submitted an Amended Petition to the Board for amendments to (i) the Urban Renewal Plan for the MMRP, (ii) the regulations of the MUR MMRP Urban Renewal District of the Village of Port Chester, and (iii) the Concept Development Plan of the MMRP to permit residential use of the

Property, and to amend the MMRP Urban Renewal Plan and MUR District regulations to add a multifamily dwelling parking regulation to the required parking table (the “Petition”)¹.

At the December 29, 2015 Waterfront Commission meeting, the Petition was discussed and on January 4, 2016, the Waterfront Commission issued a determination that the Petition was inconsistent with the Local Waterfront Revitalization Program (“LWRP”), adopted in 1992 and amended in 1999 (to specifically accommodate the G&S project as a commercial project at that time by prohibiting residential as a use).

Beginning at the January 4, 2016 Board of Trustees meeting and continuing to the February 1, 2016 meeting, the Board held a public hearing on among other things, the question of enactment of Local Law No. 02 of 2016, being a Local Law amending the official zoning map and the text of Chapter 345, Article XVI of the Village Code (Zoning).

At the February 1, 2016 meeting, the public hearing was closed and the Applicant was requested to resolve issues related to traffic and parking in order to satisfy the New York State Environmental Quality Act (“SEQRA”) process.

Given that, there are three separate matters that need to be resolved, in order that they need to be heard:

1. **LWRP Consistency** – Although the Village is currently in the process of receiving adoption of an update to its LWRP, we are currently still bound by the 1992 (as amended in 1999) version. The Waterfront Commission found the Petition to be inconsistent with the 1992 LWRP (even though there were a number of other objections, the primary threshold was the use, as residential is precluded in that sub-area; the other objections are more appropriate for site plan approval). However, per that 1992 LWRP, the Board has the ability to have a finding that the action should proceed. The attached memorandum from staff describes how such a finding could be provided. The Board would need to adopt such finding for the project to continue.
2. **SEQRA Determination of Significance** – The Board is the Lead Agency under SEQRA. Staff, consultants, and Special Counsel have reviewed the provided material, including the most recent request for additional information on traffic, pedestrians, and parking, as well as the Waterfront Commission’s comments. Should that be satisfactory, the Board would need to adopt a determination of significance – i.e., a Negative Declaration. This would complete the SEQRA process.
3. **Zoning** – Once the SEQRA process is completed, the Board has the ability to make a determination on the actual zoning text and map amendment, as formulated in Local Law No. 02 of 2016.

Therefore, should the Board wish to proceed, the attached resolutions undertake the following actions:

¹ Counsel for the Applicant and the Village Attorney have since agreed that the amendments to the MUR MMRP Urban Renewal District are not necessary.

1. Adopts the Board's LWRP Findings
2. Adopts the SEQRA Determination of Significance
3. Adopts the Local Law

Attachments
<ul style="list-style-type: none">• Department of Planning & Economic Development LWRP Memorandum• Waterfront Commission Determination• Draft resolution on LWRP Findings• Draft resolution for SEQRA determination of significance• Draft SEQRA determination of significance• Correspondence between special counsel and Applicant• Traffic studies (Dunn, Adler review, NYSDOT, Dunn Response, Adler letter)• Draft resolution on zoning• Draft Local Law

**DEPARTMENT OF PLANNING & ECONOMIC
DEVELOPMENT MEMORANDUM ON LWRP
CONSISTENCY**



VILLAGE OF PORT CHESTER
DEPARTMENT OF PLANNING & ECONOMIC
DEVELOPMENT

222 Grace Church Street, Rm. 202
Port Chester, NY 10573
(P) 914.937.6780
(F) 914.939-2733

Eric Zamft, AICP, Director
Andrea Sherman, Planner
Constance Phillips, Planning Commission Secretary

To: Board of Trustees
From: Eric Zamft, AICP, Director of Planning & Economic Development
cc: Christopher Steers, Village Manager; Tony Cerreto, Village Attorney
Date: February 29, 2016
Re: Consistency Determination for "Retail D" Petition

Members of the Board,

Background:

This memorandum provides a discussion of the consistency of the Coney's Lot/Retail D amended petition (the "Petition") submitted by G&S Port Chester, LLC (the "Applicant") on October 1, 2015 with the Village's currently adopted Local Waterfront Revitalization Program ("LWRP"), adopted in 1992 (and which include the 1999 amendments).

As you may well know, the Village is currently coordinating an update to the LWRP with the New York State Department of State ("NYS DOS"). Until the updated LWRP is adopted by the Board of Trustees, the Village is required to review actions and applications against the 1992 LWRP.

Pursuant to Chapter 332 of the Village Code and in congruence with the LWRP regulations, the Waterfront Commission is responsible for evaluating the proposed action against the policies and purposes of the adopted LWRP and provide such determination to the Lead Agency (the Board of Trustees).

As part of its petition submission, the Applicant provided a Coastal Assessment Form for Commission review. At the December 29, 2015 Waterfront Commission meeting, the Waterfront Commission voted that the petition was inconsistent with the 1992 LWRP (see attached).

As Lead Agency, the Board of Trustees may make a consistency determination different than a determination made by the Waterfront Commission (LWRP, Section 5.3). The LWRP lists four (4) conditions that must be met for an action to proceed if the Waterfront Commission finds it inconsistent, as follows:

- 1. No reasonable alternatives exist which would permit the action to be undertaken in a manner which will not substantially hinder the achievement of such LWRP policy standards and conditions*
- 2. The action would be undertaken in a manner which will minimize all adverse effects on such LWRP policy standards and conditions to the maximum extent practicable.*
- 3. The action will advance one or more of the other coastal policies.*
- 4. The action will result in an overriding Village, regional or State-wide public benefit.*

Analysis:

To that end, the Department of Planning and Economic Development evaluated said petition against the Waterfront Commission's determination under each of the four (4) conditions. That analysis follows.

- **Condition 1** – *No reasonable alternatives exist which would permit the action to be undertaken in a manner which will not substantially hinder the achievement of such LWRP policy standards and conditions.*

Development of any type will incur impacts to local resources, and any construction at this site will utilize public infrastructure. This includes land uses that are currently allowed in the MUR District's existing zoning regulations, as well as the uses possible as part of the proposed zoning district, notably residential.

The "no-action" alternative would be to leave the project site vacant. However, doing so will directly hinder the LWRP's policy objectives, which state a need to redevelop land in the Downtown Subarea. Therefore, such alternative would hinder the achievement of the LWRP's standards and conditions and is considered not reasonable.

A second alternative would be to develop the project site under the existing MUR District regulations. Under such zoning, the Applicant had proposed a multi-story building for retail and commercial use. These uses would increase traffic generation and parking demand more than the mixed-use option currently proposed, resulting in additional traffic and air quality impacts. Therefore, such alternative would hinder the achievement of the LWRP's standards and conditions and is considered not reasonable.

The proposed petition is consistent with Condition 1 that no reasonable alternatives exist that would not substantially hinder the achievement of LWRP policy standards and conditions.

- **Condition 2** – *The action would be undertaken in a manner which will minimize all adverse effects on such LWRP policy standards and conditions to the maximum extent practicable.*

The proposed action is consistent with Condition 2 in that it would be undertaken in a manner that would minimize adverse impacts to the maximum extent practicable. The proposed action, a zoning change, will not incur adverse impacts in and of itself. Rather, it is the forthcoming site specific plan that will and should be evaluated to ensure minimal adverse effects on LWRP policy standards and conditions. This will take place during the Village's site plan approval process to the maximum extent practicable.

- **Condition 3** – *The action will advance one or more of the other coastal policies.*

The proposed action is consistent with Condition 3 as it will advance two additional coastal policies:

Policy 1: Restore, revitalize and redevelop deteriorated and underutilized waterfront areas for commercial, industrial, cultural, recreational and other compatible uses.

Policy 1D: Redevelop Port Chester owned land¹ in Subarea IV, the Downtown Subarea for commercial, recreational, residential and other compatible uses.

- **Condition 4** – *The action will result in an overriding Village, regional or State-wide public benefit.*

Said action is consistent with Condition 4 since it will result in an overriding Village public benefit to develop its downtown area and waterfront in a smart growth, transit-oriented fashion. Said action is within walking distance of the Port Chester train station and Main Street and will help to bring new residents and foot-traffic to the downtown area. These new residents and shoppers will provide significant economic benefits to the Village as patrons to local stores and restaurants. Inclusion of retail uses on the first floor will also provide economic benefits by adding to the Village's non-residential tax base.

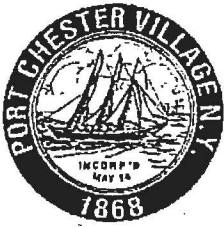
Recommendation:

Based upon the above analysis of the four (4) consistency conditions, the Department of Planning and Economic Development recommends that the Board of Trustees prepares and adopts its own written findings and that such findings state that said petition shall be able to proceed.

Note that the Waterfront Commission, in its evaluation of the LWRP Coastal Assessment Form and its Addendum, highlights a number of concerns, including infrastructure and drainage, parking, traffic, and pedestrians. These concerns are related to a specific plan for the site, rather than its zoning. Each of these concerns was evaluated for their environmental impact as part of the SEQRA process. Regardless, the Department of Planning & Economic Development suggests that such concerns be discussed as part of the site plan approval process.

¹ Note that at the time the LWRP was written in 1992, the subject site was under the Village of Port Chester's ownership. The land was later conveyed to the Port Chester Industrial Development Agency, who leased it to the Applicant as part of the larger Modified Marina Redevelopment Project.

**Waterfront Commission
LWRP Coastal Assessment Form and Addendum**



Village of Port Chester Waterfront Commission

222 Grace Church Street
Port Chester, NY 10573
Phone 914-937-6780



LWRP COASTAL ASSESSMENT FORM

An applicant seeking an approval for a non-excluded action from any agency of the Village of Port Chester which is subject to the Village of Port Chester Local Waterfront Revitalization Program (LWRP) must complete this form. This form is intended to assist the Village Waterfront Commission in determining if the proposed action is consistent with the policies and purposes of the LWRP.

SITE IDENTIFICATION INFORMATION		
Application Name: MUR Amendment	Application #:	Date Submitted:
Site Address: No. Section 142.031, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23, and 24 Street:		
Property Location: (Identify landmarks, distance from intersections, etc.) Bounded: West by N. Main Street, East by Abendoth Avenue and South by Westchester Avenue		
Village of Port Chester Tax Map Designation: Section 142.031 Block 1 Lot(s) 3,4,5,6,20,21,22,23 and 24	Zoning Designation of Site: MUR	
OWNER/APPLICANT INFORMATION		
Property Owner: G & S PORT CHESTER LLC	Phone #: 914-481-1880 Fax #: 914-206-4858	Email: ddt@tartagliaalawgroup.com
Owner Address: No. 211 Street: East 43rd Street, 25th floor Town: New York State: NY Zip:		
Applicant (If different than owner):	Phone #: Fax #:	Email:
Applicant Address (If different than owner): No. Street: Town: State: Zip:		
PROJECT DESCRIPTION		
Describe the project, proposed use and operation thereof: the MMRP Urban Renewal Plan and the regulations of the MUR District be amended to create a new district to be referred to as the "MUR-Mixed Use District" specifically limited to Land Use Area 2, that will principally permit: (i) multi-family dwellings (floors above first floor); and require (ii) maximum building height of five (5) stories or 70 feet; (iii) maximum Floor Area Ratio of 3.7 (iii) minimum lot area per dwelling unit of 240 sq. ft.; (iv) no requirement for lot width or lot depth; (v) no minimum yard requirements; and (vi) no minimum usable open space per dwelling unit		

BAO

CONSISTANCY WITH LWRP POLICIES

Actions to be undertaken within the Village's coastal area shall be evaluated for consistency in accordance with the following LWRP policy standards and conditions, which are derived from and further explained and described in Section III of the Village of Port Chester LWRP, a copy of which is on file in the Village Clerk's office and available for inspection during normal business hours. Agencies that undertake direct actions shall also consult with Section IV of the LWRP in making their consistency determination.

*YES indicates that the proposed project/action is consistent with the applicable policy,
 NO indicates that the proposed project/action is inconsistent with the applicable policy,
 NOT APPLICABLE indicates that it does not apply.*

Restore, revitalize and redevelop deteriorated and underutilized waterfront areas for commercial, industrial, cultural, recreational, public access and other compatible uses (Policies 1, 1A, 1B, 1C, 1D and 1E).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable
Facilitate the siting of water dependent uses on or near coastal waters (Policy 2).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable
Encourage the development and enhancement of those traditional uses and activities which have provided the Village a unique maritime identity (Policy 4).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable
Ensure that development occurs where adequate public infrastructure is available (Policy 5).	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p align="center"><i>See ATTACHMENT</i></p> Streamline development permit procedures (Policy 6).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable
Protect fish and wildlife resources from hazardous wastes and other contaminating pollutants (Policy 8).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable
<p align="center"><i>See ATTACHMENT</i></p> Maintain and expand the recreational use of existing fish and wildlife resources while ensuring protection of renewable fish and wildlife resources (Policy 9).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable
Maintain and expand commercial fishing resources to promote commercial fishing opportunities (Policy 10).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable

BAD

Minimize flooding and erosion hazards through proper siting of buildings and structures; construction of carefully selected, long-term structural measures; and the use of appropriate nonstructural means (Policies 11, 13, 14, 15 and 17).

Yes No
 Not Applicable

SEE ATTACHMENT

Public funds shall be used for erosion protection structures only where necessary and in an appropriate manner (Policy 16).

Yes No
 Not Applicable

Safeguard vital economic, social and environmental interests in the coastal area when major actions are undertaken (Policy 18).

Yes No
 Not Applicable

Protect, maintain, and improve public access to the shoreline and to water-related recreational resources, while protecting natural and historic resources and adjacent land uses and providing a continuous public walkway along the foreshore of the Byram River (Policies 19, 20 and 20A).

Yes No
 Not Applicable

SEE ATTACHMENT

Encourage, facilitate and prioritize water-dependent and water-enhanced recreational resources and facilities near coastal waters, with priority given to those near access to existing public transportation and areas where the use of the shore is severely restricted by existing development (Policy 21).

Yes No
 Not Applicable

Encourage the development of water-related recreational resources and facilities, as multiple uses, in appropriate locations within the shore zone (Policy 22).

Yes No
 Not Applicable

Protect, restore and enhance historic resources (Policy 23).

Yes No
 Not Applicable

Protect and enhance scenic and aesthetic resources (Policy 25).

Yes No
 Not Applicable

Site and construct energy facilities in a manner which will be compatible with the environment and contingent upon the need for a shorefront location and in such a manner as to avoid adverse environmental impacts when in operation (Policies 27 and 29).

Yes No
 Not Applicable

BAD

Protect coastal waters from direct and indirect discharge of pollutants (Policies 30, 31, 33, 34, 35, 36 and 37).

Yes No
 Not Applicable

SEE ATTACHMENT

Ensure that dredging and dredge spoil disposal are undertaken in a manner protective of natural resources (Policies 15 and 35).

Yes No
 Not Applicable

Ensure that any transportation, handling or disposal of hazardous wastes and effluent is undertaken in a manner which will not adversely affect the environment (Policy 39).

Yes No
 Not Applicable

SEE ATTACHMENT

Protect air quality in accordance with state and federal air quality standards (Policies 41 and 42).

Yes No
 Not Applicable

SEE ATTACHMENT

Preserve and protect tidal and freshwater wetlands (Policy 44).

Yes No
 Not Applicable

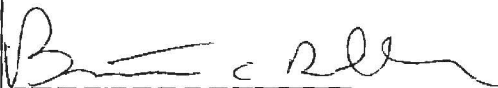
SEE ATTACHMENT

DETERMINATION OF CONSISTENCY

To be filled out by the Waterfront Commission

- This project/action is consistent with LWRP Policies and Conditions
- This project/action is NOT consistent with LWRP Policies and Conditions

The following changes to the project/action are recommended to ensure consistency with the LWRP:



Waterfront Commission Chairman

1/4/16

Date

Addendum to
LWRP Coastal Assessment Form

MUR Amendment
Section 142.031, Block 1, Lots 3,4,5,6,20,21,23 and 24

Policy 5, The Commission is concerned that adequate public infrastructure is not available beyond the likely life of the proposed Zoning change for the proposed use as described by the applicant.

Findings, The Commission has concern over the long term viability of the Buckley Drain as it transverses the above referenced location. The current condition of the drain which services a high volume of the Village of Port Chester for storm water drainage has particular effect on the majority of the Downtown Waterfront Revitalization Zone and its economic success and viability.

So much so that the Commission considers this drain and its continued operation as critical in nature.

Major flooding has been demonstrated in the past as storm drain water overflow rises out of the sewers along both North & South Main Street, Abendroth Avenue and Westchester Avenue between Waterfront Place and Broad Street.

The Commission has also identified the basin area formed by the convergence of Hasco, Bush and Glen Avenues where the Buckley drain first enters the piping system to the eventual end at the Byram River as a high potential should the piping system fail as this is where the open stream would back up after the flow becomes greater than what can flow out of the street drains which are connected to the Buckley Drain is overwhelmed between the above mentioned site and the start of the piping.

Besides flooding, the commission is concerned about the general flow of pedestrian traffic as an economic condition as well.

The applicant proposes no onsite parking, however there are a considerable number of pedestrians envisioned wanting to ingress and egress the site on a daily basis. Pedestrian safety is of the upmost concern for the economic viability of the Waterfront District and there are six lanes of traffic to be crossed between the site and the proposed parking area. As for general parking daytime parking there are four lanes. All turning lanes in this intersection make use of turning arrows to improve traffic flow but this practice occurs at the expense of adequate time for pedestrians to cross safely.

Finally, the Commission has concerns over traffic and the operation of the loading / unloading of passengers and the arrival and backing up of moving vans as rental units show a high turnover rate for two reasons.

Bad

Reason one, The elevation of this entrance seems to be below the 100 year flood mark. How does this affect the viability of the project and its use pattern?

Reason two, The proposal does not provide drive through access for large vehicles, hence this would then require traffic interruptions when large vehicles approach the site so that they can back into the site. This occurs now in a number of places in the LWRP area and may be a factor with its proximity to the intersection.

Policy 8, The applicant has checked off "Not Applicable", the Commission disagrees and calls for the best practices be defined and made as a condition of approval to insure compliance with the policies of the LWRP.

Policy 11, 13, 14, 15 and 17 as well as Policy 18, The Commission disagrees with the applicants "No" answer and refers back to the Buckley drain discussion.

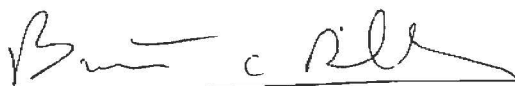
Policy 19, 20 and 20A, The Commission disagrees with the applicant's answer of "Not Applicable". Because there is no onsite parking being provided, the Commission believes that the economic viability clearly dictates that any approval must be subject to assigned parking limited to the parking structure on Waterfront Place. Any designated parking in any of the lots along the Byram River will detract from public access and the economic goals of the LWRP.

Policy 30,31,33,34,35,36 and 37, The Commission agrees with the applicant's answer but highlights the need in all the approval stages that best practices and the most modern methods be implemented.

Policy 39, 41 and 42, The Commission agrees with the applicant's answer but highlights the need in all the approval stages that best practices and the most modern methods as well as strict adherence to ALL Federal, State, County and Local Laws be fully described and be implemented as to achieve the goals of the LWRP.

Policy 44 , The Commission disagrees with the applicant's answer of "Not Applicable". There are State and Federal identified tidal wetlands with 500 feet of the site. The need for a plan including remediation measures are essential and of the utmost importance for the Commission and the objective of the LWRP.

General comment, The Commission has an overarching concern about construction staging, delivery of materials to the site, motor vehicle traffic lane closures and sidewalk closures during the construction period. This can't be left to experts alone and must be incorporated into the approvals before a building permit is issued.

 1/4/16
Brent C. Rill
CHAIRMAN, PORT CHESTER WATERFRONT COMMISSION

**BOARD OF TRUSTEES RESOLUTION:
LWRP CONSISTENCY FINDINGS**

RESOLUTION

FINDINGS OF THE VILLAGE OF PORT CHESTER BOARD OF TRUSTEES WITH REGARDS TO THE CONSISTENCY OF THE G&S RETAIL D PETITION WITH THE VILLAGE'S ADOPTED 1992 LOCAL WATERFRONT REVITALIZATION PLAN

On motion of TRUSTEE _____, seconded by TRUSTEE _____, the

following resolution was adopted by the Board of Trustees of the Village of Port Chester, New

York:

WHEREAS, on October 1, 2015, the Village Board of Trustees received an amended zoning petition from G&S Port Chester LLC (herein referred to as "Applicant") to amend the respective zoning map and text amendments to the Urban Renewal Plan for the Modified Marina Redevelopment Project, regulations to the MUR Marina Redevelopment Project Renewal District, and Concept Development Plan of the Modified Marina Redevelopment to permit multi-family residential development (the "action" or "petition" or "proposal"); and

WHEREAS, the proposal includes the parcels designated as Section 142.31, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23 and 24 of the Town of Rye Official Tax Map as well as the interstitial Liberty Street Right-of-Way; and

WHEREAS, the proposal specifically seeks creation of a new "MUR-MU Marina Urban Redevelopment Mixed Use District" to develop the subject site for a mixed residential and commercial use development, consisting of approximately 12,000 square feet of ground floor retail space and 60,000 square feet of residential space (79 residential dwelling units, primarily studios and one-bedroom units), which includes a density bonus provision for additional floor area; and

WHEREAS, pursuant to Chapter 332 of the Village Code and in congruence with the Village of Port Chester's current (1992, as amended in 1999) Local Waterfront Revitalization Program ("LWRP") regulations, the Village of Port Chester Waterfront Commission ("Waterfront Commission") is responsible for evaluating the proposed action against the policies and purposes of the adopted LWRP and provide such determination to the Lead Agency (the Board of Trustees).

WHEREAS, as part of its petition submission, the Applicant provided a Coastal Assessment Form for review by the Waterfront Commission.

WHEREAS, on December 29, 2015 said petition was heard by the Village of Port Chester Waterfront Commission at its meeting; and

WHEREAS, at the December 29, 2015 Waterfront Commission meeting the Waterfront Commission voted that the petition was inconsistent with the 1992 LWRP, as indicated in its January 4, 2016 determination; and

WHEREAS, the Waterfront Commission was bound by the 1992 LWRP since the proposed updated LWRP is currently being reviewed by the New York Department of State (“NYS DOS”), resulting in the determination of inconsistent; and

WHEREAS, the Waterfront Commission’s January 4, 2016 determination included a number of concerns regarding potential development at the subject site; and

WHEREAS, the LWRP, under Section 5.3, provides the management structure necessary to implement the LWRP, including the ability for Village agencies to issue their own findings; and

WHEREAS, the LWRP, under Section 5.3.C contains four (4) conditions that must be met for a Village agency in determining if a project or action should proceed; and

WHEREAS, the Village of Port Chester Department of Planning and Economic Development evaluated said petition regarding each of the four (4) conditions, as presented in a February 26, 2016 memorandum from the Director of Planning and Economic Development to the Board of Trustees; and

WHEREAS, the February 26, 2016 memorandum further recommended that the Board of Trustees prepare and issue findings on said petition, and

WHEREAS, the February 26, 2016 memorandum further stated that the Waterfront Commission’s concerns noted in its January 4, 2016 determination were evaluated as part of the New York State Environmental Quality Review Act (“SEQRA”) process and recommended that the Waterfront Commission’s concerns be addressed as part of the Village’s site plan approval process.

NOW, THEREFORE, be it

RESOLVED, that the Board of Trustees of the Village of Port Chester finds that said petition fits the threshold of each of the four (4) conditions as follows:

- **Condition 1** – *No reasonable alternatives exist which would permit the action to be undertaken in a manner which will not substantially hinder the achievement of such LWRP policy standards and conditions.*

Development of any type will incur impacts to local resources, and any construction at this site will utilize public infrastructure. This includes land uses that are currently allowed in the MUR District’s existing zoning regulations, as well as the uses possible as part of the proposed zoning district, notably residential.

The “no-action” alternative would be to leave the project site vacant. However, doing so will directly hinder the LWRP’s policy objectives, which state a need to redevelop land in the Downtown Subarea. Therefore, such alternative would hinder the achievement of the LWRP’s standards and conditions and is considered not reasonable.

A second alternative would be to develop the project site under the existing MUR District regulations. Under such zoning, the Applicant had proposed a multi-story building for retail

and commercial use. These uses would increase traffic generation and parking demand more than the mixed-use option currently proposed, resulting in additional traffic and air quality impacts. Therefore, such alternative would hinder the achievement of the LWRP's standards and conditions and is considered not reasonable.

The proposed petition is consistent with Condition 1 that no reasonable alternatives exist that would not substantially hinder the achievement of LWRP policy standards and conditions.

- **Condition 2** – *The action would be undertaken in a manner which will minimize all adverse effects on such LWRP policy standards and conditions to the maximum extent practicable.*

The proposed action is consistent with Condition 2 in that it would be undertaken in a manner that would minimize adverse impacts to the maximum extent practicable. The proposed action, a zoning change, will not incur adverse impacts in and of itself. Rather, it is the forthcoming site specific plan that will and should be evaluated to ensure minimal adverse effects on LWRP policy standards and conditions. This will take place during the Village's site plan approval process to the maximum extent practicable.

- **Condition 3** – *The action will advance one or more of the other coastal policies.*

The proposed action is consistent with Condition 3 as it will advance two additional coastal policies:

Policy 1: Restore, revitalize and redevelop deteriorated and underutilized waterfront areas for commercial, industrial, cultural, recreational and other compatible uses.

Policy 1D: Redevelop Port Chester owned land in Subarea IV, the Downtown Subarea for commercial, recreational, residential and other compatible uses.

- **Condition 4** – *The action will result in an overriding Village, regional or State-wide public benefit.*

Said action is consistent with Condition 4 since it will result in an overriding Village public benefit to develop its downtown area and waterfront in a smart growth, transit-oriented fashion. Said action is within walking distance of the Port Chester train station and Main Street and will help to bring new residents and foot-traffic to the downtown area. These new residents and shoppers will provide significant economic benefits to the Village as patrons to local stores and restaurants. Inclusion of retail uses on the first floor will also provide economic benefits by adding to the Village's non-residential tax base; and be it further

RESOLVED, that, based on the foregoing, the Board, as the Lead Agency, finds that said petition shall be permitted to proceed.

Approved as to Form:

Village Attorney
Anthony Cerreto

ROLL CALL

AYES:

NOES:

ABSENT:

DATE: March 7, 2016

SEQRA NEGATIVE DECLARATION

**STATE ENVIRONMENTAL QUALITY REVIEW ACT
NEGATIVE DECLARATION
NOTICE OF DETERMINATION OF NO SIGNIFICANCE**

**Village Board of the Village of Port Chester
Zoning Petition and Mixed-Use Building – Retail D**

March 7, 2016

This notice is issued pursuant to Part 617 of the regulations implementing the State Environmental Quality Review Act (“SEQRA”), Article 8 of the Environmental Conservation Law. The Board of Trustees of the Village of Port Chester (the “Board of Trustees”), as lead agency, has determined that the Proposed Action described below will not have a significant effect on the environment.

TITLE AND LOCATION OF PROPOSED ACTION:

Title: Modified Marina Redevelopment Project Retail D

Location: The parcels known as Section 142.031, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23, and 24 of the Town of Rye Official Tax Map and interstitial Liberty Street Right-of-Way, known as “Retail D,” which consists of approximately 23,138 square feet (the “Property”).

SEQRA STATUS: Unlisted Action

DESCRIPTION OF ACTION:

The Proposed Action is comprised of: (a) Amendments to (i) the Urban Renewal Plan for the Modified Marina Redevelopment Project (“MMRP”), (ii) the regulations of the MUR Marina Redevelopment Project Urban Renewal District of the Village of Port Chester, and (iii) the Concept Development Plan of the MMRP to permit residential use of the Property, and (iv) the MMRP Urban Renewal Plan and MUR District regulations to add a multifamily dwelling parking regulation to the required parking table, all of which are reflected in an Amended Petition (collectively, the “Proposed Zoning Amendments”), and (b) to develop mixed-use residential-retail development of the Property in a 5-story, 70-foot high elevator-serviced building to contain 12,000 square feet of ground-floor retail space and 60,000 square feet of residential space (79 residential dwelling units, primarily studios and one-bedroom units), which includes a density bonus provision for additional floor area.

The Applicant is G&S Port Chester LLC.

REASONS SUPPORTING THE DETERMINATION OF NO SIGNIFICANT ENVIRONMENTAL IMPACT:

The Proposed Action will not result in any significant adverse impact on the environment. In reaching this determination the Board of Trustees has considered relevant areas of environmental concern and the criteria identified in 6 N.Y.C.R.R. § 617.7.

The Board of Trustee’s rationale with respect to particular areas of relevance for the elements of the Proposed Action follows:

A. LAND USE, ZONING AND PUBLIC POLICY

The Proposed Action incorporates recommendation made by the Village Planning Commission with regard to “as-of-right” bulk requirements and the inclusion of a bonus density for additional floor area similar to those contained in Section 345-16(4) of the Village Zoning Code available in C2, C5 and C5T zoning districts. The Proposed Action limits building height to 5 stories/70 feet, and maximum floor area ratio (“FAR”) of 3.7, minimum lot area per dwelling unit of 240 square feet, no usable space per residential use, and the density bonus noted above.

The Proposed Action is consistent with the Village Comprehensive Plan adopted in 2012, That Plan includes the Property in the Higher Intensity Planning Zone Category and provides a land use strategy for mixed-use development consisting of ground-floor retail and upper-floor residential with a maximum height of five stories. (See Comprehensive Plan at page 140). The proposed mixed-use is consistent with planning concepts that combine retail and residential components in proximity in downtowns. In addition, with the proximity to the Metro-North Railroad Station in the Village, the proposed development will be transit-oriented and reduce the use of automobiles by residents – particularly during weekday morning and evening rush hours.

The Proposed Action was found by the Village Waterfront Commission to be inconsistent with the Village’s current Local Waterfront Revitalization Plan (“LWRP”), which was adopted in 1992 and approved amended in 1999. The LWRP was approved by the New York State Department of State (“NYS DOS”) pursuant to the Coastal Zone Management Act. The Village is currently in the process of updating the LWRP, with a draft having been submitted to and pending approval by the NYSDOS.

However, pursuant to Section 5.3.c of the adopted LWRP, the Board has the ability to adopt findings that the Proposed Action proceed, should it meet four conditions. The Board found, by a resolution dated February 16, 2016, that these conditions were satisfied and that Proposed Action could proceed. The Port Chester Coastal Commission commented on the Proposed Action in its January 4, 2016 comments on the Applicant’s LWRP Coastal Assessment Form (“Waterfront Commission Comments”); these comments are addressed below.

Therefore, the Proposed Action will not have any significant adverse impacts with respect to land use, zoning and public policy.

B. TRAFFIC

The Proposed Action will generate less traffic than the entirely retail use approved as part of the MMRP in 1999. However, at the request of the Village, made to confirm the absence of material changes in circumstances since the MMRP was approved in 1999, the Applicant conducted limited traffic counts in several intersections in the vicinity of the Property, and also utilized traffic data and a traffic analysis prepared by Atlantic Traffic & Design Engineers, Inc. (“Atlantic T&D”) in 2014, using traffic data collected for Costco Wholesale. Atlantic T&D had collected volume data in December 2011 for the weekday midday, weekday PM and Saturday midday periods, but not the AM peak period, at the following intersections:

- Westchester Avenue with Main Street (Route 1)/King Street;
- Westchester Avenue with Abendroth Avenue/Don Bosco Place; and
- Westchester Avenue with Traverse Avenue.

The Applicant’s traffic engineer collected vehicle turning movement data at the three intersections identified above for the weekday AM peak period on February 8, 2016 and then calculated AM Peak Hour volumes. The Applicant’s traffic engineer grew the traffic volume data collected by Atlantic T&D for the weekday PM and Saturday midday periods to 2017 using an annual growth rate of two (2) percent. The Applicant’s traffic engineer then conducted capacity analyses for the three intersections using the 2017 traffic volumes for the weekday AM Peak Hour, the weekday PM Peak Hour and Saturday midday Peak Hour.

The capacity analyses used accepted methodologies based on “Level of Service” comparisons. The analyses show that traffic impacts will be nominal and there will be no deterioration of Levels of Service at the intersections referenced above as the result of the Proposed Action.

The Waterfront Commission Comments raised a concern about impacts to traffic from the loading and unloading of passengers and the arrival and backing up of moving vans. To the extent that the first concern relates to the operation (and not construction) of the Property, any such these stops would be very short and will not materially affect traffic. The effect of moving vans would be sporadic, and of temporary duration and not considered to be significant. Similarly, the potential traffic interruption of delivery trucks backing into the Property would be sporadic and temporary. The details of site design will be addressed during site plan review.

The Applicant’s traffic study was reviewed and approved by the Village’s traffic consultants, Adler Consulting.

Therefore, the Proposed Action will not have any significant adverse impacts with respect to traffic.

C. PARKING

The Applicant owns the adjacent parking structure, and under the Land Acquisition and Development Agreement with the Village, can generally control parking within that structure and limit it to parking associated with tenants of the MMRP. However, historically, the Applicant has allowed public parking in that structure, and commuter parking for Metro-North patrons is available on the second floor deck. Consequently, the Village requested the Applicant to study the impacts, if any, of providing for parking from the proposed mixed-use development in the parking structure – although the proposed mixed-use development will require less parking than the 1999-approved retail use – to confirm the absence of material changes in circumstances since the MMRP was approved in 1999.

The Applicant's planner generated a parking compliance summary, which reflects the number of parking spaces required by the proposed mixed-use development during peak periods of parking demand, as well as the number of spaces required by the overall MMRP. The proposed mixed-use development will generate a maximum demand for 167 parking spaces, which will occur during the weekday PM peak. Given the extensive retail uses now in the area, a significant portion of the assumed traffic generated by the retail use (88 at peak hour) will likely be bypass traffic (i.e., traffic that would be in the area without the Proposed Action). According to the Applicant's parking compliance summary, at that peak, after accounting for a shared parking credit and minimum parking requirements from the MMRP approvals, and taking account for existing parking demand and future commuter parking demand, there is an excess of approximately 173 parking spaces. This would address the parking demand of the Proposed Action.

To ensure the additional capacity assumed by the Applicant's parking compliance summary, the Village requested, that the Applicant conduct a parking accumulation study of the second floor deck of the parking structure during the peak weekday and Saturday hours. The second floor deck is where excess parking capacity is most likely to exist, based on personal observations of the Applicant and a Board member.

The Applicant's traffic engineer collected parking data on the middle level of the parking deck, which includes approximately 390 parking spaces. The Applicant's traffic engineer conducted an inventory to confirm the number of parking spaces available. Approximately 350 parking spaces are currently used by Metro-North Railroad. The parking accumulation data was collected every 30 minutes on February 6, 2015, a typical weekday, between 6:00 p.m. and 10:00 p.m. and on February 8, 2015, a typical Saturday, between 11:00 a.m. and 3:00 p.m.

This study determined that the second floor deck has excess capacity during the weekday PM, weekday evening and Saturday PM peak period sufficient to address the additional parking demand of 127 spaces associated with the Proposed Action. It is also noted that the adjacent Marina Parking Lot is most highly utilized overnight and on weekends, with less utilization during weekday periods. The Applicant has agreed to adopt measures to assure that its tenants/residents of the Property must park their vehicles in the middle level of the parking deck, thus providing sufficient capacity to accommodate the parking demand generated by

the Proposed Action. These measures will include parking restrictions (the “Tenant Parking Restrictions”) in the leases for its residential tenants, or comparable provisions (approvable by the Village Manager or his/her designee), that the Applicant would commit to enforce, which prohibit the residential tenants from parking in the Marina Parking Lot overnight or for longer than four (4) hours. Such Tenant Parking Restrictions to remain in effect until the earlier of: (a) the effective date that the current Marina Parking Lot restrictions are modified or amended (e.g. conversion from free parking to a pay lot), or (b) such time as the Applicant can demonstrate to the Village that the Marina Parking Lot has sufficient parking capacity for the residential tenants associated with the Proposed Action to park in the Marina Parking Lot without an adverse parking impact. The Applicant has agreed, if necessary, to modify any arrangement that it has with Metro-North Railroad to assure the availability of sufficient parking spaces in the second floor of the parking deck to accommodate the residents/tenants of the Proposed Action as described above. The Village would also have the authority to enforce these measures through one or more mechanisms that would be adopted as part of site plan review. It is further noted that in the event there is an unanticipated demand for parking, the parking study showed that there are additional spaces available on the main level for residents/tenants of the Proposed Action or members of the public that currently use the marina parking lot.

The parking study was reviewed and approved by the Village’s traffic consultants, Adler Consulting.

The study conducted by the Applicant and the condition noted above addresses the Waterfront Commission Comments on parking.

Therefore, the Proposed Action will not have any significant adverse impacts with respect to parking.

D. PEDESTRIANS

The Proposed Action will generate some additional pedestrian traffic, primarily by customers of the retail use – although it is expected that many of these customers would be in the downtown area without the proposed retail store. It is estimated that the Proposed Action will generate approximately 75 additional pedestrians in the midday weekday peak, which is expected to be the highest pedestrian usage given the concentration of commercial uses in the area. This additional number of pedestrians is not expected to significantly impact existing conditions or cause a safety problem, as it will result in approximately one extra pedestrian per minute. The Waterfront Commission Comments raised the issue of pedestrian safety which, as noted above, has been addressed.

Therefore, the Proposed Action will not have any significant adverse impacts with respect to pedestrians.

E. DEMOGRAPHICS

The proposed mix-use development will result in the addition of 141 new residents to the Village, which represents a negligible increase on 0.5% increase in the Village population.

Therefore, the Proposed Action will not have any adverse impacts with respect to demographics.

F. SCHOOL CHILDREN

The Applicant provided an estimate of public school children to be generated by the Proposed Action, using both the well-accepted Rutgers method, as well as the results of the “Port Chester Public Schools Overcrowding and Mitigation Analysis,” prepared for the Village of Port Chester IDA by Urbanomics, Inc. (November 10, 2014) (“IDA Mitigation Study”). Using either the Rutgers methodology or that in the IDA report, the Proposed Action is projected to generate seven school children.

The IDA study indicated that a “new construction cost” to address the facility and infrastructure needs of the Port Chester-Rye Union Free School District in \$18,370, whereas the annual educational costs per student in \$13,723 (actual cost minus school aid). The Applicant has asserted that a combination of PILOT payments and/or real estate taxes generated from the Proposed Action once it is put into use will more than offset any incremental variable cost associated with the projected addition of seven school children to the local public school system. The Applicant has agreed that if the increase of such revenues over the real estate taxes generated for the School District from the Property at the time the Proposed Action is placed into use do not generate an amount that would cover the annual cost per student of \$13,723 (as reported in the IDA Mitigation Study) for the actual number of students generated each school year, increased annually from January 1, 2015 by the increase, if any, in the cost of living index for the metropolitan New York region, the Applicant will pay the School District any deficit within thirty (30) days after being informed by the School District of such deficiency. The Applicant has agreed that this obligation shall continue for a period of ten (10) years. Based on this concurrence, any potential impacts to the public school should be minimal.

The Proposed Action will not affect other community services, as it is located in the already well-served downtown area of the Village.

Therefore, the Proposed Action will not have any significant adverse impacts with respect to community services.

G. INFRASTRUCTURE

The Applicant estimated that water demand, based on a standard multiplier, will have an increased demand of 4,330 gallons per day (“gpd”), an increase of water demand associated with the formerly approved retail use of 8,160 gpd. It is expected that there is adequate capacity to accommodate this nominal increase in demand.

The Applicant estimated that the sanitary sewage will be the same as water demand. The Westchester County's Wastewater Treatment Plant in Port Chester has adequate capacity to handle this small additional volume of sanitary sewage. See Westchester County 2013 Annual Report, Water and Wastewater Operations, Solid Waste Operations, reflecting that the 2013 actual flow of 4.3 million gallons per day ("mgd"), as compared to the design flow is 6.0 mgd,

The Waterfront Commission Comments raised the issue of the long-term viability of the Buckley Drain as it transverses the Property. Although this may be a valid Village concern, the Proposed Action would not adversely impact the Drain on its own. The Comments also expressed a concern about flooding as the elevation of the entrance is below the 100-year floodplain. The details of the entrance will be addressed during site plan review, but the Applicant will need to comply with applicable laws regarding the floodplain.

Therefore, the Proposed Action will not have any adverse impacts with respect to water and sanitary sewage.

H. HAZARDOUS MATERIALS

There were several reported spills of petroleum reported to the New York State Department of Environmental Conservation ("NYSDEC") on the Property, both of which have been closed (Spill Number 9001927, closed on 06/27/1990; and Spill Number 9515849, closed on 04/05/1999. In addition, there was a release of chlorinated solvents on the Property, which was given a No Further Action letter by NYSDEC on April 30, 2001.

There is an open petroleum spill across Abendroth Avenue; however, the groundwater flows generally toward the Byram River, away from the Property.

The Waterfront Commission Comments raised the issue of hazardous substances but, as noted above, this subject has been addressed.

Therefore, the Proposed Action will not have any adverse impacts with respect to hazardous materials.

I. OPEN SPACE

The Proposed Action will provide new interior roof space, on the roof of the first floor, within the "U" configuration of the upper four stories. This larger, contiguous area of space will be available to residents of the building. The Applicant anticipates providing an array of amenities to make this space attractive. Therefore, with this provision of open space, the Proposed Action will not have any adverse impacts with respect to open space.

J. DRAINAGE AND STORMWATER

The Proposed Action will not result in an increase of impervious coverage of the Property, as the site is now impervious. Post-development stormwater runoff will be directed to the Village storm sewer system. Therefore, the Proposed Action will not have any significant adverse impacts with respect to post-construction drainage and stormwater.

K. VISUAL RESOURCES

In terms of visual appearance, the Proposed Action will be an improvement over the existing vacant sight. The five-story 70-foot building will be higher than many of the surrounding buildings but will be slightly greater than the 60-foot maximum height now permitted in the C2 Main Street Business and C5T Downtown Mixed Use Transitional Districts (exclusive of the 10-foot bonus allowed in the latter district). Thus, the new building will not be discordant with its surroundings.

Therefore, the Proposed Action will not have any significant adverse impacts with respect to visual resources.

L. CONSTRUCTION

Construction of the Retail D building is expected to take approximately 18 months. Construction activities will require the use of heavy equipment during some, but not all, phases of construction. Construction activities in general will take place between 7:00 am and 4:00 pm on weekdays, and 7:00 am and 3:00 pm on Saturdays. As there are no nearby residential dwellings, noise should not present an issue. Therefore, the Proposed Action will not have any significant adverse noise impacts.

The Applicant will utilize Best Management Construction Practices during construction, which would include practices to manage sedimentation and erosion, and will comply with all applicable state and local laws. This plan will mitigate any increased erosion or sedimentation that might otherwise be caused by construction. To the extent there are tidal wetlands within 500 feet of the Property, as noted in the Waterfront Commission Comments, the use of Best Management Construction Practices would avoid adverse impacts to these areas.

The Waterfront Commission Comments also expressed concern about construction staging, delivery of material to the Property, lane closures and sidewalk closures during construction. The construction will not be staged, but will be a continuous process. The delivery of material to the Property will generally occur during the early morning hours, prior to the AM peak, so it should not have significant adverse impacts on traffic. Lane closures are not anticipated, and sidewalk closings, if necessary, would not present material impacts. Notably, all construction-related impacts are temporary in nature.

Post-development stormwater runoff will be directed to the Village storm sewer system.

The Proposed Action therefore will not have any adverse impacts with respect to soils.

L. SUMMARY

For the foregoing reasons, as well as those set forth in the various submissions and analyses conducted by the Village Staff and the Village's independent consultants, the Proposed Action will not have any significant, adverse effect on the environment. The principal submissions and analyses in the record are listed on Appendix A hereto.

FOR FURTHER INFORMATION

Contact Person:

Eric Zamft, AICP, Director

Village of Port Chester Department of Planning & Economic Development

222 Grace Church Street

Port Chester, NY 105737

(914) 481-8037

APPENDIX A

Letter from Daniel D. Tartaglia, Esq. to Hon. Dennis Pilla re: G&S Port Chester LLC – Parcel D Amended Rezoning Petition Tax Map Designation: Section 142.031, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23 and 24	October 1, 2015
Memo/Letter from Village of Port Chester Department of Planning and Development to Port Chester Planning Commission re: Amended Petition: “Retail D”/Coney’s Lot	October 23, 2015
Letter from Walter M. Dunn, Jr., P.E. of Dunn Engineering Associates, P.C. to Douglas Riley re: Traffic Engineering Letter Report Change of Zone Petition Mixed Use Building Village of Port Chester, NY	November 12, 2015
Modified Marina Redevelopment Project Retail D Impact Assessment Report, Prepared by: Cleary Consulting	January 2016
Letter from Walter M. Dunn, Jr., P.E. to Douglas Riley re: Traffic Engineering Comments Response to 1/5/16 Memo from Mark Chertok to Daniel Tartaglia	January 11, 2016
Letter from Walter M. Dunn, Jr., P.E. to Douglas Riley re: Traffic Engineering Approaches Change of Uses on Retail D	January 29, 2016
Letter from Walter M. Dunn, Jr., P.E. to Michael O’Rourke Of Adler Consulting re: Changes of Uses on Retail D: Traffic Engineering Examination	February 12, 2016
Memorandum from Daniel Tartaglia to Mark Chertok Re: G&S Parcel “D” Rezoning Petition-Supplemental Information to EAF, dated February 16, 2016	February 16, 2016
Letter from Bernard Adler, P.E. and Michael O’Rourke, P.E., PTO of Adler Consulting & Planning, PLLC to Eric Zamft, Director of Planning and Economic Development, Village of Port Chester re traffic and parking impacts	February 16, 2016
Letter from Walter M. Dunn, Jr., P.E. PLLC to Daniel Tartaglia, Esq. re: G&S Port Chester—Retail “D” Petition; Potential Pedestrian Impacts of Less Intensive Use	February 24, 2016

Letter from Bernard Adler, P.E. and Michael O'Rourke,
P.E., PTO of Adler Consulting & Planning, PLLC to Eric
Zamft, Director of Planning and Economic Development, Village of
Port Chester re pedestrian impacts

February 26, 2016

Retail D Mixed-Use Building – Impact Assessment Report

Undated

**BOARD OF TRUSTEES RESOLUTION:
SEQRA DETERMINATION OF SIGNIFICANCE**

RESOLUTION

ISSUE NEGATIVE DECLARATION UNDER STATE ENVIRONMENTAL QUALITY REVIEW ACT (SEQRA) WITH REGARD TO THE PETITION OF G&S

On motion of TRUSTEE _____ seconded by TRUSTEE _____, the following resolution was adopted by the Board of Trustees of the Village of Port Chester, New York:

WHEREAS, G&S Port Chester LLC (the “Applicant”) owns the property parcels known as Section 142.031, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23, and 24 of the Town of Rye Official Tax Map and interstitial Liberty Street Right-of-Way, which consists of approximately 23,138 square feet (the “Property”); and

WHEREAS, on 1999, the Applicant received approval from the Board of Trustees (hereinafter referred to as the “Board”) to develop the Property for retail use as part of the Modified Marina Redevelopment Project (MMRP”) in 1999; and

WHEREAS, such approval allows use of the Property for 48,000 gross square feet of retail use (approximately 40,000 net square feet of such use); and

WHEREAS, on December 5, 2014, the Applicant submitted a Petition to the Board to amend the zoning map and text amendments to the Urban Renewal Plan for the MMRP, regulations to the MUR Marina Redevelopment Project Renewal District, and Concept Development Plan of the Modified Marina Redevelopment to permit multi-family residential development of the Property and to add a multifamily dwelling parking regulation for the required parking table; and

WHEREAS, on October 1, 2015, the Applicant submitted an Amended Petition to the Board for amendments to (i) the Urban Renewal Plan for the MMRP, (ii) the regulations of the MUR MMRP Urban Renewal District of the Village of Port Chester, and (iii) the Concept Development Plan of the MMRP to permit residential use of the Property, and to amend the MMRP Urban Renewal Plan and MUR District regulations to add a multifamily dwelling parking regulation to the required parking table (the “Proposed Rezoning”); and

WHEREAS, the Applicant specifically seeks amendments to the MUR Marina Redevelopment Project Urban Renewal District in order to develop the Property for a mixed residential and commercial use development, consisting of approximately 12,000 square feet of ground floor retail space and 60,000 square feet of residential space (79 residential dwelling units, primarily studios and one-bedroom units), which includes a density bonus provision for additional floor area (the Proposed Rezoning and proposed development are, collectively, the “Proposed Action”); and

WHEREAS, on October 5, 2015, the Board adopted a resolution accepting the Applicant’s Amended Petition for consideration; and

WHEREAS, the Board has found that the Proposed Action is an Unlisted Action pursuant to the State Environmental Quality Review Act (“SEQRA”) and the regulations at 6 NYCRR Part 617 and has noticed its intent to serve as Lead Agency for the purposes of a coordinated review; and

WHEREAS, the Village Clerk forwarded a copy of such notice of intent, Amended Petition and supporting materials to all designated involved and interested agencies; and

WHEREAS, all involved agencies have advised that they do not object to the Board’s designation as lead agency or more than 30 days have elapsed since notification of the Board’s notice of intent; and

WHEREAS, at a duly noticed meeting on December 15, 2015, the Board designated itself as lead agency pursuant to SEQRA at a duly noticed meeting; and

WHEREAS, the Board retained independent consultants to review the submissions made by the Applicant pursuant to SEQRA; and

WHEREAS, the Amended Petition submitted by the Applicant included a Long Form Environmental Assessment Form (“EAF”), together with supporting material, pursuant to SEQRA; and

WHEREAS; the Board held a public hearing on the Petition that commenced on November 2, 2015 and was closed on February 1, 2016; and

WHEREAS, in response to inquiries by the Board, its Staff and independent consultants, and the public, the Applicant submitted further information pursuant to SEQRA; and

WHEREAS, the Board Staff and its independent consultants recommended that the Board of Trustees issue a determination that the Proposed Action would not have a significant adverse environmental impact; and

WHEREAS, the Board has reviewed and carefully considered the EAF and additional submissions made by the Applicant, public comments, and the recommendations of its Staff and independent consultants. Now, therefore, be it

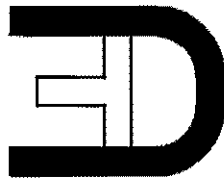
RESOLVED, that the Board of Trustees hereby determines that the Proposed Action will not have a significant adverse impact on the environment for the reasons set forth in the accompanying Negative Declaration, which is incorporated hereto and made a part hereof; and be it further

RESOLVED, that the Village Clerk is directed to undertake all filings and service of this Resolution pursuant to the regulations governing SEQRA at 6 NYCRR 617.12.

Approved as to form:

Anthony M. Cerreto, Village Attorney

2016 DUNN ENGINEERING TRAFFIC STUDY



Dunn Engineering Associates, P.C.

Consulting Engineers

66 Main Street

Westhampton Beach, N.Y. 11978

631-288-2480

631-288-2544 Fax

February 12, 2016

Mr. Michael O'Rourke, P.E.
Adler Consulting
235 Main Street
4th Floor, Suite 400
White Plains, New York 10601

Re: Change of Uses on Retail D
Traffic Engineering Examination

Dear Mr. O'Rourke:

We have reviewed and collected the data requested in your recent e-mail. (Attachment 1)

As a result, we collected and compiled the following information:

1. A.M. Weekday peak hour Traffic Counts from 7:00 A. to 9:00 A.M. at the intersections of Westchester Avenue at a) Main Street (US Route 1)/King Street, b) Abendroth Avenue/Don Bosco Place, and Traverse Avenue. (Attachment 2)
2. Saturday midday (11:00 A.M. to 3:00 P.M.) Parking Data on the middle level of the parking deck as well as the southerly portion of ground level. (Attachment 3)
3. Weekday P.M. Parking Data (6:00 P.M. to 10:00 P.M) on the idle level of the parking deck as well as the ground level. (Attachment 4)
4. Figure 1, Existing Volumes (2016), which consisted of the A.M. Traffic Counts presented in #1 above and the expansion of the 2011 P.M. and Saturday counts taken for Costco by a growth rate of 2% per year to 2016. Calculations for the annual growth rate expansion to 2016 for "existing volumes" and for 2017 No Build volumes are also included in Attachment 5. (Attachment 5)
5. Figure 2, No Build 2017. (Attachment 6)
6. Figure 3, Directional Distribution (Attachment 7) which presents the 1999 Directional Distribution and an Alternative Directional Distribution. Figure 3A presents the Directional Distribution with which you agreed.

Mr. Michael O'Rourke, P.E.
February 12, 2016
Page 2

7. Figure 4, Build Volumes (2017) which adds the site-generated traffic for the proposed change of uses. (Attachment 8). Figure 4B presents the assignment of site generated traffic based on Table 1, which contains the site generated traffic, and Figure 3A, Directional Distribution. A copy of Table 1 is included in Attachment 8 for ease of reference.
8. Capacity Analysis Summary Tables. (Attachment 9)
9. Capacity Analysis Analyses Results. (Attachment 10)

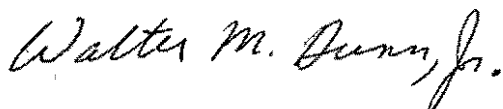
Conclusions

1. The capacity analyses performed indicated that the traffic generated by the proposed uses on Retail D of a 12,000 S.F. specialty retail center and 79 apartments can be accommodated at the study intersections of Westchester Avenue at a) Main Street (US Route 1), b) Abendroth Avenue/Don Bosco Place, and c) Traverse Avenue with very minor traffic impacts and without deterioration of the Levels of Service.
2. The Saturday (11:00 A.M. to 3:00 P.M.) and Weekday P.M. (6:00 P.M. to 10:00 P.M.) parking data collected on the middle level indicates that there is sufficient available parking to accommodate the proposed change of uses.
3. Furthermore, additional parking data collected for the same time periods on the ground level indicates that there are additional vacant spaces that can be assigned for use by the proposed specialty retail center and the residents of the apartments, if necessary. (See Attachments 3 and 4)

As a result, based on our traffic engineering analyses, it is recommended that the proposed change of uses on Retail D be approved.

If you have any questions or require additional information, please contact me.

Sincerely,



WALTER M. DUNN, JR., P.E.
President

WMD:as
L2016020

ATTACHMENT 1

"Retail D"

Traffic volume data were previously collected by Atlantic Traffic & Design in December, 2011 at the intersections of:

- Westchester Avenue with Main Street (Route 1)/King Street;
- Westchester Avenue with Abendroth Avenue/Don Bosco Place; and,
- Westchester Avenue with Traverse Avenue.

The data were collected only for the weekday midday, weekday PM and Saturday midday periods. No data were collected during the AM peak period.

The Applicant's traffic engineer will grow the traffic volume data collected by Atlantic Traffic & Design in December, 2011 for the weekday PM and Saturday midday periods to 2016 using an annual growth rate of two (2) percent. The Applicant's traffic engineer will arrange to collect vehicle turning movement data at the three intersections identified above for the weekday AM peak period and then calculate AM Peak Hour volumes. Capacity analyses will be performed for the three intersections using the 2016 traffic volumes for the weekday AM Peak Hour, the weekday PM Peak Hour and Saturday midday Peak Hour.

The Applicant's traffic engineer will arrange to collect parking data on the middle level of the parking deck which includes approximately 390 parking spaces. The Applicant's traffic engineer will conduct an inventory to confirm the number of parking spaces available. Approximately 350 parking spaces are currently used by Metro-North Railroad. Parking accumulation data will be collected every 30 minutes on a typical weekday between 6:00 p.m. and 10:00 p.m. and on a typical Saturday between 11:00 a.m. and 3:00 p.m.

The traffic and parking data and analyses submitted by the Applicant will be reviewed by the Village Traffic Consultant and discussed with the Special Counsel and Village Manager.

ATTACHMENT 2

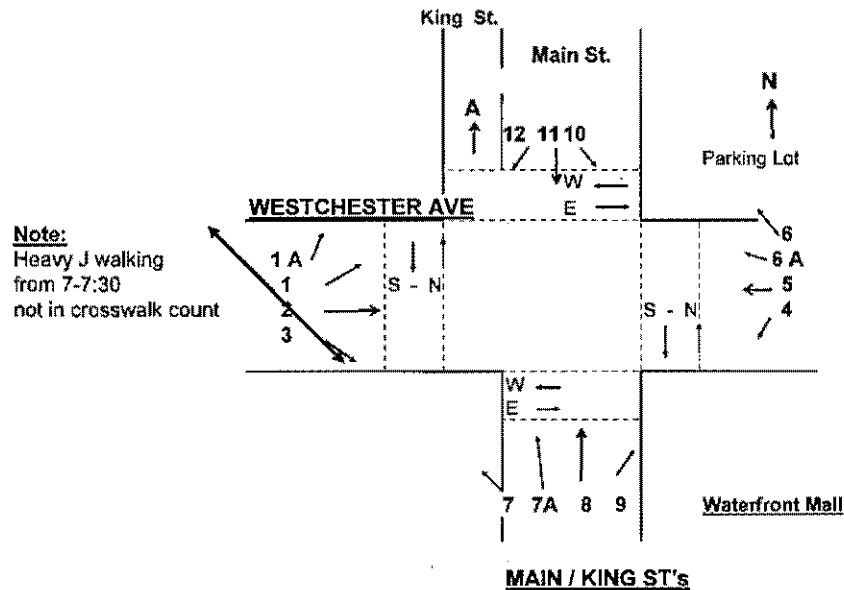
T.C.C., Inc. - TURNING MOVEMENT COUNT SURVEY

LOCATION: 1- WESTCHESTER AVE

@ MAIN / KING ST's

DATE: Monday - February 8, 2016

WESTCHESTER AVE											MAIN / KING ST's										
AM START TIME	E/B					W/B					N/B					SB					
	1A	1	2	3	TOTAL	4	5	5A	6	TOTAL	7	7A	8	9	TOTAL	10	11	12	TOTAL		
	H-LEFT	LEFT	THRU	RIGHT	TOTAL	LEFT	THRU	RIGHT	RIGHT	TOTAL	LEFT	L-TH	THRU	RIGHT	TOTAL	LEFT	THRU	RIGHT	TOTAL		
7:00	4	15	15	29	63	13	9	2	2	26	25	4	23	6	58	8	21	10	0	39	
7:15	3	18	46	25	92	17	25	4	6	52	31	2	44	4	81	16	42	20	0	78	
7:30	5	24	32	35	96	11	17	6	7	41	34	8	46	7	95	12	57	18	0	87	
7:45	4	21	46	46	117	12	43	0	8	63	49	16	47	12	124	16	52	18	0	86	
8:00	5	23	60	64	152	13	22	3	9	47	43	6	50	12	111	15	46	17	0	77	
8:15	9	15	54	58	136	15	23	2	6	46	37	15	64	10	126	10	54	22	0	86	
8:30	14	14	48	40	116	15	23	3	9	50	30	9	75	9	123	8	43	20	0	71	
8:45	4	31	33	40	108	6	26	4	9	45	31	9	89	18	147	9	54	22	0	85	
AM HOURLY																					
TOTAL VOLUME	1A	1	2	3	TOTAL	4	5	5A	6	TOTAL	7	7A	8	9	TOTAL	10	11	12	TOTAL		
1196	7:00	16	78	139	135	368	53	94	12	23	182	139	30	160	29	359	52	172	66	0	290
1399	7:15	17	86	184	170	457	53	107	13	30	203	157	32	187	35	411	59	196	73	0	328
1496	7:30	23	83	192	203	501	51	105	11	30	197	163	45	207	41	456	53	209	75	0	336
1631	7:45	32	73	205	208	521	55	111	8	32	206	159	46	236	43	484	49	194	77	0	320
1626	8:00	32	83	195	202	512	49	94	12	33	188	141	39	278	49	507	42	195	81	0	319
1631	Total Intersection Peak Hour Volume	Start of Peak Hr. E/B Approach Volume				07:45 AM	W/B Approach Volume				206	N/B Approach Volume				484	SB Approach Volume				320
		P.H.F.				0.86	P.H.F.				0.52	P.H.F.				0.96	P.H.F.				0.93

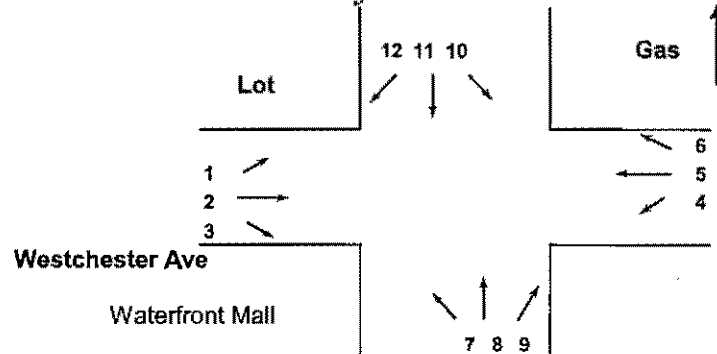


Traffic Counts Collecting, Inc.

Turning Movement Count Survey

INTERSECTION 2 - Westchester Ave
 @
Don Bosco PI / Abendroth

DATE: Monday - February 8, 2016



Westchester Ave

Don Bosco PI / Abendroth

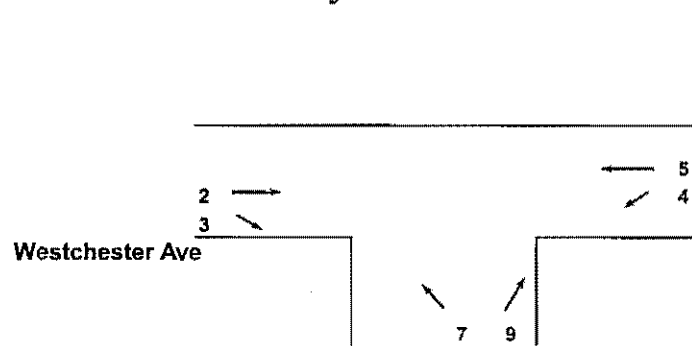
	<u>Westchester Ave</u>									<u>Don Bosco PI / Abendroth</u>										
	AM				E/B				W/B				N/B				S/B			
	START TIME	1 LEFT	2 THRU	3 RIGHT	TOTAL	4 LEFT	5 THRU	6 RIGHT	TOTAL	7 LEFT	8 THRU	9 RIGHT	TOTAL	10 LEFT	11 THRU	12 RIGHT	TOTAL			
	7:00	14	6	10	30	4	5	3	12	10	19	4	33	2	22	17	41			
	7:15	11	6	46	63	0	7	3	10	17	21	3	41	10	23	29	62			
	7:30	19	4	31	54	2	7	4	13	16	31	2	49	11	38	24	73			
	7:45	27	9	30	66	3	7	5	15	30	32	8	70	17	38	27	82			
	8:00	16	14	53	83	2	5	9	16	17	30	3	50	11	27	23	61			
	8:15	18	9	51	78	1	5	10	16	21	41	5	67	13	29	20	62			
	8:30	25	9	32	66	5	6	7	18	27	54	1	82	10	25	22	57			
	8:45	25	6	26	57	4	5	4	13	21	52	4	77	6	28	19	53			
TOTAL	AM HOURLY VOLUME																			
714	7:00	71	25	117	213	9	26	15	50	73	103	17	193	40	121	97	258			
808	7:15	73	33	160	266	7	26	21	54	80	114	16	210	49	126	103	278			
855	7:30	80	36	165	281	8	24	28	60	84	134	18	236	52	132	94	278			
889	7:45	86	41	166	293	11	23	31	65	95	157	17	269	51	119	92	262			
856	8:00	84	38	162	284	12	21	30	63	86	177	13	276	40	109	84	233			
889	Total Intersection Peak Hour Volume 889	Start of Peak Hr. E/B Approach Volume 07:45 AM			293	W/B Approach Volume			65	N/B Approach Volume			269	S/B Approach Volume			262			
		P.H.F.			0.88	P.H.F.			0.90	P.H.F.			0.82	P.H.F.			0.80			

1-3	4-6	7-9	10-12
0.81	0.83	0.69	0.79
0.80	0.84	0.75	0.85
0.85	0.94	0.84	0.85
0.88	0.50	0.82	0.89
0.88	0.88	0.84	0.94
0.64	0.65	0.69	0.69
0.47	0.43	0.48	0.48
0.25	0.25	0.25	0.25

Traffic Counts Collecting, Inc. Turning Movement Count Survey

INTERSECTION 3 - Westchester Ave
@
Traverse Ave

DATE: Monday - February 8, 2016



Westchester Ave

Traverse Ave

	AM START	E/B			W/B			N/B									
		2	3	TOTAL	4	5	TOTAL	7	9	TOTAL							
	TIME	THRU	RIGHT	TOTAL	LEFT	THRU	TOTAL	LEFT	RIGHT	TOTAL							
	7:00	2	7	9	0	0	0	9	1	10							
	7:15	1	11	12	0	0	0	7	0	7							
	7:30	1	19	20	0	2	2	9	0	9							
	7:45	5	26	31	0	2	2	12	1	13							
	8:00	3	26	29	0	2	2	22	0	22							
	8:15	6	20	26	0	1	1	13	0	13							
	8:30	4	15	19	0	6	6	10	0	10							
	8:45	7	4	11	1	1	2	4	1	5							
TOTAL	AM HOURLY VOLUME	1	2	3	TOTAL	4	5	6	TOTAL	7	8	9	TOTAL	10	11	12	TOTAL
115	7:00	0	9	63	72	0	4	0	4	37	0	2	39	0	0	0	0
149	7:15	0	10	82	92	0	6	0	6	50	0	1	51	0	0	0	0
170	7:30	0	15	91	106	0	7	0	7	56	0	1	57	0	0	0	0
174	7:45	0	18	87	105	0	11	0	11	57	0	1	58	0	0	0	0
146	8:00	0	20	65	85	1	10	0	11	49	0	1	50	0	0	0	0
174	Total Intersection Peak Hour Volume 174	Start of Peak Hr. E/B Approach Volume 07:45 AM 105			W/B Approach Volume 11			N/B Approach Volume 58			S/B Approach Volume 0						
		P.H.F. 0.85			P.H.F. 0.46			P.H.F. 0.66			P.H.F. #####						

1-3	4-6	7-9	10-12
0.59	0.50	0.75	#DN/0!
0.74	0.75	0.58	#DN/0!
0.85	0.86	0.65	#DN/0!
0.85	0.46	0.60	#DN/0!
0.73	0.46	0.57	#DN/0!
0.64	0.38	0.54	#DN/0!
0.39	0.33	0.38	#DN/0!
0.25	0.25	0.25	#DN/0!

ATTACHMENT 3

T.C.C., Inc.

PORT CHESTER PARKING SURVEY

1st FI - Spaces South of Ctr Drwy - East Structure

2nd FI - MTA - Space in Blue w/Space #

2nd FI - 40 +/- Space South side of Lot (50 space)

DATE: Saturday - February 6, 2016

SUNNY

TIME	1 st FL	2 nd # MTA	2 nd # No#													TIME
11:00	25	42	1													11:00
11:30	27	47	2													11:30
12:00	30	50	2													12:00
12:30	29	52	2													12:30
13:00	38	54	2													13:00
13:30	45	53	2													13:30
14:00	53	67	2													14:00
14:30	58	67	2													14:30
15:00	77	68	2													15:00

ATTACHMENT 4

T.C.C., Inc.

PORT CHESTER PARKING SURVEY

1st Fl - Spaces South of Ctr Drwy - East Structure
 2nd Fl - MTA - Space in Blue w/Space #
 2nd Fl - 40 +/- Space South side of Lot

DATE: Monday - February 8, 2016

Snow Showers

TIME	1 Fl.	MTA ^{Blue} 2 Fl.	2 Fl.															TIME
18:00	30	256	15															18:00
18:30	26	186	13															18:30
19:00	21	158	10															19:00
19:30	18	106	7															19:30
20:00	17	88	5															20:00
20:30	13	85	5															20:30
21:00	14	84	5															21:00
21:30	12	71	4															21:30
22:00	12	68	4															22:00

ATTACHMENT 5

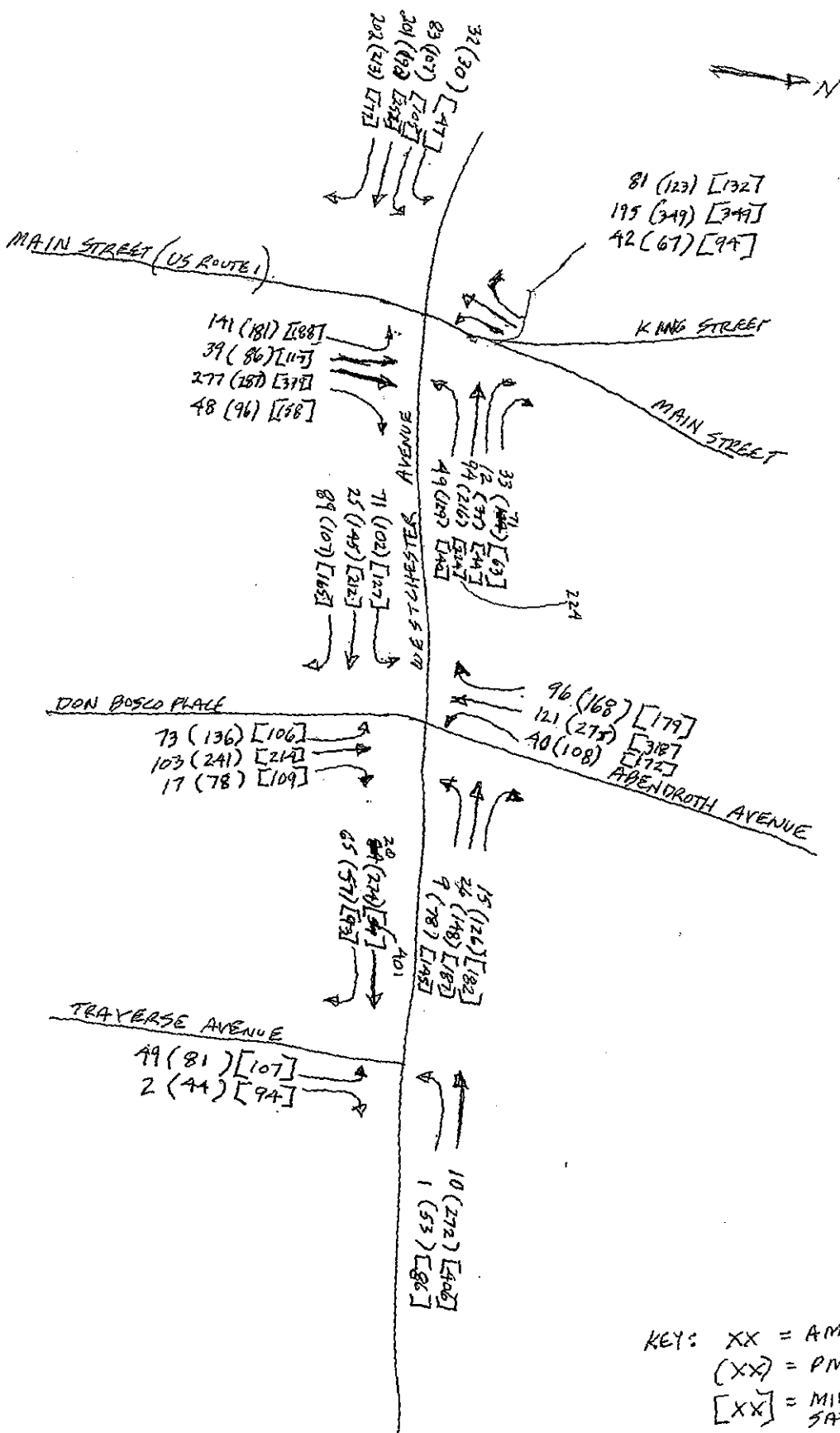


FIGURE 1
EXISTING VOLUMES (2016)

KEY: XX = AM 1
 (XX) = PM 2
 [XX] = MIDDAY SATURDAY 2

1 = ACTUAL COUNT
 2 = 2% ANNUAL GROWTH FROM 2011 TO 2016

2016 Existing - Westchester Ave @ Main St / King St							2017 No Build	
PM		2011	2012	2013	2014	2015	2016	2017
Eastbound	Lft-Lft	27	28	28	29	29	30	30
	Lft	97	99	101	103	105	107	109
	Str	172	175	179	183	186	190	194
	Rht	193	197	201	205	209	213	217
Westbound	Lft	117	119	122	124	127	129	132
	Str	196	200	204	208	212	216	221
	Rht	32	33	33	34	35	35	36
	Rt-Rt	64	65	67	68	69	71	72
Southbound	Lft	61	62	63	65	66	67	69
	Str	316	322	329	335	342	349	356
	Rht	111	113	115	118	120	123	125
Northbound	Lft	164	167	171	174	178	181	185
	Str-Lt	78	80	81	83	84	86	88
	Str	260	265	271	276	281	287	293
	Rht	87	89	91	92	94	96	98

2016 Existing - Westchester Ave @ Main St / King St							2017 No Build	
SAT		2011	2012	2013	2014	2015	2016	2017
Eastbound	Lft-Lft	43	44	45	46	47	47	48
	Lft	95	97	99	101	103	105	107
	Str	228	233	237	242	247	252	257
	Rht	160	163	166	170	173	177	180
Westbound	Lft	127	130	132	135	137	140	143
	Str	203	207	211	215	220	224	229
	Rht	40	41	42	42	43	44	45
	Rt-Rt	57	58	59	60	62	63	64
Southbound	Lft	85	87	88	90	92	94	96
	Str	316	322	329	335	342	349	356
	Rht	120	122	125	127	130	132	135
Northbound	Lft	170	173	177	180	184	188	191
	Str-Lt	106	108	110	112	115	117	119
	Str	307	313	319	326	332	339	346
	Rht	143	146	149	152	155	158	161

2016 Existing - Westchester Ave @ Abendroth Ave / Don Bosco Place							2017 No Build	
PM		2011	2012	2013	2014	2015	2016	2017
Eastbound	Lft	92	94	96	98	100	102	104
	Str	131	134	136	139	142	145	148
	Rht	97	99	101	103	105	107	109
Westbound	Lft	71	72	74	75	77	78	80
	Str	134	137	139	142	145	148	151
	Rht	114	116	119	121	123	126	128
Southbound	Lft	98	100	102	104	106	108	110
	Str	249	254	259	264	270	275	280
	Rht	152	155	158	161	165	168	171
Northbound	Lft	123	125	128	131	133	136	139
	Str	218	222	227	231	236	241	246
	Rht	71	72	74	75	77	78	80

2016 Existing - Westchester Ave @ Abendroth Ave / Don Bosco Place							2017 No Build	
SAT		2011	2012	2013	2014	2015	2016	2017
Eastbound	Lft	115	117	120	122	124	127	130
	Str	192	196	200	204	208	212	216
	Rht	149	152	155	158	161	165	168
Westbound	Lft	131	134	136	139	142	145	148
	Str	169	172	176	179	183	187	190
	Rht	165	168	172	175	179	182	186
Southbound	Lft	156	159	162	166	169	172	176
	Str	288	294	300	306	312	318	324
	Rht	162	165	169	172	175	179	182
Northbound	Lft	96	98	100	102	104	106	108
	Str	194	198	202	206	210	214	218
	Rht	99	101	103	105	107	109	111

2016 Existing - Westchester Ave @ Traverse Ave								2017 No Build
PM		2011	2012	2013	2014	2015	2016	2017
Eastbound	Str	248	253	258	263	268	274	279
	Rht	52	53	54	55	56	57	59
Westbound	Lft	48	49	50	51	52	53	54
	Str	246	251	256	261	266	272	277
Northbound	Lft	73	74	76	77	79	81	82
	Rht	40	41	42	42	43	44	45

2016 Existing - Westchester Ave @ Traverse Ave								2017 No Build
SAT		2011	2012	2013	2014	2015	2016	2017
Eastbound	Str	363	370	378	385	393	401	409
	Rht	84	86	87	89	91	93	95
Westbound	Lft	78	80	81	83	84	86	88
	Str	368	375	383	391	398	406	414
Northbound	Lft	97	99	101	103	105	107	109
	Rht	85	87	88	90	92	94	96

ATTACHMENT 6

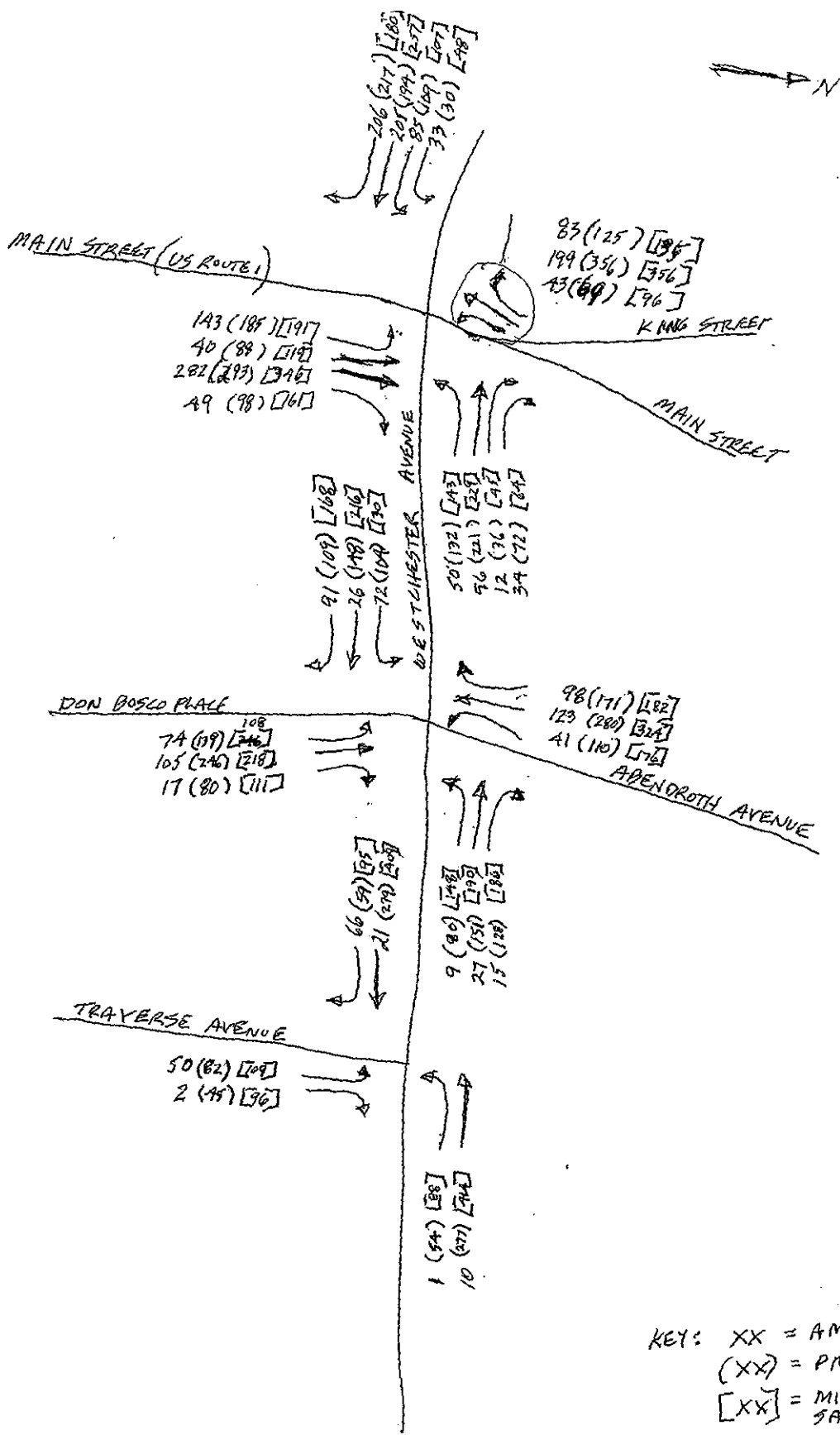
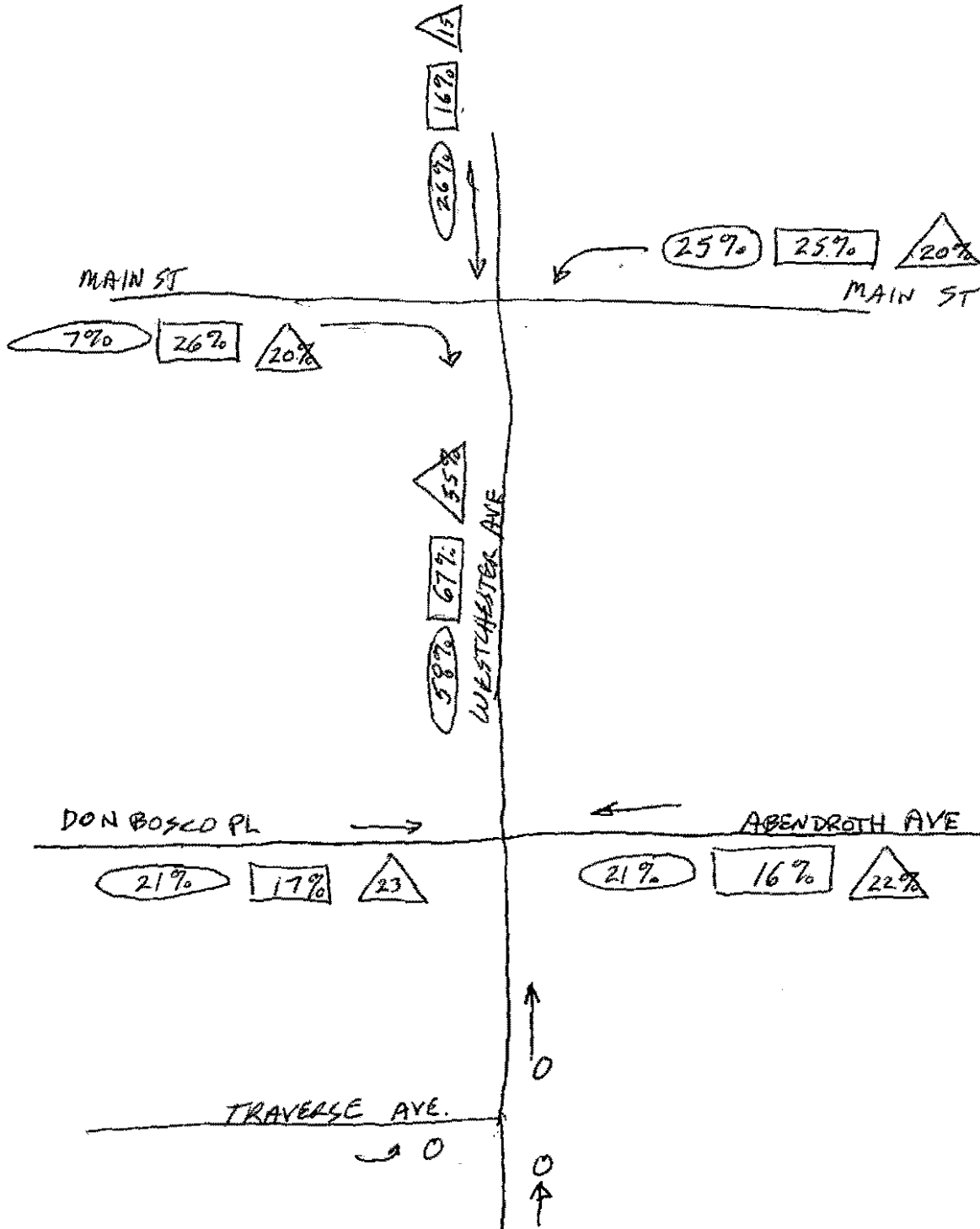


FIGURE 2
 NO BUILD (2017)

ATTACHMENT 7



KEY:




-  1999 = 3 MILE RADIUS
-  1999 = 5 MILE RADIUS COSTLO + MOVIES
-  2016 ASSUMPTIONS

FIGURE 3
DIRECTIONAL DISTRIBUTION
OF
SITE GENERATED TRAFFIC

NOTE: TWO DIRECTIONAL DISTRIBUTIONS USED IN 1999 DOCUMENTS.
ONE = 3 MILE RADIUS
TWO = 5 MILE RADIUS = COSTLO + MOVIES

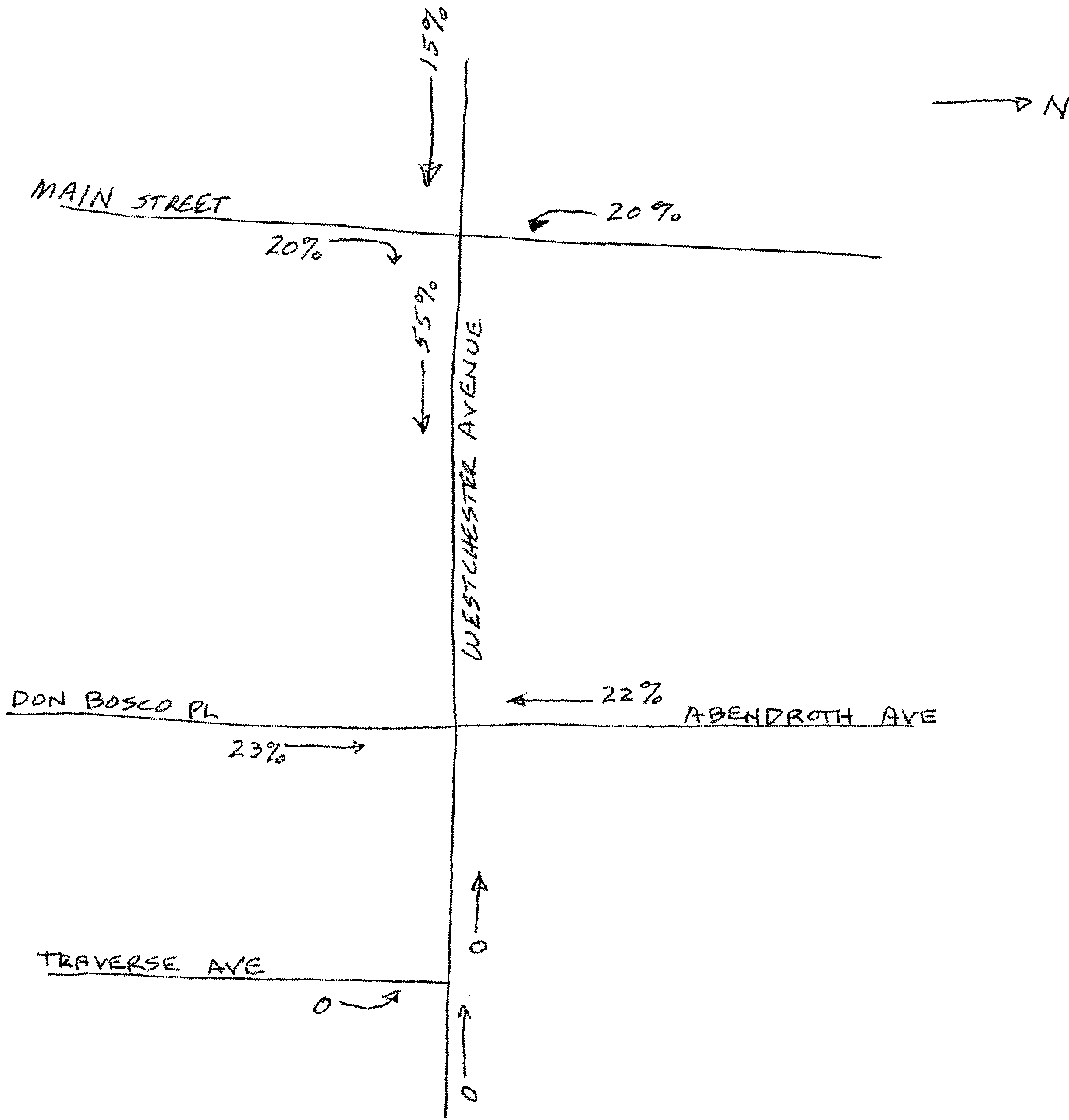
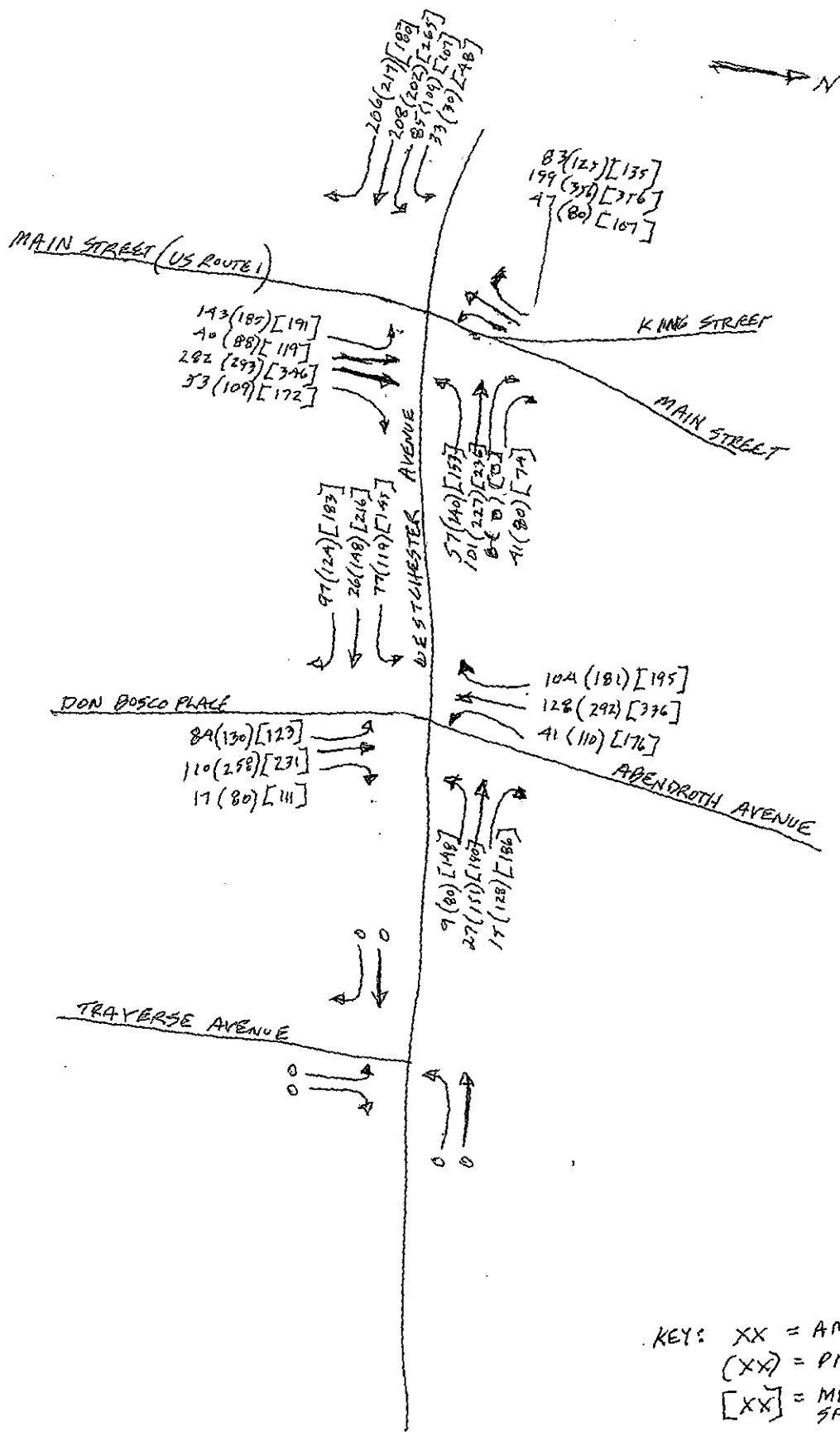


FIGURE 3A
 DIRECTIONAL DISTRIBUTION
 AGREED UPON

ATTACHMENT 8



KEY: XX = AM
 (XX) = PM
 [XX] = MIDDAY SATURDAY

FIGURE 4
 BUILD 2017

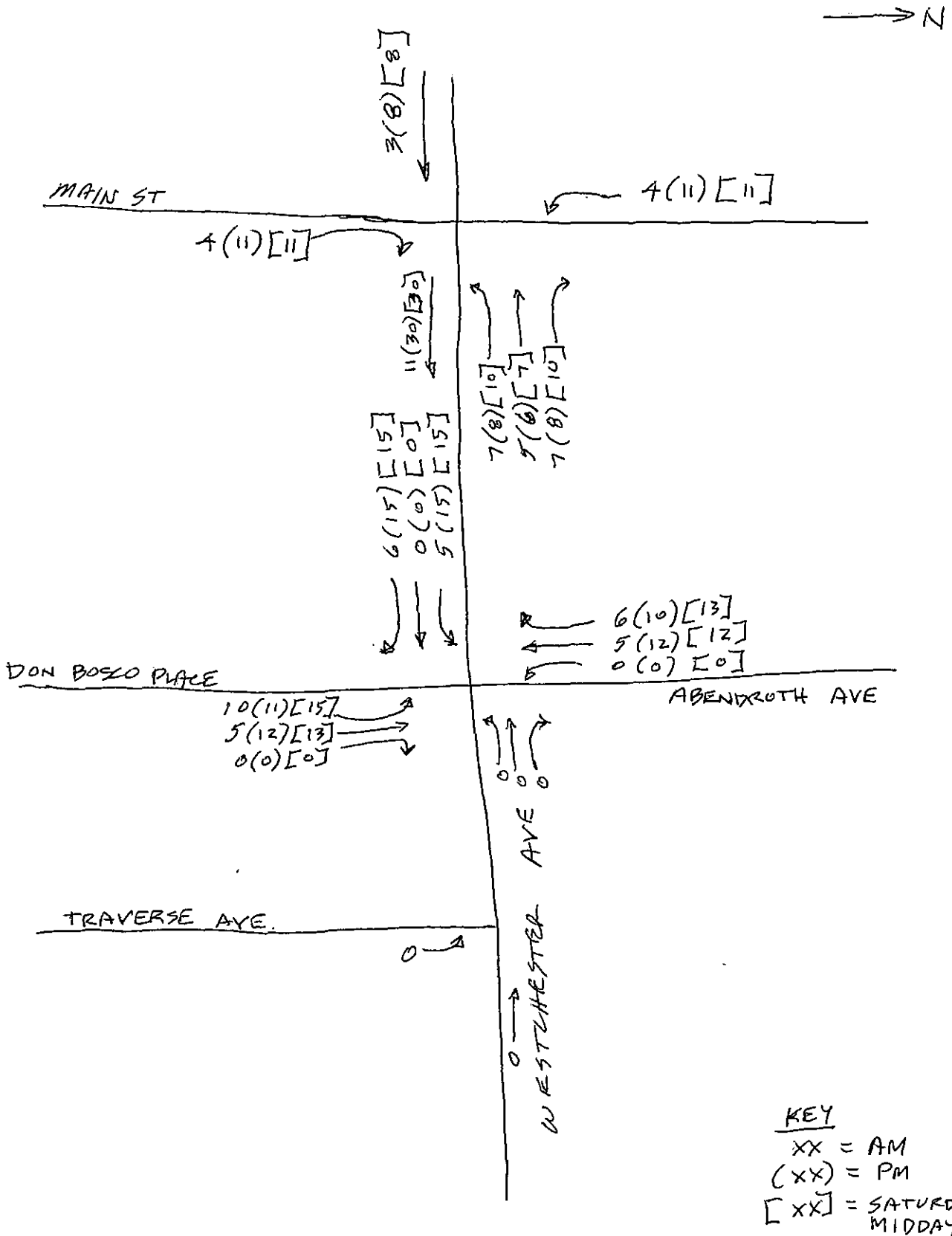


FIGURE 4 B
 ASSIGNMENT OF SITE GENERATED TRAFFIC

Proposed Use	A.M.*		P.M.*		Saturday**	
	.44 Enter	.56 Exit	.56 Enter	.44 Exit	.52 Enter	.48 Exit
Retail Use: 12,000 S.F. Specialty Retail Center Land Use Code 826	14	18	35	25	30	28
	.31 Enter	.69 Exit	.58 Enter	.42 Exit	.54 Enter	.46 Exit
Residential Apartments: 79 Units Land Use Code 223 (Mid-Rise Apartment)	7	17	18	13	25***	21***
Total	21	35	53	38	55	49
Previously Approved Use						
Retail Use: 40,000 S.F. Specialty Retail Center Land Use Code 826	48	60	113	88	100	93
Reduction in Site Generated Traffic	27	25	60	50	45	44
% Reduction in Site Generated Traffic	48%		55%		46%	

*Peak Hours of Adjacent Street Traffic

**Peak Hour of Generator

***Land Use 221 Low-Rise Apartment Data

Table 1
Comparison of Site Generated Traffic
Proposed Use vs Previously Approved Use

ATTACHMENT 9

Signalized Intersections Level of Service – AM Peak Hour								
Intersection	Movement	Lane Group	Existing 2016		No Build 2017		Build 2017	
			Delay	LOS	Delay	LOS	Delay	LOS
Main Street (US 1) at Westchester Avenue	EB	L	7.0	A	7.2	A	7.2	A
		T	7.0	A	7.1	A	7.2	A
		R	7.3	A	7.5	A	7.5	A
		Approach	7.1	A	7.3	A	7.3	A
	WB	L	8.1	A	8.3	A	8.4	A
		T	6.4	A	6.5	A	6.5	A
		R	6.0	A	6.1	A	6.1	A
		Approach	6.7	A	6.9	A	7.0	A
	NB	L	37.7	D	37.9	D	37.8	D
		T	28.0	C	27.8	C	27.8	C
		R	26.2	C	25.9	C	26.0	C
	SB	Approach	30.5	C	30.4	C	30.4	C
		L	32.3	C	32.2	C	32.4	C
		T	28.7	C	28.5	C	28.5	C
		R	26.8	C	26.6	C	26.6	C
	Overall	Approach	28.7	C	28.5	C	28.6	C
Overall		19.3	B	19.3	B	19.3	B	

Signalized Intersections Level of Service – PM Peak Hour								
Intersection	Movement	Lane Group	Existing 2016		No Build 2017		Build 2017	
			Delay	LOS	Delay	LOS	Delay	LOS
Main Street (US 1) at Westchester Avenue	EB	L	9.6	A	9.7	A	9.8	A
		T	8.0	A	8.0	A	8.1	A
		R	8.5	A	8.5	A	8.5	A
		Approach	8.6	A	8.6	A	8.7	A
	WB	L	10.3	B	10.4	B	10.6	B
		T	8.2	A	8.2	A	8.2	A
		R	7.2	A	7.2	A	7.1	A
		Approach	8.5	A	8.6	A	8.8	A
	NB	L	137.7	F	158.5	F	158.5	F
		T	26.5	C	26.6	C	26.6	C
		R	25.2	C	25.2	C	25.5	C
		Approach	57.3	E	63.1	E	62.6	E
	SB	L	32.3	C	32.6	C	33.1	C
		T	33.4	C	34.1	C	34.1	C
		R	25.7	C	25.8	C	25.8	C
		Approach	31.5	C	32.0	C	32.1	C
Overall	Approach	28.8	C	30.7	C	30.8	C	
	Overall	28.8	C	30.7	C	30.8	C	

Table A
Summary of Signalized Intersection Capacity Analysis Results
Main Street (US 1) at Westchester Avenue

Signalized Intersections Level of Service – Saturday Peak Hour								
Intersection	Movement	Lane Group	Existing 2016		No Build 2017		Build 2017	
			Delay	LOS	Delay	LOS	Delay	LOS
Main Street (US 1) at Westchester Avenue	EB	L	7.2	A	9.9	A	10.0	A
		T	6.2	A	8.5	A	8.5	A
		R	6.0	A	8.2	A	8.2	A
		Approach	6.4	A	8.8	A	8.8	A
	WB	L	8.5	A	11.6	B	12.0	B
		T	6.1	A	8.3	A	8.3	A
		R	5.3	A	7.2	A	7.1	A
		Approach	6.6	A	9.0	A	9.3	A
	NB	L	472.4	F	171.9	F	171.9	F
		T	32.6	C	24.4	C	27.4	C
		R	30.8	C	27.6	C	26.8	C
		Approach	135.4	F	80.2	F	60.6	E
	SB	L	45.1	D	27.6	C*	37.3	D
		T	55.0	E	34.1	C	34.1	C
		R	30.2	C	26.0	C	26.0	C
		Approach	47.7	D	31.2	C	32.8	C
Overall			58.8	E	31.8	C	31.9	C

Table A (Continued)
Summary of Signalized Intersection Capacity Analysis Results
Main Street (US 1) at Westchester Avenue

Signalized Intersections Level of Service – AM Peak Hour								
Intersection	Movement	Lane Group	Existing 2016		No Build 2017		Build 2017	
			Delay	LOS	Delay	LOS	Delay	LOS
Westchester Avenue at Abendroth Avenue/Don Bosco Place	EB	L	5.8	A	5.9	A	6.3	A
		T	6.5	A	6.5	A	6.9	A
		R	6.9	A	7.0	A	7.5	A
		Approach	6.4	A	6.5	A	6.9	A
	WB	L	7.1	A	7.2	A	7.6	A
		T/R	7.9	A	8.0	A	8.5	A
		Approach	7.8	A	7.9	A	8.3	A
	NB	L	46.9	D	46.7	D	46.1	D
		T/R	53.3	D	53.2	D	52.0	D
		Approach	50.9	D	50.8	D	49.6	D
	SB	L	47.3	D	47.2	D	46.8	D
		T	56.2	E	56.1	E	55.9	E
		R	55.7	E	55.6	E	55.7	E
		Approach	54.6	D	54.5	D	54.4	D
Overall			37.1	D	37.1	D	36.9	D

Signalized Intersections Level of Service – PM Peak Hour								
Intersection	Movement	Lane Group	Existing 2016		No Build 2017		Build 2017	
			Delay	LOS	Delay	LOS	Delay	LOS
Westchester Avenue at Abendroth Avenue/Don Bosco Place	EB	L	13.9	B	14.2	B	14.8	B
		T	16.7	B	17.0	B	17.5	B
		R	16.6	B	16.9	B	17.6	B
		Approach	15.9	B	16.2	B*	16.7	B
	WB	L	13.3	B	13.5	B	14.2	B
		T/R	19.3	B	19.7	B	20.7	C
		Approach	18.0	B	18.3	B	19.3	B
	NB	L	35.8	D	35.6	D	34.8	C
		T/R	62.1	E	63.6	E	63.6	E
		Approach	54.2	D	55.2	E	55.6	E
	SB	L	37.8	D	37.7	D	36.9	D
		T	51.0	D	51.4	D	49.9	D
		R	43.7	D	39.9	D	42.4	D
		Approach	46.2	D	46.4	D	45.2	D
Overall			36.2	D	36.1	D	38.5	D

Table A
Summary of Signalized Intersection Capacity Analysis Results
Westchester Avenue at Abendroth Avenue/Don Bosco Place

Signalized Intersections Level of Service – Saturday Peak Hour								
Intersection	Movement	Lane Group	Existing 2016		No Build 2017		Build 2017	
			Delay	LOS	Delay	LOS	Delay	LOS
Westchester Avenue at Abendroth Avenue/Don Bosco Place	EB	L	18.5	B	18.5	B	19.0	B
		T	22.1	C	22.1	C	22.6	C
		R	22.0	C	22.0	C	22.9	C
		Approach	21.1	C	21.1	C	21.8	C
	WB	L	16.3	B	16.3	B	16.7	B
		T/R	26.4	C	26.4	C	27.6	C
		Approach	23.5	C	23.5	C	24.5	C
	NB	L	34.7	C	34.7	C	16.7	B
		T/R	70.7	E	70.7	E	27.6	C
		Approach	61.8	E	61.8	E	24.5	C
	SB	L	39.6	D	39.6	D	40.1	D
		T	46.0	D	46.0	D	48.2	D
		R	38.4	D	38.4	D	39.2	D
Approach		42.3	D	42.3	D	43.7	D	
Overall			36.7	D	36.7	D	37.8	D

Table A (Continued)
Summary of Signalized Intersection Capacity Analysis Results
Westchester Avenue at Abendroth Avenue/Don Bosco Place

2016 Existing Condition						
Location / Movement	Average Control Delay			Level of Service		
	A.M.	P.M.	Saturday	A.M.	P.M.	Saturday
Westbound Left/Thru on Westchester Avenue	7.2	11.9	28.5	A	A	A
Northbound Left/Right from Traverse Avenue	7.6	9.9	13.6	B	B	D
Intersection	7.2	11.4	25.1	A	B	D

2017 No Build Condition						
Location / Movement	Average Control Delay			Level of Service		
	A.M.	P.M.	Saturday	A.M.	P.M.	Saturday
Westbound Left/Thru on Westchester Avenue	7.2	12.0	29.5	A	A	A
Northbound Left/Right from Traverse Avenue	7.6	9.9	13.6	A	B	B
Intersection	7.2	11.0	23.8	A	B	C

2017 Build Condition						
Location / Movement	Average Control Delay			Level of Service		
	A.M.	P.M.	Saturday	A.M.	P.M.	Saturday
Westbound Left/Thru on Westchester Avenue	7.2	12.0	29.5	A	A	A
Northbound Left/Right from Traverse Avenue	7.6	9.9	13.6	A	B	B
Intersection	7.2	11.0	23.8	A	B	C

Note: Since no site generated traffic is projected to travel through this intersection, Build analysis is same as No Build for A.M., P.M. and Saturday.

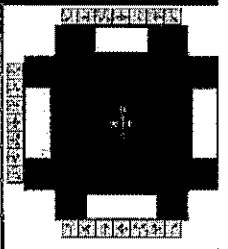
Table B
Summary of Unsignalized Intersection Capacity Analyses Results
Westchester Avenue at Traverse Avenue

ATTACHMENT 10

**Westchester Avenue
at
Main Street (US 1)**

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	AM Existing	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2016	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Kin...	File Name	am existing.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	115	201	202	49	94	45	141	316	48	42	195	81

Signal Information				Signal Phases																				
Cycle, s	90.0	Reference Phase	2																					
Offset, s	0	Reference Point	End																					
Uncoordinated	No	Simult. Gap E/W	On																					
Force Mode	Fixed	Simult. Gap N/S	On																					
				Green	57.4	22.6	0.0	0.0	0.0	0.0	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	Red	1.0	1.0	0.0	0.0	0.0	0.0

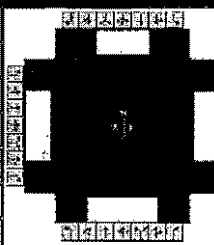
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		62.4		62.4		27.6		27.6
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.2		3.2
Queue Clearance Time (g _s), s						21.7		12.4
Green Extension Time (g _e), s		0.0		0.0		0.9		1.8
Phase Call Probability						1.00		1.00
Max Out Probability						0.96		0.03

Movement Group Results	EB			WB			NB			SB			
	L	T	R	L	T	R	L	T	R	L	T	R	
Approach Movement													
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14	
Adjusted Flow Rate (v), veh/h	125	218	220	53	102	49	153	343	52	46	212	88	
Adjusted Saturation Flow Rate (s), veh/h/ln	1313	1900	1610	1181	1900	1425	1188	1809	1610	1054	1900	1610	
Queue Service Time (g _s), s	1.7	4.2	5.1	1.7	1.9	0.6	11.2	7.1	2.3	3.4	8.5	3.9	
Cycle Queue Clearance Time (g _c), s	3.6	4.2	5.1	6.0	1.9	0.6	19.7	7.1	2.3	10.4	8.5	3.9	
Green Ratio (g/C)	0.64	0.64	0.64	0.64	0.64	0.64	0.25	0.25	0.25	0.25	0.25	0.25	
Capacity (c), veh/h	1781	1212	1027	778	1212	1818	267	908	404	262	477	404	
Volume-to-Capacity Ratio (X)	0.070	0.180	0.214	0.068	0.084	0.027	0.575	0.378	0.129	0.174	0.445	0.214	
Available Capacity (c _a), veh/h	1781	1212	1027	778	1212	1818	298	1005	447	290	528	447	
Back of Queue (Q), veh/ln (50 th percentile)	0.4	1.6	1.6	0.4	0.7	0.2	3.2	3.0	0.8	0.8	3.8	1.5	
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Uniform Delay (d ₁), s/veh	6.9	6.7	6.8	7.9	6.2	6.0	36.7	27.9	26.1	32.2	28.4	26.7	
Incremental Delay (d ₂), s/veh	0.1	0.3	0.5	0.2	0.1	0.0	1.0	0.1	0.1	0.1	0.2	0.1	
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	7.0	7.0	7.3	8.1	6.4	6.0	37.7	28.0	26.2	32.3	28.7	26.8	
Level of Service (LOS)	A	A	A	A	A	A	D	C	C	C	C	C	
Approach Delay, s/veh / LOS	7.1	A		6.7	A		30.5	C			28.7	C	
Intersection Delay, s/veh / LOS	19.3						B						

Multimodal Results	EB			WB			NB			SB			
Pedestrian LOS Score / LOS	2.5	B		2.8	C		2.6	B			2.6	B	
Bicycle LOS Score / LOS	1.4	A		0.8	A		0.9	A			1.1	A	

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	PM Existing	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2016	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Kin...	File Name	pm existing.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	137	190	213	129	216	106	181	373	96	67	349	123

Signal Information													
Cycle, s	90.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	55.0	25.0	0.0	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0			
				Red	1.0	1.0	0.0	0.0	0.0	0.0			

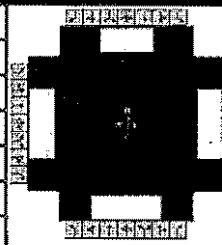
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		60.0		60.0		30.0		30.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						27.0		18.2
Green Extension Time (g _e), s		0.0		0.0		0.0		2.2
Phase Call Probability						1.00		1.00
Max Out Probability						1.00		0.43

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	149	207	232	140	235	115	197	405	104	73	379	134
Adjusted Saturation Flow Rate (s), veh/h/ln	1164	1900	1610	1194	1900	1425	1019	1809	1610	995	1900	1610
Queue Service Time (g _s), s	2.7	4.3	5.9	5.2	4.9	1.5	8.8	8.2	4.5	5.8	16.2	5.9
Cycle Queue Clearance Time (g _c), s	7.7	4.3	5.9	9.5	4.9	1.5	25.0	8.2	4.5	14.0	16.2	5.9
Green Ratio (g/C)	0.61	0.61	0.61	0.61	0.61	0.61	0.28	0.28	0.28	0.28	0.28	0.28
Capacity (c), veh/h	1455	1161	984	753	1161	1742	180	1005	447	266	528	447
Volume-to-Capacity Ratio (X)	0.102	0.178	0.235	0.186	0.202	0.066	1.096	0.403	0.233	0.274	0.719	0.293
Available Capacity (c _a), veh/h	1455	1161	984	753	1161	1742	180	1005	447	266	528	447
Back of Queue (Q), veh/ln (50 th percentile)	0.7	1.6	1.9	1.4	1.9	0.4	8.7	3.4	1.7	1.4	7.7	2.2
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	9.5	7.6	7.9	9.7	7.8	7.1	42.6	26.4	25.1	32.1	29.3	25.6
Incremental Delay (d ₂), s/veh	0.1	0.3	0.6	0.5	0.4	0.1	95.2	0.1	0.1	0.2	4.1	0.1
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	9.6	8.0	8.5	10.3	8.2	7.2	137.7	26.5	25.2	32.3	33.4	25.7
Level of Service (LOS)	A	A	A	B	A	A	F	C	C	C	C	C
Approach Delay, s/veh / LOS	8.6	A		8.5	A		57.3	E		31.5	C	
Intersection Delay, s/veh / LOS	28.8						C					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.5	B		2.8	C		2.6	B		2.6	B	
Bicycle LOS Score / LOS	1.5	A		1.3	A		1.1	A		1.5	A	

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	SAT Existing	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2016	Analysis Period	1> 7:00		
Intersection	Westchester Ave @ Kin...	File Name	sat existing.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	152	252	177	140	224	107	188	456	158	94	349	132

Signal Information														
Cycle, s	90.0	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	No	Simult. Gap E/W	On	Green	60.0	20.0	0.0	0.0	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	0.0			
				Red	1.0	1.0	0.0	0.0	0.0	0.0	0.0			

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		65.0		65.0		25.0		25.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						22.0		22.0
Green Extension Time (g _e), s		0.0		0.0		0.0		0.0
Phase Call Probability						1.00		1.00
Max Out Probability						1.00		1.00

Movement Group Results	EB			WB			NB			SB			
	L	T	R	L	T	R	L	T	R	L	T	R	
Approach Movement													
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14	
Adjusted Flow Rate (v), veh/h	165	274	192	152	243	116	204	496	172	102	379	143	
Adjusted Saturation Flow Rate (s), veh/h/in	1154	1900	1610	1123	1900	1425	1019	1809	1610	916	1900	1610	
Queue Service Time (g _s), s	2.7	5.1	4.1	5.5	4.4	1.3	2.5	11.1	8.4	8.9	17.5	6.8	
Cycle Queue Clearance Time (g _c), s	7.1	5.1	4.1	10.6	4.4	1.3	20.0	11.1	8.4	20.0	17.5	6.8	
Green Ratio (g/C)	0.67	0.67	0.67	0.67	0.67	0.67	0.22	0.22	0.22	0.22	0.22	0.22	
Capacity (c), veh/h	1586	1267	1073	765	1267	1900	109	804	358	171	422	358	
Volume-to-Capacity Ratio (X)	0.104	0.216	0.179	0.199	0.192	0.061	1.878	0.617	0.480	0.599	0.898	0.40	
Available Capacity (c _a), veh/h	1586	1267	1073	765	1267	1900	109	804	358	171	422	358	
Back of Queue (Q), veh/in (50 th percentile)	0.6	1.8	1.3	1.3	1.6	0.3	15.3	4.8	3.2	2.4	10.2	2.6	
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Uniform Delay (d ₁), s/veh	7.1	5.8	5.7	7.9	5.7	5.2	44.7	31.5	30.5	41.1	34.0	29.9	
Incremental Delay (d ₂), s/veh	0.1	0.4	0.4	0.6	0.3	0.1	427.8	1.1	0.4	4.1	21.0	0.3	
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	7.2	6.2	6.0	8.5	6.1	5.3	472.4	32.6	30.8	45.1	55.0	30.2	
Level of Service (LOS)	A	A	A	A	A	A	F	C	C	D	E	C	
Approach Delay, s/veh / LOS	6.4	A		6.6	A		135.4	F			47.7	D	
Intersection Delay, s/veh / LOS	58.8						E						

Multimodal Results	EB			WB			NB			SB			
Pedestrian LOS Score / LOS	2.5	B		2.8	C		2.6	B			2.6	B	
Bicycle LOS Score / LOS	1.5	A		1.3	A		1.2	A			1.5	A	

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	AM No Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Kin...	File Name	am no build.xus				
Project Description	Retail						

Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	118	205	206	50	96	46	143	322	49	43	199	83

Signal Information				Signal Timing											
Cycle, s	90.0	Reference Phase	2	EB			WB			NB			SB		
Offset, s	0	Reference Point	End	Green	57.1	22.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Uncoordinated	No	Simult. Gap E/W	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Force Mode	Fixed	Simult. Gap N/S	On	Red	1.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	

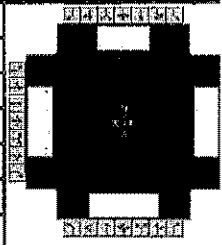
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		62.1		62.1		27.9		27.9
Change Period, (Y+R c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.2		3.2
Queue Clearance Time (g s), s						22.1		12.6
Green Extension Time (g e), s		0.0		0.0		0.8		1.8
Phase Call Probability						1.00		1.00
Max Out Probability						1.00		0.03

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	128	223	224	54	104	50	155	350	53	47	216	90
Adjusted Saturation Flow Rate (s), veh/h/ln	1310	1900	1610	1176	1900	1425	1183	1809	1610	1047	1900	1610
Queue Service Time (g s), s	1.8	4.4	5.3	1.8	1.9	0.6	11.4	7.2	2.3	3.5	8.6	4.0
Cycle Queue Clearance Time (g c), s	3.7	4.4	5.3	6.2	1.9	0.6	20.1	7.2	2.3	10.6	8.6	4.0
Green Ratio (g/C)	0.63	0.63	0.63	0.63	0.63	0.63	0.25	0.25	0.25	0.25	0.25	0.25
Capacity (c), veh/h	1767	1206	1022	769	1206	1808	268	920	410	263	483	410
Volume-to-Capacity Ratio (X)	0.073	0.185	0.219	0.071	0.087	0.028	0.580	0.380	0.130	0.178	0.448	0.221
Available Capacity (c a), veh/h	1767	1206	1022	769	1206	1808	295	1005	447	287	528	447
Back of Queue (Q), veh/ln (50 th percentile)	0.5	1.6	1.7	0.5	0.7	0.2	3.3	3.0	0.9	0.9	3.8	1.5
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d 1), s/veh	7.1	6.8	7.0	8.1	6.4	6.1	36.7	27.7	25.9	32.1	28.2	26.5
Incremental Delay (d 2), s/veh	0.1	0.3	0.5	0.2	0.1	0.0	1.2	0.1	0.1	0.1	0.2	0.1
Initial Queue Delay (d 3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	7.2	7.1	7.5	8.3	6.5	6.1	37.9	27.8	25.9	32.2	28.5	26.6
Level of Service (LOS)	A	A	A	A	A	A	D	C	C	C	C	C
Approach Delay, s/veh / LOS	7.3	A		6.9	A		30.4	C			28.5	C
Intersection Delay, s/veh / LOS	19.3						B					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.5	B		2.8	C		2.6	B			2.6	B
Bicycle LOS Score / LOS	1.4	A		0.8	A		0.9	A			1.1	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	PM No Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Kin...	File Name	pm no build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	139	194	217	132	221	108	185	381	98	69	356	125

Signal Information													
Cycle, s	90.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	55.0	25.0	0.0	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0			
				Red	1.0	1.0	0.0	0.0	0.0	0.0			

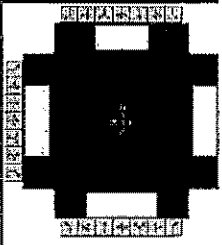
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		60.0		60.0		30.0		30.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						27.0		18.6
Green Extension Time (g _e), s		0.0		0.0		0.0		2.1
Phase Call Probability						1.00		1.00
Max Out Probability						1.00		0.49

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	151	211	236	143	240	117	201	414	107	75	387	136
Adjusted Saturation Flow Rate (s), veh/h/ln	1158	1900	1610	1189	1900	1425	1012	1809	1610	987	1900	1610
Queue Service Time (g _s), s	2.8	4.4	6.0	5.4	5.1	1.5	8.4	8.4	4.6	6.0	16.6	6.0
Cycle Queue Clearance Time (g _c), s	7.9	4.4	6.0	9.8	5.1	1.5	25.0	8.4	4.6	14.4	16.6	6.0
Green Ratio (g/C)	0.61	0.61	0.61	0.61	0.61	0.61	0.28	0.28	0.28	0.28	0.28	0.28
Capacity (c), veh/h	1445	1161	984	749	1161	1742	174	1005	447	262	528	447
Volume-to-Capacity Ratio (X)	0.105	0.182	0.240	0.192	0.207	0.067	1.154	0.412	0.238	0.286	0.733	0.307
Available Capacity (c _a), veh/h	1445	1161	984	749	1161	1742	174	1005	447	262	528	447
Back of Queue (Q), veh/ln (50 th percentile)	0.7	1.7	2.0	1.4	1.9	0.4	9.4	3.5	1.7	1.4	7.9	2.2
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	9.5	7.7	8.0	9.8	7.8	7.1	42.7	26.5	25.1	32.4	29.5	25.6
Incremental Delay (d ₂), s/veh	0.1	0.3	0.6	0.6	0.4	0.1	115.7	0.1	0.1	0.2	4.6	0.1
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	9.7	8.0	8.5	10.4	8.2	7.2	158.5	26.6	25.2	32.6	34.1	25.8
Level of Service (LOS)	A	A	A	B	A	A	F	C	C	C	C	C
Approach Delay, s/veh / LOS	8.6	A		8.6	A		63.1	E			32.0	C
Intersection Delay, s/veh / LOS	30.7						C					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.5	B		2.8	C		2.6	B			2.6	B
Bicycle LOS Score / LOS	1.5	A		1.3	A		1.1	A			1.5	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	SAT No Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Kin...	File Name	sat no build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	155	257	180	143	229	109	191	119	207	96	356	135

Signal Information													
Cycle, s	90.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	55.0	25.0	0.0	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0			
				Red	1.0	1.0	0.0	0.0	0.0	0.0			

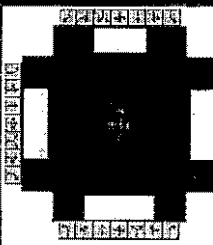
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		60.0		60.0		30.0		30.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						27.0		18.6
Green Extension Time (g _e), s		0.0		0.0		0.0		1.9
Phase Call Probability						1.00		1.00
Max Out Probability						1.00		0.46

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	168	279	196	155	249	118	208	129	225	104	387	147
Adjusted Saturation Flow Rate (s), veh/h/ln	1149	1900	1610	1117	1900	1425	1012	1809	1610	1281	1900	1610
Queue Service Time (g _s), s	3.2	6.0	4.8	6.6	5.3	1.5	8.4	2.4	10.6	6.0	16.6	6.5
Cycle Queue Clearance Time (g _c), s	8.5	6.0	4.8	12.7	5.3	1.5	25.0	2.4	10.6	8.4	16.6	6.5
Green Ratio (g/C)	0.61	0.61	0.61	0.61	0.61	0.61	0.28	0.28	0.28	0.28	0.28	0.28
Capacity (c), veh/h	1429	1161	984	688	1161	1742	174	1005	447	401	528	447
Volume-to-Capacity Ratio (X)	0.118	0.241	0.199	0.226	0.214	0.068	1.192	0.129	0.503	0.260	0.733	0.321
Available Capacity (c _a), veh/h	1429	1161	984	688	1161	1742	174	1005	447	401	528	447
Back of Queue (Q), veh/ln (50 th percentile)	0.8	2.3	1.6	1.6	2.0	0.4	10.1	1.0	3.9	1.8	7.9	2.4
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	9.7	8.0	7.7	10.9	7.8	7.1	42.7	24.3	27.3	27.5	29.5	25.8
Incremental Delay (d ₂), s/veh	0.2	0.5	0.5	0.8	0.4	0.1	129.1	0.0	0.3	0.1	4.6	0.2
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	9.9	8.5	8.2	11.6	8.3	7.2	171.9	24.4	27.6	27.6	34.1	26.0
Level of Service (LOS)	A	A	A	B	A	A	F	C	C	C	C	C
Approach Delay, s/veh / LOS	8.8	A		9.0	A		80.2	F		31.2	C	
Intersection Delay, s/veh / LOS	31.8						C					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.5	B		2.8	C		2.6	B		2.6	B	
Bicycle LOS Score / LOS	1.5	A		1.4	A		1.0	A		1.5	A	

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	AM Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Kin...	File Name	am_build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	118	208	206	57	101	41	143	322	53	47	199	83

Signal Information														
Cycle, s	90.0	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	No	Simult. Gap E/W	On	Green	57.1	22.9	0.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0				
				Red	1.0	1.0	0.0	0.0	0.0	0.0				

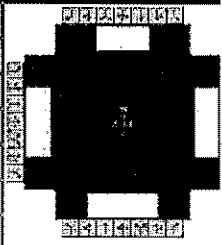
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		62.1		62.1		27.9		27.9
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.2		3.2
Queue Clearance Time (g _s), s						22.1		13.0
Green Extension Time (g _e), s		0.0		0.0		0.8		1.8
Phase Call Probability						1.00		1.00
Max Out Probability						1.00		0.04

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	128	226	224	62	110	45	155	350	58	51	216	90
Adjusted Saturation Flow Rate (s), veh/h/ln	1304	1900	1610	1173	1900	1425	1183	1809	1810	1047	1900	1610
Queue Service Time (g _s), s	1.8	4.4	5.3	2.1	2.0	0.5	11.4	7.2	2.5	3.8	8.6	4.0
Cycle Queue Clearance Time (g _c), s	3.8	4.4	5.3	6.5	2.0	0.5	20.1	7.2	2.5	11.0	8.6	4.0
Green Ratio (g/C)	0.63	0.63	0.63	0.63	0.63	0.63	0.25	0.25	0.25	0.25	0.25	0.25
Capacity (c), veh/h	1756	1205	1022	766	1205	1808	268	921	410	263	483	410
Volume-to-Capacity Ratio (X)	0.073	0.188	0.219	0.081	0.091	0.025	0.580	0.380	0.141	0.194	0.447	0.221
Available Capacity (c _a), veh/h	1756	1205	1022	766	1205	1808	296	1005	447	287	528	447
Back of Queue (Q), veh/ln (50 th percentile)	0.5	1.7	1.7	0.5	0.8	0.1	3.3	3.0	0.9	1.0	3.8	1.5
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	7.1	6.8	7.0	8.2	6.4	6.1	36.7	27.7	25.9	32.2	28.2	26.5
Incremental Delay (d ₂), s/veh	0.1	0.3	0.5	0.2	0.1	0.0	1.2	0.1	0.1	0.1	0.2	0.1
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	7.2	7.2	7.5	8.4	6.5	6.1	37.8	27.8	26.0	32.4	28.5	26.6
Level of Service (LOS)	A	A	A	A	A	A	D	C	C	C	C	C
Approach Delay, s/veh / LOS	7.3	A		7.0	A		30.4	C			28.6	C
Intersection Delay, s/veh / LOS	19.3						B					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.5	B		2.8	C		2.6	B			2.6	B
Bicycle LOS Score / LOS	1.4	A		0.8	A		1.0	A			1.1	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	PM Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Kin...	File Name	pm build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	139	202	217	140	227	80	185	381	109	80	356	125

Signal Information													
Cycle, s	90.0	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	No	Simult. Gap E/W	On	Green	55.0	25.0	0.0	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	4.0	0.0	0.0	0.0	0.0			
				Red	1.0	1.0	0.0	0.0	0.0	0.0			

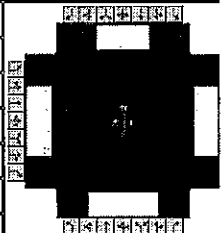
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		60.0		60.0		30.0		30.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						27.0		18.6
Green Extension Time (g _e), s		0.0		0.0		0.0		2.2
Phase Call Probability						1.00		1.00
Max Out Probability						1.00		0.50

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	151	220	236	152	247	87	201	414	118	87	387	136
Adjusted Saturation Flow Rate (s), veh/h/ln	1151	1900	1610	1180	1900	1425	1012	1809	1610	987	1900	1610
Queue Service Time (g _s), s	2.8	4.6	6.0	5.9	5.2	1.1	8.4	8.4	5.2	7.1	16.6	6.0
Cycle Queue Clearance Time (g _c), s	8.0	4.6	6.0	10.4	5.2	1.1	25.0	8.4	5.2	15.5	16.6	6.0
Green Ratio (g/C)	0.61	0.61	0.61	0.61	0.61	0.61	0.28	0.28	0.28	0.28	0.28	0.28
Capacity (c), veh/h	1433	1161	984	741	1161	1742	174	1005	447	262	528	447
Volume-to-Capacity Ratio (X)	0.105	0.189	0.240	0.205	0.213	0.050	1.154	0.412	0.265	0.332	0.733	0.300
Available Capacity (c _a), veh/h	1433	1161	984	741	1161	1742	174	1005	447	262	528	447
Back of Queue (Q), veh/ln (50th percentile)	0.7	1.8	2.0	1.5	2.0	0.3	9.4	3.5	1.9	1.7	7.9	2.2
Queue Storage Ratio (RQ) (50th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	9.6	7.7	8.0	10.0	7.8	7.0	42.7	26.5	25.3	32.8	29.5	25.6
Incremental Delay (d ₂), s/veh	0.1	0.4	0.6	0.6	0.4	0.1	115.7	0.1	0.1	0.3	4.6	0.1
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	9.8	8.1	8.5	10.6	8.2	7.1	158.5	26.6	25.5	33.1	34.1	25.8
Level of Service (LOS)	A	A	A	B	A	A	F	C	C	C	C	C
Approach Delay, s/veh / LOS	8.7	A		8.8	A		62.6	E		32.1	C	
Intersection Delay, s/veh / LOS	30.8						C					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.5	B	2.8	C	2.6	B	2.6	B
Bicycle LOS Score / LOS	1.5	A	1.3	A	1.1	A	1.5	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	SAT Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Kin...	File Name	sat build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	155	265	180	153	236	74	191	465	172	107	356	135

Signal Information				Signal Timing															
Cycle, s	90.0	Reference Phase	2																
Offset, s	0	Reference Point	End																
Uncoordinated	No	Simult. Gap E/W	On																
Force Mode	Fixed	Simult. Gap N/S	On																
		Green		55.0	25.0	0.0	0.0	0.0	0.0										
		Yellow		4.0	4.0	0.0	0.0	0.0	0.0										
		Red		1.0	1.0	0.0	0.0	0.0	0.0										

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2		6		8		4
Case Number		5.0		5.0		5.0		5.0
Phase Duration, s		60.0		60.0		30.0		30.0
Change Period, (Y+R _c), s		5.0		5.0		5.0		5.0
Max Allow Headway (MAH), s		0.0		0.0		3.3		3.3
Queue Clearance Time (g _s), s						27.0		23.7
Green Extension Time (g _e), s		0.0		0.0		0.0		0.7
Phase Call Probability						1.00		1.00
Max Out Probability						1.00		1.00

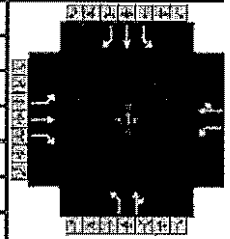
Movement Group Results	EB			WB			NB			SB			
	L	T	R	L	T	R	L	T	R	L	T	R	
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14	
Adjusted Flow Rate (v), veh/h	168	288	196	166	257	80	208	505	187	116	387	147	
Adjusted Saturation Flow Rate (s), veh/h/ln	1141	1900	1610	1108	1900	1425	1012	1809	1610	908	1900	1610	
Queue Service Time (g _s), s	3.2	6.3	4.8	7.3	5.5	1.0	8.4	10.6	8.5	11.1	16.6	6.5	
Cycle Queue Clearance Time (g _c), s	8.7	6.3	4.8	13.5	5.5	1.0	25.0	10.6	8.5	21.7	16.6	6.5	
Green Ratio (g/C)	0.61	0.61	0.61	0.61	0.61	0.61	0.28	0.28	0.28	0.28	0.28	0.28	
Capacity (c), veh/h	1416	1161	984	680	1161	1742	174	1005	447	226	528	447	
Volume-to-Capacity Ratio (X)	0.119	0.248	0.199	0.244	0.221	0.046	1.192	0.503	0.418	0.515	0.733	0.321	
Available Capacity (c _a), veh/h	1416	1161	984	680	1161	1742	174	1005	447	226	528	447	
Back of Queue (Q), veh/ln (50 th percentile)	0.8	2.4	1.6	1.8	2.1	0.3	10.1	4.4	3.2	2.4	7.9	2.4	
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Uniform Delay (d ₁), s/veh	9.8	8.0	7.7	11.1	7.9	7.0	42.7	27.3	26.6	36.4	29.5	25.8	
Incremental Delay (d ₂), s/veh	0.2	0.5	0.5	0.9	0.4	0.1	129.1	0.2	0.2	0.9	4.6	0.2	
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (d), s/veh	10.0	8.5	8.2	12.0	8.3	7.1	171.9	27.4	26.8	37.3	34.1	26.0	
Level of Service (LOS)	A	A	A	B	A	A	F	C	C	D	C	C	
Approach Delay, s/veh / LOS	8.8	A		9.3	A		60.6	E			32.8	C	
Intersection Delay, s/veh / LOS	31.9						C						

Multimodal Results	EB			WB			NB			SB			
Pedestrian LOS Score / LOS	2.5	B		2.8	C		2.6	B			2.6	B	
Bicycle LOS Score / LOS	1.6	A		1.3	A		1.2	A			1.6	A	

Westchester Avenue
at
Abendroth Avenue/Don Bosco Place

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	AM Existing	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2016	Analysis Period	1> 7:00		
Intersection	Westchester Ave @ Abe...	File Name	am existing.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	71	25	89	9	26	15	73	103	17	40	121	96

Signal Information				Signal Timing and Phases											
Cycle, s	120.0	Reference Phase	2												
Offset, s	0	Reference Point	End	Green	1.7	3.9	77.1	4.6	1.8	10.9					
Uncoordinated	No	Simult. Gap E/W	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0					
Force Mode	Fixed	Simult. Gap N/S	On	Red	1.0	0.0	1.0	1.0	0.0	1.0					

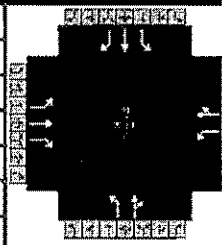
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	10.5	86.0	6.7	82.1	11.4	17.7	9.6	15.9
Change Period, (Y+R _c), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	3.7		2.2		6.8	10.4	4.6	10.3
Green Extension Time (g _e), s	0.1	0.0	0.0	0.0	0.1	0.7	0.0	0.7
Phase Call Probability	0.92		0.28		0.93	1.00	0.77	1.00
Max Out Probability	0.00		0.00		0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	77	27	97	10	45		79	130		43	132	104
Adjusted Saturation Flow Rate (s), veh/h/ln	1740	1863	1533	1723	1748		1774	1799		1774	1863	1576
Queue Service Time (g _s), s	1.7	0.6	2.6	0.2	1.1		4.8	8.4		2.6	8.3	7.7
Cycle Queue Clearance Time (g _c), s	1.7	0.6	2.6	0.2	1.1		4.8	8.4		2.6	8.3	7.7
Green Ratio (g/C)	0.70	0.68	0.68	0.66	0.64		0.14	0.11		0.13	0.09	0.09
Capacity (c), veh/h	983	1257	1035	944	1123		182	191		152	170	144
Volume-to-Capacity Ratio (X)	0.079	0.022	0.093	0.010	0.040		0.436	0.683		0.285	0.774	0.725
Available Capacity (c _a), veh/h	1449	1257	1035	1461	1123		310	531		306	522	442
Back of Queue (Q), veh/ln (50 th percentile)	0.6	0.2	0.8	0.1	0.4		2.1	3.8		1.2	4.0	3.1
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	5.8	6.4	6.8	7.1	7.9		46.3	51.7		46.9	53.3	53.1
Incremental Delay (d ₂), s/veh	0.0	0.0	0.2	0.0	0.1		0.6	1.6		0.4	2.8	2.6
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	5.8	6.5	6.9	7.1	7.9		46.9	53.3		47.3	56.2	55.7
Level of Service (LOS)	A	A	A	A	A		D	D		D	E	E
Approach Delay, s/veh / LOS	6.4		A	7.8		A	50.9		D	54.6		D
Intersection Delay, s/veh / LOS	37.1						D					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.2		B	2.4		B	2.3		B	2.5		B
Bicycle LOS Score / LOS	0.8		A	0.6		A	0.8		A	0.9		A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	PM Existing	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2016	Analysis Period	1> 7:00		
Intersection	Westchester Ave @ Abe...	File Name	Pm existing.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	102	145	107	78	148	126	136	241	78	108	275	168

Signal Information				Signal Phases											
Cycle, s	120.0	Reference Phase	2												
Offset, s	0	Reference Point	End	Green	5.6	0.2	59.9	8.1	1.7	24.4					
Uncoordinated	No	Simult. Gap E/W	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0					
Force Mode	Fixed	Simult. Gap N/S	On	Red	1.0	0.0	1.0	1.0	0.0	1.0					

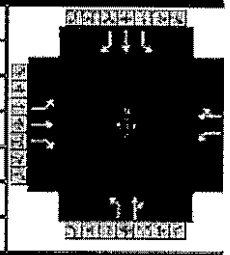
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	10.9	65.1	10.6	64.9	14.8	31.1	13.1	29.4
Change Period, (Y+R _c), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	5.7		4.8		9.8	24.9	8.2	20.3
Green Extension Time (g _e), s	0.1	0.0	0.1	0.0	0.1	1.2	0.1	1.4
Phase Call Probability	0.98		0.94		0.99	1.00	0.98	1.00
Max Out Probability	0.00		0.00		0.09	0.21	0.01	0.06

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	111	158	116	85	298		148	347		117	299	183
Adjusted Saturation Flow Rate (s), veh/h/ln	1740	1863	1533	1723	1720		1774	1767		1774	1863	1576
Queue Service Time (g _s), s	3.7	5.5	4.9	2.8	12.6		7.8	22.9		6.2	18.3	12.5
Cycle Queue Clearance Time (g _c), s	3.7	5.5	4.9	2.8	12.6		7.8	22.9		6.2	18.3	12.5
Green Ratio (g/C)	0.55	0.50	0.50	0.55	0.50		0.29	0.22		0.27	0.20	0.20
Capacity (c), veh/h	562	933	768	662	859		260	384		190	379	321
Volume-to-Capacity Ratio (X)	0.197	0.169	0.151	0.128	0.347		0.568	0.902		0.617	0.789	0.568
Available Capacity (c _a), veh/h	778	933	768	879	859		337	469		292	468	397
Back of Queue (Q), veh/ln (50 th percentile)	1.4	2.4	1.8	1.1	5.2		3.4	11.6		2.7	8.9	4.9
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	13.8	16.3	16.2	13.2	18.2		35.1	45.7		36.6	45.4	43.1
Incremental Delay (d ₂), s/veh	0.1	0.4	0.4	0.0	1.1		0.7	16.4		1.2	5.6	0.6
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	13.9	16.7	16.6	13.3	19.3		35.8	62.1		37.8	51.0	43.7
Level of Service (LOS)	B	B	B	B	B		D	E		D	D	D
Approach Delay, s/veh / LOS	15.9		B	18.0		B	54.2		D	46.2		D
Intersection Delay, s/veh / LOS	36.2						D					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.3	B	2.4	B	2.3	B	2.5	B
Bicycle LOS Score / LOS	1.1	A	1.1	A	1.3	A	1.5	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	SAT No Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1> 7:00		
Intersection	Westchester Ave @ Abe...	File Name	sat no build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	130	216	168	148	190	186	108	218	111	176	324	182

Signal Information				Signal Phases												
Cycle, s	120.0	Reference Phase	2	[Signal Diagrams]												
Offset, s	0	Reference Point	End	[Signal Diagrams]												
Uncoordinated	No	Simult. Gap E/W	On	Green	7.3	0.8	53.3	8.0	3.9	26.7	[Signal Diagrams]					
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0	[Signal Diagrams]					
				Red	1.0	0.0	1.0	1.0	0.0	1.0	[Signal Diagrams]					

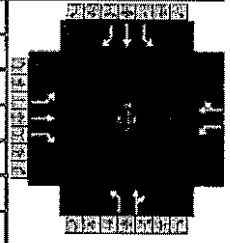
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	12.3	58.3	13.2	59.1	13.0	31.7	16.8	35.6
Change Period, (Y+R _c), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	7.2		8.0		8.0	26.1	11.7	22.9
Green Extension Time (g _e), s	0.1	0.0	0.2	0.0	0.1	0.6	0.1	1.5
Phase Call Probability	0.99		1.00		0.98	1.00	1.00	1.00
Max Out Probability	0.00		0.00		0.01	1.00	0.87	0.11

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	141	235	183	161	409		117	358		191	352	198
Adjusted Saturation Flow Rate (s), veh/h/ln	1740	1863	1533	1723	1710		1774	1739		1774	1863	1576
Queue Service Time (g _s), s	5.2	9.6	9.0	6.0	20.7		6.0	24.1		9.7	20.9	12.8
Cycle Queue Clearance Time (g _c), s	5.2	9.6	9.0	6.0	20.7		6.0	24.1		9.7	20.9	12.8
Green Ratio (g/C)	0.51	0.44	0.44	0.51	0.45		0.29	0.22		0.33	0.25	0.25
Capacity (c), veh/h	416	827	681	581	772		244	387		257	474	402
Volume-to-Capacity Ratio (X)	0.339	0.284	0.268	0.277	0.530		0.482	0.924		0.745	0.742	0.497
Available Capacity (c _a), veh/h	549	827	681	700	772		348	408		304	497	421
Back of Queue (Q), veh/ln (50 th percentile)	2.1	4.3	3.4	2.3	8.7		2.6	13.0		4.6	10.1	5.0
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	18.3	21.2	21.0	16.2	23.8		34.2	45.6		33.4	41.1	38.1
Incremental Delay (d ₂), s/veh	0.2	0.9	1.0	0.1	2.6		0.5	25.1		6.2	4.9	0.3
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	18.5	22.1	22.0	16.3	26.4		34.7	70.7		39.6	46.0	38.4
Level of Service (LOS)	B	C	C	B	C		C	E		D	D	D
Approach Delay, s/veh / LOS	21.1	C		23.5	C		61.8	E		42.3	D	
Intersection Delay, s/veh / LOS	36.7						D					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.3	B		2.4	B		2.3	B		2.5	B	
Bicycle LOS Score / LOS	1.4	A		1.4	A		1.3	A		1.7	A	

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	AM No Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1> 7:00		
Intersection	Westchester Ave @ Abe...	File Name	am no build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	72	26	91	9	27	15	74	105	17	41	123	98

Signal Information				Signal Phases												
Cycle, s	120.0	Reference Phase	2	[Signal Diagrams]												
Offset, s	0	Reference Point	End	[Signal Diagrams]												
Uncoordinated	No	Simult. Gap E/W	On	Green	1.7	3.9	76.9	4.6	1.8	11.1	[Signal Diagrams]					
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0	[Signal Diagrams]					
				Red	1.0	0.0	1.0	1.0	0.0	1.0	[Signal Diagrams]					

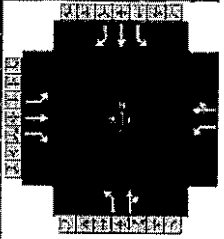
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	10.6	85.8	6.7	81.9	11.5	17.9	9.6	16.1
Change Period, (Y+R _c), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	3.7		2.2		6.9	10.5	4.7	10.4
Green Extension Time (g _e), s	0.1	0.0	0.0	0.0	0.1	0.7	0.0	0.7
Phase Call Probability	0.93		0.28		0.93	1.00	0.77	1.00
Max Out Probability	0.00		0.00		0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	78	28	99	10	46		80	133		45	134	107
Adjusted Saturation Flow Rate (s), veh/h/ln	1740	1863	1533	1723	1750		1774	1800		1774	1863	1576
Queue Service Time (g _s), s	1.7	0.6	2.7	0.2	1.2		4.9	8.5		2.7	8.4	7.9
Cycle Queue Clearance Time (g _c), s	1.7	0.6	2.7	0.2	1.2		4.9	8.5		2.7	8.4	7.9
Green Ratio (g/C)	0.69	0.67	0.67	0.65	0.64		0.15	0.11		0.13	0.09	0.09
Capacity (c), veh/h	979	1254	1032	941	1122		183	193		153	172	146
Volume-to-Capacity Ratio (X)	0.080	0.023	0.096	0.010	0.041		0.439	0.685		0.290	0.776	0.730
Available Capacity (c _a), veh/h	1442	1254	1032	1454	1122		310	530		307	521	441
Back of Queue (Q), veh/ln (50 th percentile)	0.6	0.2	0.9	0.1	0.4		2.2	3.9		1.2	4.0	3.2
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	5.9	6.5	6.8	7.2	7.9		46.1	51.6		46.8	53.2	53.0
Incremental Delay (d ₂), s/veh	0.0	0.0	0.2	0.0	0.1		0.6	1.6		0.4	2.8	2.6
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d'), s/veh	5.9	6.5	7.0	7.2	8.0		46.7	53.2		47.2	56.1	55.6
Level of Service (LOS)	A	A	A	A	A		D	D		D	E	E
Approach Delay, s/veh / LOS	6.5		A	7.9		A	50.8		D	54.5		D
Intersection Delay, s/veh / LOS	37.1						D					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.2		B	2.4		B	2.3		B	2.5		B
Bicycle LOS Score / LOS	0.8		A	0.6		A	0.8		A	1.0		A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC		Analysis Date	2/11/2016		Area Type	Other
Jurisdiction	Westchester		Time Period	PM No Build		PHF	0.92
Urban Street	Westchester Avenue		Analysis Year	2017		Analysis Period	1> 7:00
Intersection	Westchester Ave @ Abe...		File Name	pm no build.xus			
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	104	148	109	80	151	128	139	246	80	110	280	71

Signal Information				Signal Phases												
Cycle, s	120.0	Reference Phase	2	[Signal Diagrams]												
Offset, s	0	Reference Point	End	[Signal Diagrams]												
Uncoordinated	No	Simult. Gap E/W	On	Green	5.7	0.2	59.5	8.2	1.8	24.7	[Signal Diagrams]					
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0	[Signal Diagrams]					
				Red	1.0	0.0	1.0	1.0	0.0	1.0	[Signal Diagrams]					

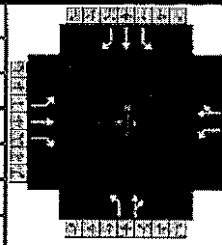
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	10.9	64.7	10.7	64.5	15.0	31.4	13.2	29.7
Change Period, (Y+R _c), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	5.8		4.9		9.9	25.5	8.3	20.6
Green Extension Time (g _e), s	0.1	0.0	0.1	0.0	0.1	1.0	0.1	1.2
Phase Call Probability	0.98		0.94		0.99	1.00	0.98	1.00
Max Out Probability	0.00		0.00		0.11	0.23	0.01	0.06

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	113	161	118	87	303		151	354		120	304	77
Adjusted Saturation Flow Rate (s), veh/h/ln	1740	1863	1533	1723	1721		1774	1767		1774	1863	1576
Queue Service Time (g _s), s	3.8	5.7	5.1	2.9	13.0		7.9	23.5		6.3	18.6	4.9
Cycle Queue Clearance Time (g _c), s	3.8	5.7	5.1	2.9	13.0		7.9	23.5		6.3	18.6	4.9
Green Ratio (g/C)	0.54	0.50	0.50	0.54	0.50		0.29	0.22		0.27	0.21	0.21
Capacity (c), veh/h	552	926	762	655	853		262	389		190	383	325
Volume-to-Capacity Ratio (X)	0.205	0.174	0.155	0.133	0.356		0.577	0.910		0.630	0.794	0.238
Available Capacity (c _a), veh/h	762	926	762	865	853		336	468		290	466	395
Back of Queue (Q), veh/ln (50 th percentile)	1.5	2.5	1.8	1.1	5.3		3.4	12.1		2.8	9.2	1.9
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	14.1	16.6	16.4	13.5	18.5		34.8	45.6		36.4	45.2	39.8
Incremental Delay (d ₂), s/veh	0.1	0.4	0.4	0.0	1.2		0.8	18.0		1.3	6.2	0.1
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	14.2	17.0	16.9	13.5	19.7		35.6	63.6		37.7	51.4	39.9
Level of Service (LOS)	B	B	B	B	B		D	E		D	D	D
Approach Delay, s/veh / LOS	16.2		B	18.3		B	55.2		E	46.4		D
Intersection Delay, s/veh / LOS	36.1						D					

Multimodal Results	EB			WB			NB			SB		
	Pedestrian LOS Score / LOS	2.3		B	2.4		B	2.3		B	2.5	
Bicycle LOS Score / LOS	1.1		A	1.1		A	1.3		A	1.3		A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	SAT No Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Abe...	File Name	sat no build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	130	216	168	148	190	186	108	218	111	176	324	182

Signal Information				Signal Phases												
Cycle, s	120.0	Reference Phase	2	[Signal Diagrams]												
Offset, s	0	Reference Point	End	[Signal Diagrams]												
Uncoordinated	No	Simult. Gap E/W	On	Green	7.3	0.8	53.3	8.0	3.9	26.7	[Signal Diagrams]					
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0	[Signal Diagrams]					
				Red	1.0	0.0	1.0	1.0	0.0	1.0	[Signal Diagrams]					

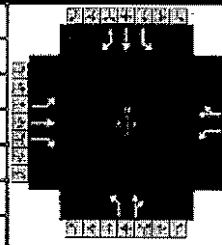
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	12.3	58.3	13.2	59.1	13.0	31.7	16.8	35.6
Change Period, (Y+R _c), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	7.2		8.0		8.0	26.1	11.7	22.9
Green Extension Time (g _e), s	0.1	0.0	0.2	0.0	0.1	0.6	0.1	1.5
Phase Call Probability	0.99		1.00		0.98	1.00	1.00	1.00
Max Out Probability	0.00		0.00		0.01	1.00	0.87	0.11

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	141	235	183	161	409		117	358		191	352	198
Adjusted Saturation Flow Rate (s), veh/h/in	1740	1863	1533	1723	1710		1774	1739		1774	1863	1579
Queue Service Time (g _s), s	5.2	9.6	9.0	6.0	20.7		6.0	24.1		9.7	20.9	12.8
Cycle Queue Clearance Time (g _c), s	5.2	9.6	9.0	6.0	20.7		6.0	24.1		9.7	20.9	12.8
Green Ratio (g/C)	0.51	0.44	0.44	0.51	0.45		0.29	0.22		0.33	0.25	0.25
Capacity (c), veh/h	416	827	681	581	772		244	387		257	474	402
Volume-to-Capacity Ratio (X)	0.339	0.284	0.268	0.277	0.530		0.482	0.924		0.745	0.742	0.493
Available Capacity (c _a), veh/h	549	827	681	700	772		348	408		304	497	421
Back of Queue (Q), veh/in (50 th percentile)	2.1	4.3	3.4	2.3	8.7		2.6	13.0		4.6	10.1	5.0
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	18.3	21.2	21.0	16.2	23.8		34.2	45.6		33.4	41.1	38.1
Incremental Delay (d ₂), s/veh	0.2	0.9	1.0	0.1	2.6		0.5	25.1		6.2	4.9	0.3
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	18.5	22.1	22.0	16.3	26.4		34.7	70.7		39.6	46.0	38.4
Level of Service (LOS)	B	C	C	B	C		C	E		D	D	D
Approach Delay, s/veh / LOS	21.1		C	23.5		C	61.8		E	42.3		D
Intersection Delay, s/veh / LOS	36.7						D					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.3	B	2.4	B	2.3	B	2.5	B
Bicycle LOS Score / LOS	1.4	A	1.4	A	1.3	A	1.7	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	AM Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1> 7:00		
Intersection	Westchester Ave @ Abe...	File Name	am build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	77	28	97	9	27	15	84	110	17	41	128	104

Signal Information				Signal Timing (s)											
Cycle, s	120.0	Reference Phase	2												
Offset, s	0	Reference Point	End	Green	1.7	4.0	75.7	4.6	2.6	11.5					
Uncoordinated	No	Simult. Gap E/W	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0					
Force Mode	Fixed	Simult. Gap N/S	On	Red	1.0	0.0	1.0	1.0	0.0	1.0					

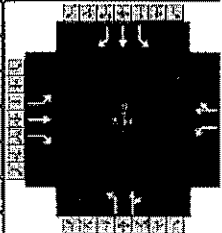
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	10.6	84.7	6.7	80.7	12.2	19.0	9.6	16.5
Change Period, (Y+R _c), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	3.9		2.2		7.5	10.8	4.7	10.8
Green Extension Time (g _e), s	0.1	0.0	0.0	0.0	0.1	0.7	0.0	0.7
Phase Call Probability	0.94		0.28		0.95	1.00	0.77	1.00
Max Out Probability	0.00		0.00		0.00	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	84	28	105	10	46		91	138		45	139	113
Adjusted Saturation Flow Rate (s), veh/h/ln	1740	1863	1533	1723	1750		1774	1801		1774	1863	1576
Queue Service Time (g _s), s	1.9	0.6	3.0	0.2	1.2		5.5	8.8		2.7	8.8	8.4
Cycle Queue Clearance Time (g _c), s	1.9	0.6	3.0	0.2	1.2		5.5	8.8		2.7	8.8	8.4
Green Ratio (g/C)	0.69	0.66	0.66	0.64	0.63		0.16	0.12		0.13	0.10	0.10
Capacity (c), veh/h	967	1237	1018	927	1104		195	211		162	178	151
Volume-to-Capacity Ratio (X)	0.087	0.023	0.104	0.011	0.041		0.469	0.656		0.275	0.782	0.750
Available Capacity (c _a), veh/h	1412	1237	1018	1425	1104		310	531		315	509	431
Back of Queue (Q), veh/ln (50 th percentile)	0.6	0.2	1.0	0.1	0.5		2.4	4.0		1.2	4.2	3.4
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	6.2	6.9	7.3	7.6	8.4		45.5	50.7		46.4	53.0	52.9
Incremental Delay (d ₂), s/veh	0.0	0.0	0.2	0.0	0.1		0.7	1.3		0.3	2.8	2.8
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d'), s/veh	6.3	6.9	7.5	7.6	8.5		46.1	52.0		46.8	55.9	55.7
Level of Service (LOS)	A	A	A	A	A		D	D		D	E	E
Approach Delay, s/veh / LOS	6.9	A		8.3	A		49.6	D		54.4	D	
Intersection Delay, s/veh / LOS	36.9						D					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.2	B	2.4	B	2.3	B	2.5	B
Bicycle LOS Score / LOS	0.8	A	0.6	A	0.9	A	1.0	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	PM Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Abe...	File Name	pm build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	119	148	124	80	151	128	130	258	80	110	292	181

Signal Information				Signal Phases															
Cycle, s	120.0	Reference Phase	2																
Offset, s	0	Reference Point	End	Green	5.7	0.9	57.9	8.1	1.2	26.2									
Uncoordinated	No	Simult. Gap E/W	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0									
Force Mode	Fixed	Simult. Gap N/S	On	Red	1.0	0.0	1.0	1.0	0.0	1.0									

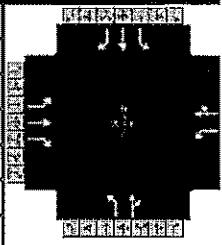
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	11.5	63.8	10.7	62.9	14.3	32.4	13.1	31.2
Change Period, (Y+Rc), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (gs), s	6.5		5.0		9.3	26.3	8.2	21.3
Green Extension Time (ge), s	0.2	0.0	0.1	0.0	0.1	1.1	0.1	1.4
Phase Call Probability	0.99		0.94		0.99	1.00	0.98	1.00
Max Out Probability	0.00		0.00		0.05	0.39	0.01	0.09

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	129	161	135	87	303		141	367		120	317	197
Adjusted Saturation Flow Rate (s), veh/h/ln	1740	1863	1533	1723	1721		1774	1769		1774	1863	1576
Queue Service Time (gs), s	4.5	5.8	5.9	3.0	13.3		7.3	24.3		6.2	19.3	13.4
Cycle Queue Clearance Time (gc), s	4.5	5.8	5.9	3.0	13.3		7.3	24.3		6.2	19.3	13.4
Green Ratio (g/C)	0.54	0.49	0.49	0.53	0.48		0.30	0.23		0.29	0.22	0.22
Capacity (c), veh/h	546	913	751	645	831		259	404		190	407	345
Volume-to-Capacity Ratio (X)	0.237	0.176	0.179	0.135	0.365		0.546	0.910		0.630	0.781	0.57
Available Capacity (ca), veh/h	733	913	751	844	831		343	470		291	476	403
Back of Queue (Q), veh/ln (50 th percentile)	1.7	2.6	2.2	1.2	5.5		3.2	12.6		2.7	9.4	5.2
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d1), s/veh	14.7	17.1	17.1	14.2	19.5		34.1	45.1		35.6	44.2	41.9
Incremental Delay (d2), s/veh	0.1	0.4	0.5	0.0	1.2		0.7	18.5		1.3	5.8	0.6
Initial Queue Delay (d3), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	14.8	17.5	17.6	14.2	20.7		34.8	63.6		36.9	49.9	42.4
Level of Service (LOS)	B	B	B	B	C		C	E		D	D	D
Approach Delay, s/veh / LOS	16.7		B	19.3		B	55.6		E	45.2		D
Intersection Delay, s/veh / LOS	36.5						D					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.3		B	2.4		B	2.3		B	2.5		B
Bicycle LOS Score / LOS	1.2		A	1.1		A	1.3		A	1.5		A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	DEA			Duration, h	0.25		
Analyst	CC	Analysis Date	2/11/2016	Area Type	Other		
Jurisdiction	Westchester	Time Period	SAT Build	PHF	0.92		
Urban Street	Westchester Avenue	Analysis Year	2017	Analysis Period	1 > 7:00		
Intersection	Westchester Ave @ Abe...	File Name	sat build.xus				
Project Description	Retail						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	145	216	183	148	190	186	123	231	111	176	336	195

Signal Information				Signal Phases											
Cycle, s	120.0	Reference Phase	2												
Offset, s	0	Reference Point	End	Green	8.0	0.2	52.5	8.8	3.0	27.4					
Uncoordinated	No	Simult. Gap E/W	On	Yellow	4.0	0.0	4.0	4.0	0.0	4.0					
Force Mode	Fixed	Simult. Gap N/S	On	Red	1.0	0.0	1.0	1.0	0.0	1.0					

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	13.0	57.5	13.2	57.7	13.8	32.4	16.8	35.5
Change Period, (Y+R _c), s	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Max Allow Headway (MAH), s	3.1	0.0	3.1	0.0	3.1	3.1	3.1	3.1
Queue Clearance Time (g _s), s	7.9		8.1		8.8	27.1	11.8	23.8
Green Extension Time (g _e), s	0.2	0.0	0.2	0.0	0.1	0.3	0.1	1.4
Phase Call Probability	0.99		1.00		0.99	1.00	1.00	1.00
Max Out Probability	0.01		0.01		0.03	1.00	0.88	0.23

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	158	235	199	161	409		134	372		191	365	212
Adjusted Saturation Flow Rate (s), veh/h/ln	1740	1863	1533	1723	1710		1774	1743		1774	1863	1576
Queue Service Time (g _s), s	5.9	9.7	10.1	6.1	21.1		6.8	25.1		9.8	21.8	13.9
Cycle Queue Clearance Time (g _c), s	5.9	9.7	10.1	6.1	21.1		6.8	25.1		9.8	21.8	13.9
Green Ratio (g/C)	0.50	0.44	0.44	0.51	0.44		0.30	0.23		0.33	0.25	0.25
Capacity (c), veh/h	412	815	671	573	751		246	398		255	473	401
Volume-to-Capacity Ratio (X)	0.383	0.288	0.296	0.281	0.544		0.543	0.933		0.751	0.773	0.529
Available Capacity (c _a), veh/h	523	815	671	681	751		338	409		301	484	410
Back of Queue (Q), veh/ln (50 th percentile)	2.3	4.4	3.8	2.4	9.0		3.0	13.7		4.6	10.8	5.4
Queue Storage Ratio (RQ) (50 th percentile)	0.00	0.00	0.00	0.00	0.00		0.00	0.00		0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	18.8	21.7	21.8	16.8	24.8		33.7	45.4		33.4	41.6	38.6
Incremental Delay (d ₂), s/veh	0.2	0.9	1.1	0.1	2.8		0.7	27.4		6.6	6.7	0.6
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	19.0	22.6	22.9	16.7	27.6		34.4	72.8		40.1	48.2	39.2
Level of Service (LOS)	B	C	C	B	C		C	E		D	D	D
Approach Delay, s/veh / LOS	21.8		C	24.5		C	62.6		E	43.7		D
Intersection Delay, s/veh / LOS	37.8						D					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.3		B	2.4		B	2.3		B	2.5		B
Bicycle LOS Score / LOS	1.5		A	1.4		A	1.3		A	1.8		A

**Westchester Avenue
at
Traverse Avenue**

ALL-WAY STOP CONTROL ANALYSIS								
General Information					Site Information			
Analyst	CC				Intersection	Westchester @ Traverse		
Agency/Co.	DEA				Jurisdiction	Westchester		
Date Performed	2/11/2016				Analysis Year	2016		
Analysis Time Period	Am Existing 2016							
Project ID Westchester Retail								
East/West Street: Westchester Avenue					North/South Street: Traverse Avenue			
Volume Adjustments and Site Characteristics								
Approach	Eastbound				Westbound			
Movement	L	T	R		L	T	R	
Volume (veh/h)	0	20	65		1	10	0	
%Thrus Left Lane								
Approach	Northbound				Southbound			
Movement	L	T	R		L	T	R	
Volume (veh/h)	49	0	2		0	0	0	
%Thrus Left Lane								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Configuration	TR		LT		LR			
PHF	0.92		0.92		0.90			
Flow Rate (veh/h)	91		11		56			
% Heavy Vehicles	0		0		0			
No. Lanes	1		1		1		0	
Geometry Group	1		1		1			
Duration, T	0.25							
Saturation Headway Adjustment Worksheet								
Prop. Left-Turns	0.0		0.1		1.0			
Prop. Right-Turns	0.8		0.0		0.0			
Prop. Heavy Vehicle	0.0		0.0		0.0			
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2		
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6		
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7		
hadj, computed	-0.5		0.0		0.2			
Departure Headway and Service Time								
hd, initial value (s)	3.20		3.20		3.20			
x, initial	0.08		0.01		0.05			
hd, final value (s)	3.58		4.13		4.27			
x, final value	0.090		0.013		0.066			
Move-up time, m (s)	2.0		2.0		2.0			
Service Time, t _s (s)	1.6		2.1		2.3			
Capacity and Level of Service								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Capacity (veh/h)	1011		1100		800			
Delay (s/veh)	6.9		7.2		7.6			
LOS	A		A		A			
Approach: Delay (s/veh)	6.9		7.2		7.6			
LOS	A		A		A			
Intersection Delay (s/veh)	7.2							
Intersection LOS	A							

ALL-WAY STOP CONTROL ANALYSIS									
General Information					Site Information				
Analyst	CC				Intersection	Westchester @ Traverse			
Agency/Co.	DEA				Jurisdiction	Westchester			
Date Performed	2/11/2016				Analysis Year	2016			
Analysis Time Period	PM Existing 2016								
Project ID Westchester Retail									
East/West Street: Westchester Avenue					North/South Street: Traverse Avenue				
Volume Adjustments and Site Characteristics									
Approach	Eastbound				Westbound				
Movement	L	T	R	L	T	R			
Volume (veh/h)	0	274	57	53	272	0			
%Thrus Left Lane									
Approach	Northbound				Southbound				
Movement	L	T	R	L	T	R			
Volume (veh/h)	81	0	44	0	0	0			
%Thrus Left Lane									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Configuration	TR		LT		LR				
PHF	0.92		0.92		0.90				
Flow Rate (veh/h)	358		352		138				
% Heavy Vehicles	0		0		0				
No. Lanes	1		1		1		0		
Geometry Group	1		1		1				
Duration, T	0.25								
Saturation Headway Adjustment Worksheet									
Prop. Left-Turns	0.0		0.2		0.7				
Prop. Right-Turns	0.2		0.0		0.3				
Prop. Heavy Vehicle	0.0		0.0		0.0				
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2			
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6			
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7			
hadj, computed	-0.1		0.0		-0.1				
Departure Headway and Service Time									
hd, initial value (s)	3.20		3.20		3.20				
x, initial	0.32		0.31		0.12				
hd, final value (s)	4.64		4.77		5.47				
x, final value	0.461		0.466		0.210				
Move-up time, m (s)	2.0		2.0		2.0				
Service Time, t _s (s)	2.6		2.8		3.5				
Capacity and Level of Service									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Capacity (veh/h)	778		749		657				
Delay (s/veh)	11.5		11.9		9.9				
LOS	B		B		A				
Approach: Delay (s/veh)	11.5		11.9		9.9				
LOS	B		B		A				
Intersection Delay (s/veh)	11.4								
Intersection LOS	B								

ALL-WAY STOP CONTROL ANALYSIS									
General Information					Site Information				
Analyst	CC				Intersection	Westchester @ Traverse			
Agency/Co.	DEA				Jurisdiction	Westchester			
Date Performed	2/11/2016				Analysis Year	2016			
Analysis Time Period	SAT Existing 2016								
Project ID Westchester Retail									
East/West Street: Westchester Avenue					North/South Street: Traverse Avenue				
Volume Adjustments and Site Characteristics									
Approach	Eastbound				Westbound				
Movement	L	T	R	L	T	R			
Volume (veh/h)	0	401	93	86	406	0			
%Thrus Left Lane									
Approach	Northbound				Southbound				
Movement	L	T	R	L	T	R			
Volume (veh/h)	107	0	94	0	0	0			
%Thrus Left Lane									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Configuration	TR		LT		LR				
PHF	0.92		0.92		0.90				
Flow Rate (veh/h)	536		534		222				
% Heavy Vehicles	0		0		0				
No. Lanes	1		1		1		0		
Geometry Group	1		1		1				
Duration, T	0.25								
Saturation Headway Adjustment Worksheet									
Prop. Left-Turns	0.0		0.2		0.5				
Prop. Right-Turns	0.2		0.0		0.5				
Prop. Heavy Vehicle	0.0		0.0		0.0				
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2			
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6			
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7			
hadj, computed	-0.1		0.0		-0.2				
Departure Headway and Service Time									
hd, initial value (s)	3.20		3.20		3.20				
x, initial	0.48		0.47		0.20				
hd, final value (s)	5.38		5.52		6.42				
x, final value	0.801		0.818		0.396				
Move-up time, m (s)	2.0		2.0		2.0				
Service Time, t _s (s)	3.4		3.5		4.4				
Capacity and Level of Service									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Capacity (veh/h)	670		651		555				
Delay (s/veh)	26.4		28.5		13.6				
LOS	D		D		B				
Approach: Delay (s/veh)	26.4		28.5		13.6				
LOS	D		D		B				
Intersection Delay (s/veh)	25.1								
Intersection LOS	D								

ALL-WAY STOP CONTROL ANALYSIS								
General Information					Site Information			
Analyst	CC				Intersection	Westchester @ Traverse		
Agency/Co.	DEA				Jurisdiction	Westchester		
Date Performed	2/11/2016				Analysis Year	2017		
Analysis Time Period	AM No Build							
Project ID Westchester Retail								
East/West Street: Westchester Avenue					North/South Street: Traverse Avenue			
Volume Adjustments and Site Characteristics								
Approach	Eastbound				Westbound			
Movement	L	T	R		L	T	R	
Volume (veh/h)	0	21	66		1	10	0	
%Thrus Left Lane								
Approach	Northbound				Southbound			
Movement	L	T	R		L	T	R	
Volume (veh/h)	50	0	2		0	0	0	
%Thrus Left Lane								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Configuration	TR		LT		LR			
PHF	0.92		0.92		0.90			
Flow Rate (veh/h)	93		11		57			
% Heavy Vehicles	0		0		0			
No. Lanes	1		1		1		0	
Geometry Group	1		1		1			
Duration, T	0.25							
Saturation Headway Adjustment Worksheet								
Prop. Left-Turns	0.0		0.1		1.0			
Prop. Right-Turns	0.8		0.0		0.0			
Prop. Heavy Vehicle	0.0		0.0		0.0			
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2		
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6		
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7		
hadj, computed	-0.5		0.0		0.2			
Departure Headway and Service Time								
hd, initial value (s)	3.20		3.20		3.20			
x, initial	0.08		0.01		0.05			
hd, final value (s)	3.58		4.13		4.27			
x, final value	0.093		0.013		0.068			
Move-up time, m (s)	2.0		2.0		2.0			
Service Time, t _s (s)	1.6		2.1		2.3			
Capacity and Level of Service								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Capacity (veh/h)	1033		1100		814			
Delay (s/veh)	7.0		7.2		7.6			
LOS	A		A		A			
Approach: Delay (s/veh)	7.0		7.2		7.6			
LOS	A		A		A			
Intersection Delay (s/veh)	7.2							
Intersection LOS	A							

ALL-WAY STOP CONTROL ANALYSIS								
General Information				Site Information				
Analyst	CC			Intersection	Westchester @ Traverse			
Agency/Co.	DEA			Jurisdiction	Westchester			
Date Performed	2/11/2016			Analysis Year	2017			
Analysis Time Period	PM No Build							
Project ID <i>Westchester Retail</i>								
East/West Street: <i>Westchester Avenue</i>				North/South Street: <i>Traverse Avenue</i>				
Volume Adjustments and Site Characteristics								
Approach	Eastbound				Westbound			
Movement	L	T	R	L	T	R		
Volume (veh/h)	0	59	279	54	277	0		
%Thrus Left Lane								
Approach	Northbound				Southbound			
Movement	L	T	R	L	T	R		
Volume (veh/h)	82	0	49	0	0	0		
%Thrus Left Lane								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Configuration	TR		LT		LR			
PHF	0.92		0.92		0.90			
Flow Rate (veh/h)	367		359		145			
% Heavy Vehicles	0		0		0			
No. Lanes	1		1		1		0	
Geometry Group	1		1		1			
Duration, T	0.25							
Saturation Headway Adjustment Worksheet								
Prop. Left-Turns	0.0		0.2		0.6			
Prop. Right-Turns	0.8		0.0		0.4			
Prop. Heavy Vehicle	0.0		0.0		0.0			
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2		
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6		
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7		
hadj, computed	-0.5		0.0		-0.1			
Departure Headway and Service Time								
hd, initial value (s)	3.20		3.20		3.20			
x, initial	0.33		0.32		0.13			
hd, final value (s)	4.27		4.76		5.42			
x, final value	0.435		0.475		0.218			
Move-up time, m (s)	2.0		2.0		2.0			
Service Time, t _s (s)	2.3		2.8		3.4			
Capacity and Level of Service								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Capacity (veh/h)	853		764		659			
Delay (s/veh)	10.5		12.0		9.9			
LOS	B		B		A			
Approach: Delay (s/veh)	10.5		12.0		9.9			
LOS	B		B		A			
Intersection Delay (s/veh)	11.0							
Intersection LOS	B							

ALL-WAY STOP CONTROL ANALYSIS								
General Information				Site Information				
Analyst	CC			Intersection	Westchester @ Traverse			
Agency/Co.	DEA			Jurisdiction	Westchester			
Date Performed	2/11/2016			Analysis Year	2017			
Analysis Time Period	SAT No Build							
Project ID Westchester Retail								
East/West Street: Westchester Avenue				North/South Street: Traverse Avenue				
Volume Adjustments and Site Characteristics								
Approach	Eastbound				Westbound			
Movement	L	T	R	L	T	R		
Volume (veh/h)	0	95	409	88	414	0		
% Thrus Left Lane								
Approach	Northbound				Southbound			
Movement	L	T	R	L	T	R		
Volume (veh/h)	109	0	96	0	0	0		
% Thrus Left Lane								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Configuration	TR		LT		LR			
PHF	0.92		0.92		0.90			
Flow Rate (veh/h)	547		544		227			
% Heavy Vehicles	0		0		0			
No. Lanes	1		1		1		0	
Geometry Group	1		1		1			
Duration, T	0.25							
Saturation Headway Adjustment Worksheet								
Prop. Left-Turns	0.0		0.2		0.5			
Prop. Right-Turns	0.8		0.0		0.5			
Prop. Heavy Vehicle	0.0		0.0		0.0			
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2		
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6		
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7		
hadj, computed	-0.5		0.0		-0.2			
Departure Headway and Service Time								
hd, initial value (s)	3.20		3.20		3.20			
x, initial	0.49		0.48		0.20			
hd, final value (s)	5.03		5.49		6.39			
x, final value	0.764		0.830		0.403			
Move-up time, m (s)	2.0		2.0		2.0			
Service Time, t_s (s)	3.0		3.5		4.4			
Capacity and Level of Service								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Capacity (veh/h)	720		655		568			
Delay (s/veh)	22.4		29.5		13.6			
LOS	C		D		B			
Approach: Delay (s/veh)	22.4		29.5		13.6			
LOS	C		D		B			
Intersection Delay (s/veh)	23.8							
Intersection LOS	C							

NOTE: SINCE NO SITE GENERATED TRAFFIC IS PROJECTED TO TRAVEL THROUGH THIS INTERSECTION, BUILD ANALYSIS IS SAME AS NO BUILD FOR AM, PM & SATURDAY

ALL-WAY STOP CONTROL ANALYSIS

General Information		Site Information	
Analyst	CC	Intersection	Westchester @ Traverse
Agency/Co.	DEA	Jurisdiction	Westchester
Date Performed	2/11/2016	Analysis Year	2017
Analysis Time Period	AM BUILD		

Project ID *Westchester Retail*

East/West Street: *Westchester Avenue* North/South Street: *Traverse Avenue*

Volume Adjustments and Site Characteristics						
Approach	Eastbound			Westbound		
	L	T	R	L	T	R
Movement						
Volume (veh/h)	0	21	66	1	10	0
%Thrus Left Lane						
Approach	Northbound			Southbound		
	L	T	R	L	T	R
Movement						
Volume (veh/h)	50	0	2	0	0	0
%Thrus Left Lane						

	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Configuration	TR		LT		LR			
PHF	0.92		0.92		0.90			
Flow Rate (veh/h)	93		11		57			
% Heavy Vehicles	0		0		0			
No. Lanes	1		1		1		0	
Geometry Group	1		1		1			
Duration, T	0.25							

Saturation Headway Adjustment Worksheet								
Prop. Left-Turns	0.0		0.1		1.0			
Prop. Right-Turns	0.8		0.0		0.0			
Prop. Heavy Vehicle	0.0		0.0		0.0			
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2		
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6		
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7		
nadj, computed	-0.5		0.0		0.2			

Departure Headway and Service Time								
hd, initial value (s)	3.20		3.20		3.20			
x, initial	0.08		0.01		0.05			
hd, final value (s)	3.58		4.13		4.27			
x, final value	0.093		0.013		0.068			
Move-up time, m (s)	2.0		2.0		2.0			
Service Time, t _s (s)	1.6		2.1		2.3			

Capacity and Level of Service								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Capacity (veh/h)	1033		1100		814			
Delay (s/veh)	7.0		7.2		7.6			
LOS	A		A		A			
Approach: Delay (s/veh)	7.0		7.2		7.6			
LOS	A		A		A			
Intersection Delay (s/veh)	7.2							
Intersection LOS	A							

ALL-WAY STOP CONTROL ANALYSIS									
General Information					Site Information				
Analyst	CC				Intersection	Westchester @ Traverse			
Agency/Co.	DEA				Jurisdiction	Westchester			
Date Performed	2/11/2016				Analysis Year	2017			
Analysis Time Period									
Project ID <i>Westchester Retail</i>									
East/West Street: <i>Westchester Avenue</i>					North/South Street: <i>Traverse Avenue</i>				
Volume Adjustments and Site Characteristics									
Approach	Eastbound				Westbound				
Movement	L	T	R		L	T	R		
Volume (veh/h)	0	59	279		54	277	0		
%Thrus Left Lane									
Approach	Northbound				Southbound				
Movement	L	T	R		L	T	R		
Volume (veh/h)	82	0	49		0	0	0		
%Thrus Left Lane									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Configuration	TR		LT		LR				
PHF	0.92		0.92		0.90				
Flow Rate (veh/h)	367		359		145				
% Heavy Vehicles	0		0		0				
No. Lanes	1		1		1		0		
Geometry Group	1		1		1				
Duration, T	0.25								
Saturation Headway Adjustment Worksheet									
Prop. Left-Turns	0.0		0.2		0.6				
Prop. Right-Turns	0.8		0.0		0.4				
Prop. Heavy Vehicle	0.0		0.0		0.0				
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2			
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6			
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7			
hadj, computed	-0.5		0.0		-0.1				
Departure Headway and Service Time									
hd, initial value (s)	3.20		3.20		3.20				
x, initial	0.33		0.32		0.13				
hd, final value (s)	4.27		4.76		5.42				
x, final value	0.435		0.475		0.218				
Move-up time, m (s)	2.0		2.0		2.0				
Service Time, t _s (s)	2.3		2.8		3.4				
Capacity and Level of Service									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Capacity (veh/h)	853		764		659				
Delay (s/veh)	10.5		12.0		9.9				
LOS	B		B		A				
Approach: Delay (s/veh)	10.5		12.0		9.9				
LOS	B		B		A				
Intersection Delay (s/veh)	11.0								
Intersection LOS	B								

ALL-WAY STOP CONTROL ANALYSIS									
General Information					Site Information				
Analyst	CC				Intersection	Westchester @ Traverse			
Agency/Co.	DEA				Jurisdiction	Westchester			
Date Performed	2/11/2016				Analysis Year	2017			
Analysis Time Period	Sat Build								
Project ID <i>Westchester Retail</i>									
East/West Street: <i>Westchester Avenue</i>					North/South Street: <i>Traverse Avenue</i>				
Volume Adjustments and Site Characteristics									
Approach	Eastbound			Westbound					
Movement	L	T	R	L	T	R			
Volume (veh/h)	0	95	409	88	414	0			
%Thrus Left Lane									
Approach	Northbound			Southbound					
Movement	L	T	R	L	T	R			
Volume (veh/h)	109	0	96	0	0	0			
%Thrus Left Lane									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Configuration	TR		LT		LR				
PHF	0.92		0.92		0.90				
Flow Rate (veh/h)	547		544		227				
% Heavy Vehicles	0		0		0				
No. Lanes	1		1		1		0		
Geometry Group	1		1		1				
Duration, T	0.25								
Saturation Headway Adjustment Worksheet									
Prop. Left-Turns	0.0		0.2		0.5				
Prop. Right-Turns	0.8		0.0		0.5				
Prop. Heavy Vehicle	0.0		0.0		0.0				
hLT-adj	0.2	0.2	0.2	0.2	0.2	0.2			
hRT-adj	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6			
hHV-adj	1.7	1.7	1.7	1.7	1.7	1.7			
hadj, computed	-0.5		0.0		-0.2				
Departure Headway and Service Time									
hd, initial value (s)	3.20		3.20		3.20				
x, initial	0.49		0.48		0.20				
hd, final value (s)	5.03		5.49		6.39				
x, final value	0.764		0.830		0.403				
Move-up time, m (s)	2.0		2.0		2.0				
Service Time, t _s (s)	3.0		3.5		4.4				
Capacity and Level of Service									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Capacity (veh/h)	720		655		568				
Delay (s/veh)	22.4		29.5		13.6				
LOS	C		D		B				
Approach: Delay (s/veh)	22.4		29.5		13.6				
LOS	C		D		B				
Intersection Delay (s/veh)	23.8								
Intersection LOS	C								

**ADLER CONSULTING REVIEW OF 2016 DUNN TRAFFIC
STUDY**

Adler Consulting,
235 Main Street
White Plains, NY 10601-2401

Transportation Planning & Traffic Engineering, PLLC

Tel # (914) 997-8510
Fax # (914) 997-7140

February 16, 2016

Eric Zamft, AICP
Director of Planning and Development
Village of Port Chester
222 Grace Church Street, Suite 100
Port Chester, NY 10573

Re: Proposed Change of Zone Petition
Mixed-Use Building
Retail "D"
Village of Port Chester, NY

Dear Mr. Zamft:

In response to your request, **Adler Consulting** has reviewed the Traffic Letter Report, concerning "Retail D" dated, February 12, 2015, prepared by Dunn Engineering Associates, P.C. for G&S Port Chester LLC for the above subject application. The following are our findings:

A. GENERAL

At its Meeting on February 1, 2016, the Board of Trustees of the Village of Port Chester required that the Applicant provide additional traffic and parking data and prepare analyses for the three critical intersections in the immediate vicinity of the Project for the anticipated 2017 traffic conditions. In response to those requirements, the Applicant's traffic engineer:

- Collected 2016 turning movement count data for the AM Peak Hour;
- Grew the traffic volumes for the PM Peak Hour and the Saturday Peak Hour based on available data from 2011 to the year 2016 using an annual growth rate of two (2) percent;

- Grew the 2016 traffic volumes to the Horizon Year of 2017 using an annual growth rate of two (2) percent;
- Developed directional arrival and departure patterns of the anticipated trip generation volumes;
- Used the agreed-upon trip distribution for the traffic volumes expected to be generated by the project and assigned these traffic volumes to the roadway network;
- Established 2017 Build traffic volumes;
- Performed capacity analyses for the 2017 Build traffic volumes;
- Prepared Level-of-Service Summary Tables; and,
- Collected Parking data for specific portions of the garage area for a Saturday and Weekday.

B. ANALYSIS

The Applicant's traffic engineer collected the required traffic and parking data and grew the traffic volume data to represent the 2017 Build conditions. The capacity analyses submitted by the traffic engineer indicate that the intersections studied would be expected to operate at marginally acceptable Levels-of-Service "D" or better during the weekday AM, PM and Saturday Peak Hours. It is noted that Level-of-Service "F" conditions are anticipated on the northbound Main Street approach at the intersection with Westchester Avenue in the weekday PM Peak and Saturday Peak Hours during both No-Build and Build conditions.

The inventory of the parking spaces in the garage indicated that there are 131 parking spaces currently available in the designated parking area on the first floor, 350 parking spaces in the Metropolitan Transportation Authority (MTA) area on the second floor and 50 parking spaces in the other, non-MTA area, on the second floor. The parking data collected in the garage on a weekday indicated that a minimum of approximately 54 parking spaces are available on the first floor, approximately 94 parking spaces are available in the Metropolitan Transportation Authority (MTA)

area on the second floor and approximately 35 parking spaces are available in the non-MTA parking area on the second floor. A total of approximately 183 parking spaces are available on a weeknight when the proposed residential component peaks. These spaces are sufficient to accommodate an anticipated demand of approximately 127 parking spaces. The parking demand was calculated based on approximately four (4) parking spaces per 1,000 square feet of retail space or 48 parking spaces for the retail portion and one (1) space for each of the 79 residential units.

The data collected on Saturday indicated that a minimum of approximately 100 parking spaces are currently available on the first floor, approximately 282 parking spaces are available in the Metropolitan Transportation Authority (MTA) area on the second floor and approximately 48 parking spaces are available in the non-MTA area on the second floor. A total of approximately 430 parking spaces are available on Saturday. These parking spaces are sufficient to accommodate the anticipated demand.

C. SUMMARY

A review of the capacity analyses submitted by the Applicant indicated that the proposed project would not have a significant impact on the operations of the intersections in the immediate vicinity of the proposed project. A review of the parking accumulation data collected indicated that sufficient parking spaces would be available to accommodate the anticipated parking demand for the project.

We trust that this information will assist you with your continued review of this project. Should you have any questions, please do not hesitate to contact us.

Sincerely,
Adler Consulting,
Transportation Planning & Traffic Engineering, PLLC



Bernard Adler, P.E.
President
NY Professional Engineer No. 048373



Michael P. O'Rourke, P.E., PTOE
Senior Associate
NY Professional Engineer No. 061375

NYSDOT LETTER



Department of
Transportation

ANDREW M. CUOMO
Governor

MATTHEW J. DRISCOLL
Commissioner

TODD WESTHUIS, P.E.
Acting Regional Director

February 16, 2016

Jesica Youngblood
Village of Port Chester
Department of Planning & Development
222 Grace Church St, Suite 202
Port Chester, NY 10573

RECEIVED
Village of Port Chester

FEB 19 2016

Planning and Zoning

Re: G&S Port Chester, LLC
"Retail D" development proposal

Ms. Youngblood:

I reviewed the submission from the Village dated January 12, 2016 regarding the subject proposal and offer the following comments and questions:

- The site fronts NYSDOT highways on the west and south sides. The plan indicates that no parking will be provided at the site, and thus no driveway will be required. However, a NYSDOT highway work permit will still be required for any other improvements to the state highway ROW. As the parcel has no sidewalk, NYSDOT recommends sidewalk around the entire site to connect to existing sections and crosswalk on adjacent parcels.
- The Traffic study mentions that the impact will be less than the original permitted use. Were all required mitigations and/or improvements from the prior approval completed?

Thank you for the opportunity to review this proposal.

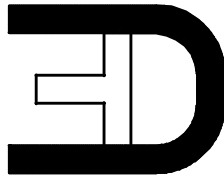
Sincerely,

Chris Lee
Permit Engineer

For Jorge Argote
Resident Engineer
Westchester County South

Cc: File

DUNN ENGINEERING RESPONSE TO NYSDOT LETTER



Dunn Engineering Associates, P.C.

Consulting Engineers

66 Main Street

Westhampton Beach, N.Y. 11978

631-288-2480

631-288-2544 Fax

February 24, 2016

Daniel Tartaglia, Esq.
800 Westchester Avenue
Rye Brook, New York 10573

Re: G&S Port Chester – Retail “D” Petition
Potential Pedestrian Impacts of Less Intensive Use

Dear Mr. Tartaglia:

This letter is in response to recent additional comments from the Village of Port Chester on the traffic and pedestrian impacts of the proposed re-zoning of the “Retail D” site from “all retail” to a mixed use of retail and residential.

First, the Village has asked for comments on a letter received from the New York State Department of Transportation (“NYSDOT”) dated February 16, 2016. NYSDOT should be advised that the proposed development would have standard width sidewalks on the three sides of the site which have frontage on public ways, including the portion of the site adjacent to ways under the control of NYSDOT (east side of U.S. 1, Main Street). The NYSDOT letter correctly observes that no curb cut is proposed on the State Highway, which conforms to the Access Management Plan of the NYSDOT. The petitioner nonetheless acknowledges that a NYSDOT permit may still be required for any ancillary project construction that is adjacent to the State Highway. NYSDOT should also be advised that all prior required roadway mitigation for this location was fully performed by petitioner, including road widenings, signalization improvements, crosswalk installation, pedestrian signal controls, re-paving, signs and pavement markings.

Secondly, as regards minor “increases” in pedestrian volumes, we are of the opinion that the pedestrian improvements previously made by petitioner are more than adequate to accommodate pedestrians destined to and from the site for the proposed change of use. These concerns were carefully considered by the NYSDOT, the Village consultants and the County of Westchester, as part of the overall original traffic engineering examination that covered 23 intersections and numerous parcels, of which this site is one. Furthermore, it should be noted that the previously approved use of all retail uses on this site included a higher generation of traffic and a higher generation of pedestrian activity. Thus, we believe that no further work would be required in order to make the site safe and practical for use by pedestrians using the sidewalks and crosswalks in the vicinity of the proposed less intensive project.

Daniel Tartaglia, Esq.
February 24, 2016
Page 2

The previously implemented pedestrian improvements include:

- A minimum of five feet wide sidewalks adjacent to the site frontages on a) the east side of U.S. 1, Main Street, b) the north side of Westchester Avenue, and c) the west side of Abendroth Avenue.
- Pedestrian crosswalks at the signalized intersections of Westchester Avenue at 1) Main Street, and 2) Abendroth Avenue/Don Bosco Place, as well as at the unsignalized intersection of Westchester Avenue at Traverse Street.
- Modernization of Traffic Signal installation at Main Street at Westchester Avenue with pedestrian signals and handicap ramps.
- New traffic signal installation on Westchester Avenue at Abendroth Avenue/Don Bosco Place with pedestrian signals and handicap ramps.

As a result of our traffic engineering examination, and the previously implemented mitigating improvements, there will not be any negative impact on traffic or pedestrians due to the proposed change of uses on Retail D.

Thank you for your attention to this matter.

Sincerely,



WALTER M. DUNN, JR., P.E.
President

WMD:as
L2016023
P23061

ADLER CONSULTING LETTER REGARDING PEDESTRIANS

February 26, 2016

Eric Zamft, AICP
Director of Planning and Development
Village of Port Chester
222 Grace Church Street, Suite 100
Port Chester, NY 10573

Re: Proposed Change of Zone Petition
Mixed-Use Building
Retail "D"
Village of Port Chester, NY

Dear Mr. Zamft:

In response to your request, **Adler Consulting** has assessed the potential increase in the number of pedestrians that may occur in the vicinity of "Retail D" project. The following are our findings:

A pedestrian analysis indicated that approximately 233 new pedestrians would be generated by the retail portion of "Retail D" during the weekday midday hour. However, since the "Retail D" portion is relatively small compared to the overall retail space in the area representing about two (2) percent of the total retail area, it was assumed that only 25 percent of the number of pedestrians generated by the space would be new trips and that the majority of the trips would be 'pass-by' pedestrian trips with pedestrians already in the area who would enter the new retail facility. It was, therefore, assumed that the retail space would generate approximately 59 new pedestrians during the weekday midday hour.

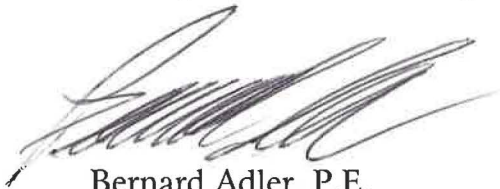
For the residential space, it was calculated that the 79 dwelling units would generate approximately 16 new pedestrians during the weekday midday hour.

Eric Zamft, AICP
February 26, 2016
Page 2

Therefore, it is anticipated that there would be approximately 75 additional pedestrians due to "Retail D" in the weekday midday hour. It is anticipated that there would be no impact on intersection operations due to the minimal increase in the number of pedestrians in the area.

We trust that this information will assist you with your continued review of this project. Should you have any questions, please do not hesitate to contact us.

Sincerely,
Adler Consulting,
Transportation Planning & Traffic Engineering, PLLC



Bernard Adler, P.E.
President
NY Professional Engineer No. 048373



Michael P. O'Rourke, P.E., PTOE
Senior Associate
NY Professional Engineer No. 061375

PROPOSED LOCAL LAW NO. 2 OF 2016

LOCAL LAW NO. 02 of 2016

A LOCAL LAW AMENDING THE OFFICIAL ZONING MAP AND THE TEXT OF CHAPTER 345 OF THE VILLAGE CODE, MUR MARINA REDEVELOPMENT PROJECT RENEWAL DISTRICT AND CONCEPT DEVELOPMENT PLAN OF THE MODIFIED MARINA REDEVELOPMENT PROJECT TO PERMIT MULTI-FAMILY RESIDENTIAL DEVELOPMENT IN A PORTION OF THE PROJECT AREA KNOWN AND DESIGNATED AS RETAIL "D" AND MORE FULLY DESCRIBED AS SECTION 142.31, BLOCK 1, LOTS 3, 4, 5, 6, 20, 21, 22, 23 and 24 ON THE TOWN OF RYE OFFICIAL TAX MAP AND AN INTERSTITIAL RIGHT-OF-WAY SHOWN ON SAID TAX MAP

Be it enacted by the Board of Trustees of the Village of Port Chester, New York as follows:

SECTION 1: The Code of the Village of Port Chester, Article XVI, Section 345-67, is hereby amended to read as follows:

§ 345-67. MUR Marina Redevelopment Project Urban Renewal District

A. Purpose; area covered; zoning objectives; proposed actions.

- (1) Area covered. An Urban Renewal Plan for the Village Center Urban Renewal Plan Area was adopted by the Board of Trustees of the Village of Port Chester, New York, on November 1, 1977. An Urban Renewal Plan for the Marina Redevelopment Urban Renewal Plan Area was adopted by the Board of Trustees of the Village of Port Chester, New York, on June 23, 1982. Each of these Urban Renewal Plans has been amended from time to time, the most recent of such amendments having been approved by the Village Board of Trustees on July 14, 1999, on the recommendation of the Planning Commission. The Urban Renewal Plans, as so amended, are sometimes collectively referred to below as the "Urban Renewal Plans." The Urban Renewal Plan Amendment adopted on July 14, 1999, delineates an area referred to in such amendment as the "Modified Marina Redevelopment Project," consisting of contiguous portions of the Village Center Urban Renewal Plan Area and the Marina Redevelopment Urban Renewal Plan Area. The boundaries of the Modified Marina Redevelopment Project correspond to the boundaries of the zoning district designated on the Zoning Map as the "MUR Marina Redevelopment Project Urban Renewal District."
- (2) Zoning objectives. The objectives of the MUR Marina Redevelopment Project Urban Renewal District are to provide for comprehensive planning of redevelopment within the MUR Marina Redevelopment Project Urban Renewal District and to encourage and permit the development of uses which will result in or accomplish the following:
 - (a) Eliminate substandard, blighted, deteriorated and deteriorating conditions within the MUR Marina Redevelopment Project Urban Renewal District.
 - (b) Attract new businesses, create employment opportunities, generate additional tax revenues and further private investment.
 - (c) ~~Develop new commercial~~ Encourage a mix of uses, including but not limited to retail, wholesale membership warehouse club, multiscreen cinema theater, ~~and~~ supermarket uses, and residential uses, within and in the vicinity of the MUR Marina Redevelopment Project Urban Renewal District.

Residential uses are permitted only in Land Use Area 2 as referenced in §345-68, titled "Map of Land Use Areas and Streets Within the Modified Marina Redevelopment Project."

- (d) Preserve and enhance property values within and in the vicinity of the MUR Marina Redevelopment Project Urban Renewal District and provide for protection of neighboring areas from any adverse impacts of redevelopment within such district.
 - (e) Provide parking facilities necessary to serve new uses either through the creation of new facilities, the reconfiguration or upgrading of existing facilities or the sharing of facilities.
 - (f) Enhance public access to the waterfront and retain public boating opportunities along the Byram River.
 - (g) Upgrade and provide all utilities necessary to support redevelopment.
 - (h) Improve vehicular access in the area through the realignment and reconstruction of certain streets so that both cars and pedestrians can travel safely.
- (3) Underlying regulations. The land use and development controls set forth in Subsection B below supersede, as provided in said Subsection B below, the controls set forth in this Zoning Regulation for those portions of the C-2 Central Business, DW Design Waterfront Development, M-1 Light Industrial and PTD Planned Tower Development Zoning Districts which are within the MUR Marina Redevelopment Project Urban Renewal District, as well as the controls set forth in §§ 345-14 and 345-15 hereof (all hereinafter referred to as the "underlying controls"). Except during any period in which a special permit is in effect pursuant to Subsection B below in this § 345-67, the underlying controls shall remain in effect for the portions of said zoning districts situated within the MUR Marina Redevelopment Project Urban Renewal District.

B. Land use and development controls.

- (1) Permitted land uses and development controls. Set forth in this Subsection B are land use and development controls for the MUR Marina Redevelopment Project Urban Renewal District. The controls set forth in this Subsection B will, during any period in which a special permit is granted and remains valid and in full force and effect under said Subsection B(4), replace and supersede all of the underlying controls. The land use areas referred to below in this Subsection B are shown on the map referenced in § 345-68, which map is titled "Map of Land Use Areas and Streets Within the Modified Marina Redevelopment Project."

(a) Land Use Area 1.

[1] Permitted land uses:

- [a] Retail sales, including supermarket with customary accessory uses, retail and personal services or restaurant types of uses, general and professional offices and uses customarily accessory to each of the foregoing uses, parking and loading and open space uses.
- [b] Multiscreen cinema theater, theaters and accessory uses customarily found in such a complex.

[2] Permitted development: 275,000 square feet of building floor area.

[3] Permitted height: three stories; 70 feet.

(b) Land Use Area 2.

[1] Permitted land uses:

[a] the same as Land Use Area 1, except for multiscreen cinema theater, theaters and supermarket.

[b] Multi-family dwellings.

[2] ~~Permitted development: 40,000 square feet of building floor area.~~ Maximum floor-area-ratio (FAR): 3.7.

[a] Bonus FAR program. Additional development potential in the form of bonus FAR can be earned for new development in the MUR District Land Use Area 2 and can achieve up to a maximum of 0.3 floor area ratio above the maximum FAR as defined in §345-67(B)(1)(b)[2].

[b] The assessed value of the bonus FAR shall be determined by the Town Assessor. The Village Board of Trustees may set a bonus program payment at a minimum of 15% of the assessed bonus FAR.

[c] The Village Board of Trustees shall appropriate the bonus program payment into one or more of the three established funds as described in §345-16 for open space, housing rehabilitation, or a downtown public parking garage. All three of these funds shall be kept by the Village as dedicated funds in a separate account to be used only for their respective purposes as specified in §345-16 (C)(6).

[3] Permitted height: ~~three stories; 45 feet~~ five stories; 70 feet.

[4] Useable open space: A minimum requirement for useable open space is 25 square feet per dwelling unit.

(c) Land Use Area 3.

[1] Permitted land uses:

[a] Retail sales, retail, wholesale membership warehouse club, personal services and restaurant uses, neighborhood facilities, general and professional offices, as well as uses customarily accessory to each of the foregoing uses.

[b] Multiscreen cinema theater, theaters and accessory uses customarily found in such a complex.

[c] Parking and loading and open space uses.

[2] Permitted development: 275,000 square feet of building floor area.

[3] Permitted height: two stories; 65 feet.

(d) Land Use Area 3A.

[1] Permitted land uses:

[a] Same as Land Use Area 3.

[b] Open storage of materials or equipment and uses customarily accessory thereto.

[2] Permitted development: an aggregate of 275,000 square feet of building floor area, including development on Land Use Area 3.

[3] Permitted height: same as Land Use Area 3.

(e) Land Use Area 4.

[1] Permitted land uses:

[a] General and professional offices and uses customarily accessory thereto.

[b] Retail and personal services uses, restaurant and customary accessory uses.

[c] Parking and loading and open space uses.

[2] Permitted development: 80,000 square feet of building floor area.

[3] Permitted height: three stories; 45 feet.

(f) Land Use Area 5.

[1] Permitted land uses: open space, park and water-related recreation uses which shall be accessible to the general public.

(2) Concept Development Plan. Any special permit issued under Subsection B(4) below shall require compliance with the land use and development controls set forth in this Subsection B and with the Amended Concept Development Plan dated May 20, 1999, which map, together with such land use and development controls, set forth the proposed general building layout and disposition of uses, general parking area locations, access, general circulation, densities, number of stories and/or height and public open space. Such map is on file in the office of the Village Clerk and is hereinafter referred to as the "Concept Development Plan."

- (3) Other development controls and prohibited uses. There shall become effective with respect to the entire MUR Marina Redevelopment Project Urban Renewal District, upon the approval of a special permit under Subsection B(4) below and upon the satisfaction of all conditions of the effectiveness of such special permit set forth in this section, the following additional land use and development controls:
- (a) For Land Use Areas 3 and 4, there shall be a continuous pedestrian promenade on the waterfront side of these parcels. The width of the promenade shall be an average minimum of 10 feet and an average width of 30 feet from the bulkhead.
 - (b) For Land Use Areas 3 and 4, the building setback on the waterfront side of the parcels shall be a minimum of 15 feet on average, and may be staggered.
 - (c) Unless paved, all open areas which are visible from the street shall be landscaped.
 - (d) All buildings shall be consistent with the New York State Uniform Fire Prevention and Building Code requirements for handicapped access. All streets and sidewalks shall provide access for the handicapped.
 - (e) No use permitted herein shall be noxious or offensive by reason of emission of odor, dust, noise, smoke, gas, fumes or radiation or which in any other way presents a nuisance and hazard to public health and safety.
 - (f) There shall be no restriction of occupancy or use of any part of the MUR Marina Redevelopment Project on the basis of race, creed, color, sex or national origin.
 - (g) The requirements for off-street parking for uses in the Marina Redevelopment Project Urban Renewal District shall be as follows:
 - [1] Parking for all uses shall be provided in a combination of on-street and off-street parking facilities. At least 85% of the parking required for uses in the MUR Marina Redevelopment Project Urban Renewal District, considered on an overall basis, shall be located within the boundaries of the MUR Marina Redevelopment Project Urban Renewal District. The Board of Trustees shall, upon the request of the applicant for the special permit, approve up to 15% of the total number of parking spaces required on an overall basis for all uses in the MUR Marina Redevelopment Project Urban Renewal District, to be located outside of the district boundaries, subject to demonstration, by the applicant for the special permit, of adequate control over such spaces and that all such spaces are within 650 feet of the uses they are intended to serve.
 - [2] Minimum parking spaces.
 - [a] The minimum number of parking spaces to be provided for each individual category of use in the proposed development shall be computed in accordance with the criteria noted below, and the aggregate number of spaces available to all of the uses at any specific point in time shall be sufficient to meet the combined parking requirements of these uses at such specific point in time, less a reduction for multipurpose trips. The reduction for multipurpose trips shall be computed only when parking demand is generated by two or more categories of nonresidential use and shall not exceed the smaller of the following two numbers:

Midday	0.5 per 1,000 square feet of gross floor area
9:00 a.m.	0.4 per 1,000 square feet of gross floor area
9:00 p.m.	0.01 per 1,000 square feet of gross floor area
12:00 midnight	0.01 per 1,000 square feet of gross floor area

Boat slips

Weekdays

Monday through Friday

6:00 p.m.	0.23 per slip
9:00 p.m.	0.23 per slip
9:00 a.m.	0.08 per slip
Midday	0.08 per slip
12:00 midnight	0.02 per slip

Weekends

Saturday

9:00 a.m.	0.5 per slip
Midday	0.5 per slip
9:00 p.m.	0.23 per slip
12:00 midnight	0.02 per slip

Multifamily Dwellings

Weekdays

Monday through Friday

<u>9:00 a.m.</u>	<u>0.75 per 1,000 square feet of floor area</u>
<u>12:00 p.m.</u>	<u>0.75 per 1,000 square feet of floor area</u>
<u>6:00 p.m.</u>	<u>1.0 per 1,000 square feet of floor area</u>
<u>9:00 p.m.</u>	<u>1.25 per 1,000 square feet of floor area</u>

Weekends

Saturday and Sunday

<u>9:00 a.m.</u>	<u>0.5 per 1,000 square feet of floor area</u>
<u>12:00 p.m.</u>	<u>0.5 per 1,000 square feet of floor area</u>
<u>6:00 p.m.</u>	<u>1.0 per 1,000 square feet of floor area</u>
<u>9:00 p.m.</u>	<u>1.25 per 1,000 square feet of floor area</u>

- (h) Parking decks, garages, arcades, covered ways, parking areas, fire access, other accessways, street furniture, sculpture and landscaped areas may be developed within the public rights-of-way so long as access to abutting non-project properties is maintained.
- (i) Notwithstanding anything to the contrary in this zoning regulation, the permitted building floor area set forth in Subsection B(1) above shall mean the sum of the total horizontal floor areas within a building which are designed for the exclusive use and occupancy of tenants, measured from the interior faces of exterior walls, not including the areas of:
 - [1] Floor space of cellars.
 - [2] Floor space designed and intended for the parking of motor vehicles of any kind.
 - [3] Elevator shafts.
 - [4] Stairwells and exit corridors.

- [5] Floor space used for mechanical equipment having structural headroom of less than seven feet, six inches.
 - [6] Floor space of mezzanines and other areas not accessible to the general public.
- (4) Special permit for permitted uses.
- (a) Before a building permit may be issued by the Building Inspector for any use permitted under the preceding provisions of this Subsection B, the Village Board of Trustees must adopt, by majority vote, a resolution granting a special permit for such use. The provisions of Article X of this Zoning Regulation shall not be applicable with respect to special permits or to the procedures pertaining thereto authorized and described in this Subsection B or in Subsection C below.
 - (b) An application for such a special permit shall not be made with respect to less than the entire area of the MUR Marina Redevelopment Project Urban Renewal District, and the applicant shall have a reasonable prospect of being able to achieve ownership or control of such area, evidenced by satisfaction of at least one of the following eligibility standards:
 - [1] Ownership of not less than 60% of the total area contained within all land use areas, considered in the aggregate, shown on the map referred to in § 345-68, which map is titled "Map of Land Use Areas and Streets Within the Modified Marina Redevelopment Project," which map is dated March 22, 1999.
 - [2] Designation of the applicant as a qualified and eligible sponsor for the entire Modified Marina Redevelopment Project pursuant to Article 15 of the General Municipal Law, accompanied by an executed development agreement between the applicant and the Village of Port Chester setting forth the manner in which the applicant will proceed in conjunction with such designation.
 - [3] The existence of an executed land disposition agreement between the applicant and the Village Board, approved by the Village Board pursuant to Article 15 of the General Municipal Law, in connection with the Modified Marina Redevelopment Project.
 - (c) An applicant shall apply to the Village Board for a special permit. In deciding whether to approve such application, the Village Board shall consider whether the following preconditions of such approval have been satisfied:
 - [1] The proposed development is consistent with the Concept Development Plan, the Urban Renewal Plans, the land use and development controls set forth in Subsection B above and the purposes of the MUR Marina Redevelopment Project Urban Renewal District set forth in Subsection A above.
 - [2] The potential impacts of the proposed use have been evaluated and mitigated to the maximum extent practicable as set forth in an environmental impact statement completed for the Modified Marina Redevelopment Project in accordance with the New York State Environmental Quality Review Act and as set forth in a statement of findings which the Village Board of Trustees has adopted with respect to such environmental impact statement.

- (d) Any Village Board resolution approving a special permit pursuant to this Subsection B(4) shall require the applicant to implement those environmental impact mitigation measures for which the applicant is responsible as set forth in the above-mentioned statement of findings.
- (e) After the approval of a special permit under Subsection B(4), the successful applicant therefor or any other person, firm or entity who or which meets any of the eligibility standards set forth in Subsection B(4)(b) above is herein below referred to as the "special permit grantee."
- (f) Conditions:
 - [1] Notwithstanding the approval of a special permit by the Village Board under this Subsection B(4), such special permit shall not be effective and a building permit shall not be issued pursuant to such special permit until each of the following conditions has been satisfied:
 - [a] A land disposition agreement has been approved by the Village Board of Trustees and duly executed and delivered in accordance with § 507 of the General Municipal Law.
 - [b] The entire area within the MUR Marina Redevelopment Project Urban Renewal District is owned in fee by the Village of Port Chester, and/or by the Village of Port Chester Industrial Development Agency, provided that in the event of fee ownership of all or any part of such district by such Industrial Development Agency, such ownership is permitted under the land disposition agreement referred to in Subsection B(4)(f)[1][a] above.
 - [2] Upon the satisfaction of each of the conditions set forth above in this Subsection B(4)(f), the restriction on construction activity set forth above in this Subsection B(4)(f) shall automatically terminate, no further Village action being required for such termination. If, after Village Board approval of a special permit under this Subsection B(4) and after satisfaction of the conditions set forth in Subsection B(4)(f)[1][a] and [b] above in this Subsection B(4)(f), the ownership referred to in said Subsection B(4)(f)[1][b] shall cease, such cessation shall not be deemed to cause or require termination of the special permit with respect to any portion of the MUR Marina Redevelopment Project Urban Renewal District.
- (g) In the event that the condition specified in Subsection B(4)(f)[1][a] above is not satisfied within 90 days following the date of Village Board approval of the special permit and if the special permit grantee shall not have negotiated the land disposition agreement in good faith prior to the end of such ninety-day period, or if during the ninety-day period or thereafter the special permit grantee does not negotiate the land disposition agreement in good faith, then the Village Board of Trustees may, upon 15 days' notice to the special permit grantee, terminate the special permit. In the event of such termination, the underlying controls shall remain in effect.
- (h) In connection with the grant of any special permit under this Subsection B(4), the Village Board shall assure that adequate provision is made for the following:
 - [1] Within the MUR Marina Redevelopment Project Urban Renewal District, all of the parcels and structures will be acquired by the Village of Port Chester pursuant to Article 15 of the General Municipal Law of the State of New York and the Urban Renewal Plans.

[2] Redevelopment will include the development of new public infrastructure, including but not limited to streets, sidewalks, curbs, water, sewer and drainage facilities, parking, lighting and other utilities and services, the closing and realignment of portions of public streets, the placement of required fill to raise new buildings above flood levels, development of a new waterfront park at the foot of Westchester Avenue and a new public promenade along the waterfront within the MUR Redevelopment Project Urban Renewal District. Redevelopment within the MUR Marina Redevelopment Project Urban Renewal District also shall include the development of new uses, including commercial, retail, wholesale membership warehouse club, multiscreen cinema theater, restaurant and parking, and may include health clubs and family entertainment centers and general office uses.

[3] Street alignments and configurations shall generally conform to those shown on the following map titled "Map of Land Use Areas and Streets Within the Modified Marina Redevelopment Project" and dated March 22, 1999, which map is on file in the office of the Village Clerk.

(i) If, prior to the adoption of this section and the amendment of the Zoning Map in connection therewith, an application for approval of a special permit was submitted in anticipation of such adoption and amendment, then if such application satisfies the requirements of this Subsection B(4), the Village Board may approve such special permit immediately following such reclassification and adoption, either on or after the date or dates on which such reclassification and adoption shall have occurred.

C. Properties not vacated.

(1) Modification of controls. If the Village of Port Chester does not complete urban renewal relocation activities, as contemplated in the Urban Renewal Plans, with respect to the entire Modified Marina Redevelopment Project, within any time period specified in the urban renewal land disposition agreement between the Village and a special permit grantee, and consultations between the special permit grantee and the Village result in agreement requiring modifications to the Concept Development Plan and, if necessary, the land use and development controls made applicable through approval of a special permit under Subsection B above and through satisfaction of the conditions thereof, then it is intended that such Concept Development Plan will be modified accordingly and reflected in an amendment to the special permit and to the land disposition agreement. The Village shall enter into such consultations upon the request of the special permit grantee. The term "Concept Development Plan" as used in this Subsection C, shall be deemed to refer collectively to the Concept Development Plan and the above-mentioned land use and development controls.

(2) Additional modification provisions. If modifications pursuant to Subsection C(1) above do not occur within 120 days following the date of delivery of a request as therein provided, then the following shall be applicable:

(a) Notice of determination.

[1] If the special permit grantee determines that compliance with those provisions of the Concept Development Plan, any site plan approval or conditions thereof approved by the Village Board in connection with the Modified Marina Redevelopment Project, the provisions of this Zoning Regulation or any condition of the special permit which specifies or restricts the type and/or

building bulk of the uses which may be constructed within the MUR Marina Redevelopment Urban Renewal District or which imposes a time schedule for construction of such uses is rendered impracticable by the failure to achieve vacant possession, then the special permit grantee may give the Village formal written notice of such determination, accompanied by the following:

[a] A statement containing information, data and analysis supporting the determination;

[b] A plan setting forth the special permit grantee's proposed modification of the Concept Development Plan;

[c] A request for concurrence by the Village Board of Trustees in the special permit grantee's determination of impracticability; and

[d] A request for approval, by the Village Board of Trustees, of a revised Concept Development Plan.

[2] Within 60 days following the submission of the above to the Village, the Village Board of Trustees will deliver to the special permit grantee a notice of Village Board action granting or denying the requests described in Subsection C(2)(a)[1][c] and [d] above. If the Village's notice indicates non-concurrence as to impracticability and/or the Village's disapproval of the revised Concept Development Plan, then it shall set forth the reasons for such non-concurrence as to impracticability and/or the Village's disapproval of the revised Concept Development Plan, and the special permit grantee shall have the right to repeat the procedure described above in this Subsection C(2) on one or more occasions.

(b) The Village Board of Trustees shall not unreasonably withhold any concurrence and/or approval requested by the special permit grantee under Subsection C(2)(a) above. Notwithstanding any contrary provision of this Subsection C, prior to approval of a revised Concept Development Plan the Village Board of Trustees shall make findings as to compliance with the following criteria:

[1] The revised Concept Development Plan shall be consistent with the objectives of the Urban Renewal Plans;

[2] The proposed uses set forth in the revised Concept Development Plan shall consist only of uses permitted to be constructed within the MUR Marina Redevelopment Project Urban Renewal District, under the Urban Renewal Plans; and

[3] The Concept Development Plan shall conform with the limitations on height, density and floor area set forth, with respect to said district, in the Urban Renewal Plan.

(a) Upon approving a revised Concept Development Plan as provided above, the Village Board shall be deemed to have modified the special permit and any previously approved site plan in order to reflect said approval and any such special criteria.

(b) If, after obtaining Village Board concurrence and approval under Subsection C(2)(a) and (b) above, the special permit grantee decides to proceed with construction in accordance with a revised Concept

Development Plan, then, except as provided in Subsection D below, the special permit grantee shall not be relieved from compliance with any provisions of this Zoning Regulation (as specified in this § 345-67) which set forth permitted uses, maximum density or maximum floor area or maximum building heights.

- (c) If, within 90 days following the expiration of the one-hundred-twenty-day period set forth in the introductory provisions of this Subsection C(2), a notice and requests are not submitted to the Village in accordance with Subsection C(2)(a) above in this Subsection C(2) or if following the delivery of such notice and requests to the Village and before the granting of the requests such notice and requests are withdrawn by the special permit grantee or if requests submitted in accordance with Subsection C(2)(a) above in this Subsection C(2) are not granted and construction does not commence in compliance with an approved or modified special permit and Concept Development Plan within 90 days following expiration of the aforesaid ninety-day period, the special permit shall terminate and shall be of no further force or effect. In the event of such termination, the land use and development controls applicable within the MUR Marina Redevelopment Project Urban Renewal District shall be the underlying controls, as defined in Subsection A(3) above in this § 345-67, provided that no building permit shall be issued for any proposed development in accordance with such underlying controls until the Village Board shall have approved a revised Concept Development Plan, complying as to content and completeness with the provisions of Subsection B(2) above but implementing such underlying controls.
- (d) Any special permit approved by the Village Board under Subsection B above and any resolution adopted by the Village Board approving a site plan as set forth in Subsection D below shall contain provisions implementing the provisions of this Subsection C(2).

D. Site plan and Concept Development Plan.

- (1) Contents, submission and approval of plans. The Concept Development Plan and the permitted land uses and development controls specified in Subsection B of this § 345-67 indicate the proposed general layout and disposition of uses, buildings, parking, access, circulation, densities, number of stories and/or height, public open space, public uses and promenades. Any promenade situated along the shore of the Byram River shall, except for railing required for safety of pedestrians, be open and unenclosed on the riverward side of such promenade.
- (2) Site plan approval. Before a building permit may be issued by the Building Inspector for any use for which a special permit has been issued under Subsection C(4) above, a site plan shall be approved therefor in accordance with the provisions of Article V of this Zoning Regulation, as modified by the provisions of this Subsection D. Authority to approve site plans for such uses is vested in the Village Board of Trustees.
- (3) Applications for site plan approval. The special permit grantee under this section may submit applications for site plan approval of all or any part of the Modified Marina Redevelopment Project. The applicant for site plan approval shall submit to the director of the Office of Planning and Development of the Village site plans which show cross sections, elevations grading and other site plan data which carry out all or any part of the Concept Development Plan. Site plans shall comply, as to content and completeness, with § 345-23 of this Zoning Regulation. The site plans shall also show any parking facilities proposed to be located outside of the Modified Marina Redevelopment Project which are intended to serve the development depicted in such site plans, along with documentation showing the applicant's control over these facilities.

- (4) Board action. The Board of Trustees shall not disapprove a site plan submitted under this Subsection D or require that such site plan be revised or amended, by reason of or with respect to general location and disposition of uses, buildings, parking areas and open space, density and height of structures and site access and circulation, provided that the site plan is consistent with the approved Concept Development Plan and the land use and development controls of § 345-67 of this Zoning Regulation. Site plans shall be approved or disapproved within 90 days of the date when submission is deemed complete by the Director of the Office of Planning and Development. Subsequent to site plan approval the developer shall submit detailed construction plans and specifications to the Office of Planning and Development and the Port Chester Building Department, which plans shall be reviewed with respect to their conformance with Chapter 151, Building and Fire Prevention, and approved site plans. Construction plans and specifications must be consistent with approved site plans prior to issuance of a building permit.
- (5) Overall effect of Concept Development Plan. All or any portion of the land area within the MUR Marina Redevelopment Project Urban Renewal District may be subdivided either horizontally or vertically for purposes of sale, lease or mortgage and/or (to the extent permitted by law) tax lot creation without the requirement for any other or further approvals under any law, ordinance, rule or regulation of the Village of Port Chester, including subdivision, zoning or site plan review. All of the requirements set forth herein for the MUR Marina Redevelopment Project Urban Renewal District, including, without limitation, those regulating the use of buildings and land and the height, bulk and arrangement of buildings shall be applied on an overall basis to the MUR Marina Redevelopment Project Urban Renewal District and shall not be applied to any individual areas within said district, notwithstanding the subdivision of the area within said district into two or more separate parcels and/or the separate ownership of such parcels, provided that no development within the MUR Marina Redevelopment Project Urban Renewal District may take place pursuant to a special permit approved under this section except in accordance with a Concept Development Plan and site plans which have been approved by the Village Board.
- (6) Amendment of Concept Development Plan. The Concept Development Plan may be amended by majority vote of the Village Board of Trustees, provided that a minor amendment (which results in less than a ten-percent variation from the overall approved Concept Development Plan) may be granted by a majority vote of the Village Board of Trustees. A substantial change in use or density shall require the same procedure as that required for an amendment to the provisions of this Zoning Regulation, as set forth in Subsection F below and in Article VII of this Zoning Regulation.

E. Applicability of other codes and ordinances. Except as set forth in Subsection A above and in the following subsections, all other applicable federal, state, county and Village of Port Chester codes and ordinances shall govern the development, ownership and management of all properties within the MUR Marina Redevelopment Project Urban Renewal District.

F. Amendments to the Zoning Regulation provisions for the MUR Marina Redevelopment Project Urban Renewal District. The zoning provisions set forth in the preceding paragraphs of this § 345-67 may be amended at any time as follows:

- (1) Minor changes. A minor change, as defined below, shall not be deemed to require an amendment of the Zoning Regulation specified in this § 345-67, and approval of any such minor change may be granted by a majority vote of the Village Board of Trustees. A minor change shall be deemed to be:
 - (a) One that modifies a dimensional requirement or an overall permitted development floor area or

density by 10% or less; or

- (b) One that represents a clarification, refinement or interpretation of the zoning requirements set forth in this § 345-67.
- (2) Other changes. Any change which is not within Subsection F(1)(a) or (b) above shall be deemed to require an amendment of the Zoning Regulation and shall require a procedure identical with that followed in the adoption of zoning amendments as specified in Article VII of this Zoning Regulation.

SECTION 2: The Amended Concept Development Plan dated May 20, 1999, made part of the Zoning Regulation pursuant to Article XVI, Section 345-68, of the Code of the Village of Port Chester, is hereby amended to permit development for multi-family dwellings in that portion of the Project Area of the Modified Marina Redevelopment Project known and designated as Retail “D” and more fully described as Section 142.31, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23 and 24 of the Town of Rye Official Tax Map including an interstitial right-of- way shown on said map.

SECTION 3. Severability

If any provision of this local law is held to be invalid or unenforceable in whole or in part, such invalidity or unenforceability shall attach only to such provision or part thereof and the remaining part of such provision and all other provisions hereof shall continue in full force and effect.

SECTION 4. Effective Date

This local law shall take effect immediately as provided by law upon due publication and filing with the Secretary of State.

~~Strikethrough~~ denote wording to be DELETED.

Underlined denote wording to be ADDED

**BOARD OF TRUSTEES RESOLUTION:
PROPOSED ZONING TEXT AMENDMENTS**

RESOLUTION

ADOPTING LOCAL LAW NO. 02 OF 2016 AMENDING CHAPTER 345, ARTICLE XVI OF THE PORT CHESTER VILLAGE CODE (ZONING)

On motion of TRUSTEE _____, seconded by TRUSTEE _____, the

following resolution was adopted by the Board of Trustees of the Village of Port Chester, New York:

WHEREAS, G&S Port Chester LLC (the “Applicant”) owns the property parcels known as Section 142.031, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23, and 24 of the Town of Rye Official Tax Map and interstitial Liberty Street Right-of-Way, which consists of approximately 23,138 square feet (the “Property”); and

WHEREAS, on 1999, the Applicant received approval from the Board of Trustees (hereinafter referred to as the “Board”) to develop the Property for retail use as part of the Modified Marina Redevelopment Project (MMRP) in 1999; and

WHEREAS, such approval allows use of the Property for 48,000 gross square feet of retail use (approximately 40,000 net square feet of such use); and

WHEREAS, on December 5, 2014, the Applicant submitted a Petition to the Board to amend the zoning map and text amendments to the Urban Renewal Plan for the MMRP, regulations to the MUR Marina Redevelopment Project Renewal District, and Concept Development Plan of the Modified Marina Redevelopment to permit multi-family residential development of the Property and to add a multifamily dwelling parking regulation for the required parking table; and

WHEREAS, on October 1, 2015, the Applicant submitted an Amended Petition to the Board for amendments to (i) the Urban Renewal Plan for the MMRP, (ii) the regulations of the MUR MMRP Urban Renewal District of the Village of Port Chester, and (iii) the Concept Development Plan of the MMRP to permit residential use of the Property, and to amend the MMRP Urban Renewal Plan and MUR District regulations to add a multifamily dwelling parking regulation to the required parking table (the “Proposed Rezoning”); and

WHEREAS, the Applicant plans to develop the Property for a mixed residential and commercial use development, consisting of approximately 12,000 square feet of ground floor retail space and 60,000 square feet of residential space (79 residential dwelling units, primarily studios and one-bedroom units), which includes a density bonus provision for additional floor area (the Proposed Rezoning and proposed development are, collectively, the “Proposed Action”); and

WHEREAS; the Board held a public hearing on the Petition that commenced on January 4, 2016 and was closed on February 1, 2016 upon, among other things, the question of enactment of Local Law No. 02 of 2016, being a Local Law amending the official zoning map and the text

of Chapter 345, Article XVI of the Village Code (Zoning) as set forth on Exhibit “A” which is on file with the Village Clerk’s Office of the Village of Port Chester; and

WHEREAS, the Board of Trustees determined that the Proposed Action will not have a significant adverse impact on the environment as described in the Negative Declaration; and

WHEREAS, the Board of Trustees adopted the Negative Declaration on March 7, 2016.

NOW, THEREFORE, be it

RESOLVED, by the Village of Port Chester Board of Trustees that Local Law No. 02 of 2016, is hereby enacted as follows and effective upon its filing with the New York State Department of State:

LOCAL LAW NO. 02 of 2016

A local law amending the Official Zoning Map and the text of Chapter 345 of the Village Code, MUR Marina Redevelopment Project Renewal District and Concept Development Plan of the Modified Marina Redevelopment project to permit multi-family residential development in a portion of the project area known and designated as Retail “D” and more fully described as Section 142.31, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23 and 24 on the Town of Rye Official Tax Map and an interstitial right-of-way shown on said tax map.

Approved as to Form:

Village Attorney
Anthony Cerreto

ROLL CALL

AYES:

NOES:

ABSENT:

DATE: March 7, 2016

AMENDED PETITION

October 1, 2015

Via Email

Hon. Dennis Pilla
Mayor and Trustees
Village of Port Chester
222 Grace Church Street
Port Chester, NY 10573

Re: G&S Port Chester LLC – Parcel D Amended Rezoning Petition
Tax Map Designation: Section 142.031, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23, and 24

Gentlemen:

Attached please find an Amended Petition for Rezoning the MUR zone and the MMRP Urban Renewal Plan for Land Use Area 2 (“Parcel D”) to permit mixed-use development.

The submitted Amended Petition embodies the recommendations of the Planning Commission by scaling back the originally requested “as of right” bulk development of the parcel significantly, with the opportunity for bonus density for additional floor area similar to §345-16(4) of the Zoning Code available in the C2, C5 and C5T zoning districts.

We respectfully request that you accept the Petition, provide an opportunity for a public presentation and schedule a public hearing date.

Respectfully Submitted,
Tartaglia Law Group, LLC



Daniel D. Tartaglia, Esq.

BOARD OF TRUSTEES: VILLAGE OF PORT CHESTER
COUNTY OF WESTCHESTER: STATE OF NEW YORK
-----X

In the Matter of the Application of

G & S PORT CHESTER LLC

AMENDED
PETITION

For amendments to (i) the Urban Renewal Plan for the Modified Marina Redevelopment Project, (ii) the regulations of the MUR Marina Redevelopment Project Urban Renewal District of the Village of Port Chester, and (iii) the Concept Development Plan of the Modified Marina Redevelopment Project.

-----X

G & S Port Chester LLC (the “Petitioner”), by its attorneys Tartaglia Law Group, LLC, hereby petitions the Board of Trustees of the Village of Port Chester, as follows:

THE PETITIONER AND THE MODIFIED MARINA REDEVELOPMENT PROJECT

1. The Petitioner is a limited liability company organized and existing under the laws of the State of New York, having an address at 211 East 43rd Street, 25th Floor, New York, New York.

2. The Petitioner is the duly designated sponsor and redeveloper of the Modified Marina Redevelopment Project (the “Project”) of the Village of Port Chester (the “Village”) pursuant to a certain Land Acquisition and Disposition Agreement (the “LADA”) dated as of July 14, 1999 and made by and among the Petitioner, the Village and the Village of Port Chester Industrial Development Agency (the “IDA”), as amended to date.

3. Under the LADA: (a) the Project area is divided into several “Units” for private redevelopment; (b) fee title to each of the Units was conveyed by the Village to the IDA; and (c) each Unit was leased to Petitioner in a “straight-lease transaction,” as that term is defined in the

New York State Industrial Development Agency Act (New York General Municipal Law Article 18A).

4. The Project was approved by the Board of Trustees of the Village (the “Village Board”) as of July 14, 1999, by, among other things, the Second Amendment to the Urban Renewal Plan for the Village Center Urban Renewal Plan Area and the Marina Redevelopment Urban Renewal Plan (such Second Amendment is hereinafter referred to as the “MMRP Urban Renewal Plan”). The Project is an approved program for the redevelopment of an approximately twenty-eight (28) acre area in the downtown of the Village in and adjacent to the Village's central business district. The objectives of the Project are to eliminate blight and deteriorated conditions within the Project area, revitalize the Village's downtown and waterfront areas by attracting new businesses and creating employment opportunities, preserve and enhance property values and enhance the Village's aesthetics. The Project consists of a total of approximately 500,000 ft² of retail and commercial uses together with related infrastructure, parking and public spaces. A ground floor plan which shows the general layout of the Project is attached hereto as Exhibit “A.” The total permitted development is 670,000 ft² (see: Table below).

5. The Land Use and Development Controls for the Project are set forth in §345-67B of Chapter 345 of the Village’s Zoning Code. This section breaks the Project into primarily five (5) land use areas. Each Land Use Area has bulk limitations on permitted development (square footage of development) and permitted height. The Table below sets for these controls:

Land Use Area	Uses Permitted	Permitted Development of Building Floor Area	Permitted Height
1	Retail Sales, including supermarket with customary accessory uses, and retail and personal services or restaurant types of uses, general and professional offices and uses customarily accessory to each of the foregoing uses, parking and loading and open	275,000 ft ²	3 stories; 70'

	space uses. Multi-screen cinema theater, theaters and accessory uses customarily found in such a complex.		
2	The same land uses as an Area 1, except for multi-screen cinema theater, theaters and supermarket	40,000 ft ²	3 stories; 45'
3	Retail sales, retail, wholesale membership warehouse club, personal services and restaurant uses, neighborhood facilities, general and professional offices as well as uses customarily accessory to each of the foregoing uses. Multi-screen cinema theater, theaters and accessory uses customarily found in such a complex. Parking and loading and open space uses	275,000 ft ²	2 stories; 65'
4	General and professional offices and uses customarily accessory thereto. Retail and personal services uses, restaurant and customary accessory uses. Parking and loading and open space uses.	80,000 ft ²	3 stories; 45'
5	Open space, parking and water-related recreation uses which shall be excess above to the general public.		
Total Permitted Floor Area:		670,000 ft²	

THE PROPERTY

6. The real property which is the subject of this Petition is known and designated on the tax assessment map of the Village as Section 142.031, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23, and 24 and consists of approximately 23,138 square feet (the “Property”). It is located at the northeast corner of North Main Street and Westchester Avenue, and is also known as “Retail D” of the Project (see: “Proposed Ground Floor Plan Retail Bldg. D” shown on Exhibit “A”). The Property is also designated as Unit 2B of the Project under the LADA (“Unit 2B”). A perimeter description of the Property is also attached hereto as part of Exhibit “A.”

7. The Property also encompasses all of “Land Use Area 2” as described above and as shown on the Village's “Map of Land Use Areas and Streets within the Modified Marina Redevelopment Project” (the “Land Use Area Map”).

8. The Property is currently approved for the construction of approximately 40,000 ft² of floor area in a three (3) story building.

THE PROPOSED BUILDING

9. At the time that the Project was approved, the Village Board's focus was on retail and commercial development, and not on residential development. However, over the course of the last decade, there has been a strong trend toward “smart growth,” which advocates mixed-use development concentrated in urban cores, with residential uses located near public transportation. As has been shown in many thriving nearby communities such as the City of White Plains, where luxury residential development has attracted residents with high incomes supporting a new and exciting downtown living experience, locating residential uses in a downtown area that offers a range of public transportation, employment opportunities, and housing choices leads to long term sustainability of the downtown.

10. The value of residential development in the downtown was not lost on the Village Board at the time the Project was approved. Although the Project does not currently include a residential component, the Village Board's findings under the State Environmental Quality Review Act made in connection with the approval of the Project acknowledge that the success of the commercial portion of the Project could be a catalyst for future residential development in the downtown area, which would also be a positive and beneficial impact of the Project.

ORIGINAL PETITION

11. In its original Petition submitted to this Board and reviewed by the Village's Planning Commission and staff, the Petitioner proposed to construct on the Property an approximately 90,000 square foot, five (5) story mixed-use residential and commercial building (the “Building”) to be known as “Waterfront Place.” Renderings of the proposed Building are attached hereto as Exhibit “B.”

12. The originally proposed ground floor of the Building was to have contained approximately 12,000 square feet of neighborhood retail space.

13. The remaining four (4) floors of the Building were to constitute the residential component, and, subject to design variations, would have contained approximately seventy-nine (79) rental dwelling units consisting of the following unit mix: fifty-six (56) one-bedroom units, sixteen (16) two-bedroom units and seven (7) studio units. A typical residential floor plan is attached hereto as Exhibit “C.”

14. In addition to the availability of the public transportation and retail and entertainment opportunities in close proximity, the Building would include a number of amenities for the residential component, including an exercise room and an outdoor rooftop courtyard which would provide passive recreational space for use by the residents.

AMENDED PETITION

15. Based upon feedback and comments from the Planning Commission and from Village Staff, the Petitioner is submitting this Amended Petition which proposes to reduce the permitted floor area of the Building from the originally proposed 90,000 square feet to a Building of approximately 72,000 square feet. The Petitioner is proposing that the MUR also contain a density bonus provision for additional floor area similar to §345-16(4) of the Zoning Code available in the C2, C5 and C5T zoning districts.

THE PROPOSED AMENDMENTS TO THE MMRP URBAN RENEWAL PLAN, REGULATIONS OF THE MUR MARINA REDEVELOPMENT PROJECT URBAN RENEWAL AREA AND CONCEPT DEVELOPMENT PLAN OF THE PROJECT

16. Land use in the Project is controlled by the MMRP Urban Renewal Plan, the regulations of the MUR Marina Redevelopment Project Urban Renewal District (the “MUR

District”), and the “Concept Development Plan” required by those regulations. The retail component of the proposed Building is permitted under all three controls.

17. Under the Zoning Regulation, the residential component of the Building is considered a “multifamily dwelling.” The MMRP Urban Renewal Plan, MUR District regulations and Concept Development Plan do not currently permit multifamily dwellings in Land Use Area 2, or the development within Land Use Area 2 of more than 40,000 square feet of floor area in a building greater than three (3) stories and 45 feet high. Therefore, to permit the development of the Building on the Property, the Petitioner respectfully requests that: (a) the MMRP Urban Renewal Plan and the regulations of the MUR District be amended to create a new district to be referred to as the “MUR-Mixed Use District” specifically limited to Land Use Area 2, that will principally permit: (i) multi-family dwellings (floors above first floor); and require (ii) maximum building height of five (5) stories or 70 feet; (iii) maximum Floor Area Ratio of 3.7 (iii) minimum lot area per dwelling unit of 240 sq. ft.; (iv) no requirement for lot width or lot depth; (v) no minimum yard requirements; and (vi) no minimum usable open space per dwelling unit, and (b) the Concept Development Plan be amended to conform to these proposed amendments to the MMRP Urban Renewal Plan and MUR District regulations. The proposed amendments to the MMRP Urban Renewal Plan and MUR District Regulations are set forth in Exhibit “D” hereto.

18. The mixed-use proposed Project is completely consistent the Land Use Strategies and recommendations for the Downtown North and South Main Street and Abendroth Avenue (west side) land area as set forth in the Village’s 2012 Comprehensive Plan.

19. There is ample existing parking to accommodate the proposed residential use of the Building. However, since the Project as currently approved does not contain a residential component, the MMRP Urban Renewal Plan and MUR District regulations do not include a

parking requirement for multifamily dwellings. Accordingly, the Petitioner respectfully requests that the table of required parking in the MMRP Urban Renewal Plan and in the MUR District (Section 345-67.B(3)(g)[2][b] of the Port Chester Zoning Regulation) be amended to include a parking standard for “multi-family dwellings” as set forth in Exhibit “E” hereto.

20. The Petitioner respectfully submits that the Building is an appropriate use of the Property, will contribute to the continuing revitalization and sustainability of the downtown and will further the objectives of the Project and the goals for sustainable development that are set forth in the draft Comprehensive Plan under discussion in the Village.

COMPLIANCE WITH THE STATE ENVIRONMENTAL QUALITY REVIEW ACT

21. Under the State Environmental Quality Review Act and the regulations promulgated thereunder (“SEQRA”), the actions directly and indirectly proposed by the Petition are classified as “Unlisted.”

22. Accordingly, the Petitioner shall comply in all respects with the requirements of SEQRA regarding an Unlisted action. A long Environmental Assessment Form prepared by Cleary Consulting has been submitted herewith.

WHEREFORE, the Petitioner respectfully requests that this Petition be granted as set forth herein.

Respectfully submitted,
TARTAGLIA LAW GROUP, LLC
Attorneys for Petitioner
800 Westchester Avenue
Rye Brook, NY 10573
(914) 481-1880

Dated: Rye Brook, New York
September 25, 2015

EXHIBIT “D”

Proposed Amendments to the MMRP Urban Renewal Plan

Section B.1.b (Permitted Land Use and Development Controls) is amended as follows (matter in {brackets} is deleted; underlined matter is added):

Land Use Parcel 2

Permitted Land Uses: Same as Parcel 1, except for multiscreen cinema theater, theaters, and supermarket.

Multifamily dwellings

{Permitted Development: } {Forty thousand (40,000) square feet of building floor area.} 3.7

Permitted Height: {Three (3) stories; forty five (45) feet} Five (5) stories; 70 feet.

Proposed Amendments to the MUR District regulations

Section 345-67.B(1)(b) is amended as follows (matter in {brackets} is deleted; underlined matter is added):

(b) Land Use Area 2.

[1] Permitted land uses:

[a] Same as Land Use Area 1, except for multiscreen cinema theater, theaters and supermarket.

[b] Multi-family dwellings

[2] {Permitted development: 40,000 square feet of building floor area.} Maximum Floor Area Ratio is 3.7. At the option of the property owner, the development potential of a site can be increased in the form of additional floor area if the property owner makes a monetary contribution to the Village equal to 15% of the assessed value of the bonus floor space, as determined by the Village Assessor. If there is no Village Assessor, the calculation shall be based upon the assessed value of the bonus floor space as determined by the Town Assessor.

[3] Permitted height: {three stories; 45 feet} five stories; 70 feet.

EXHIBIT “E”

Proposed Amendments to the MMRP Urban Renewal Plan and MUR District regulations

Section B.2.g of the MMRP Urban Renewal Plan and Section 345-67.B(3)(g)[2][b] of the MUR District Regulations (Tables of Required Parking) are both amended to include the following use category and standards:

Multifamily Dwellings

Weekdays

Monday through Friday

9:00 am. 0.75 per 1,000 square feet of floor area

12:00 p.m. 0.75 per 1,000 square feet of floor area

6:00 p.m. 1.0 per 1,000 square feet of floor area

9:00 p.m. 1.25 per 1,000 square feet of floor area

Weekends

Saturday and Sunday

9:00 a.m. 0.5 per 1,000 square feet of floor area

12:00 p.m. 0.5 per 1,000 square feet of floor area

6:00 p.m. 1.0 per 1,000 square feet of floor area

9:00 p.m. 1.25 per 1,000 square feet of floor area



FRONT PLACE

FRONT PLACE

RETAIL 1

RETAIL 2

RETAIL 3

RETAIL 4

RETAIL 5

RETAIL 6

RETAIL 7

RETAIL RETAIL RETAIL



WATERGATE

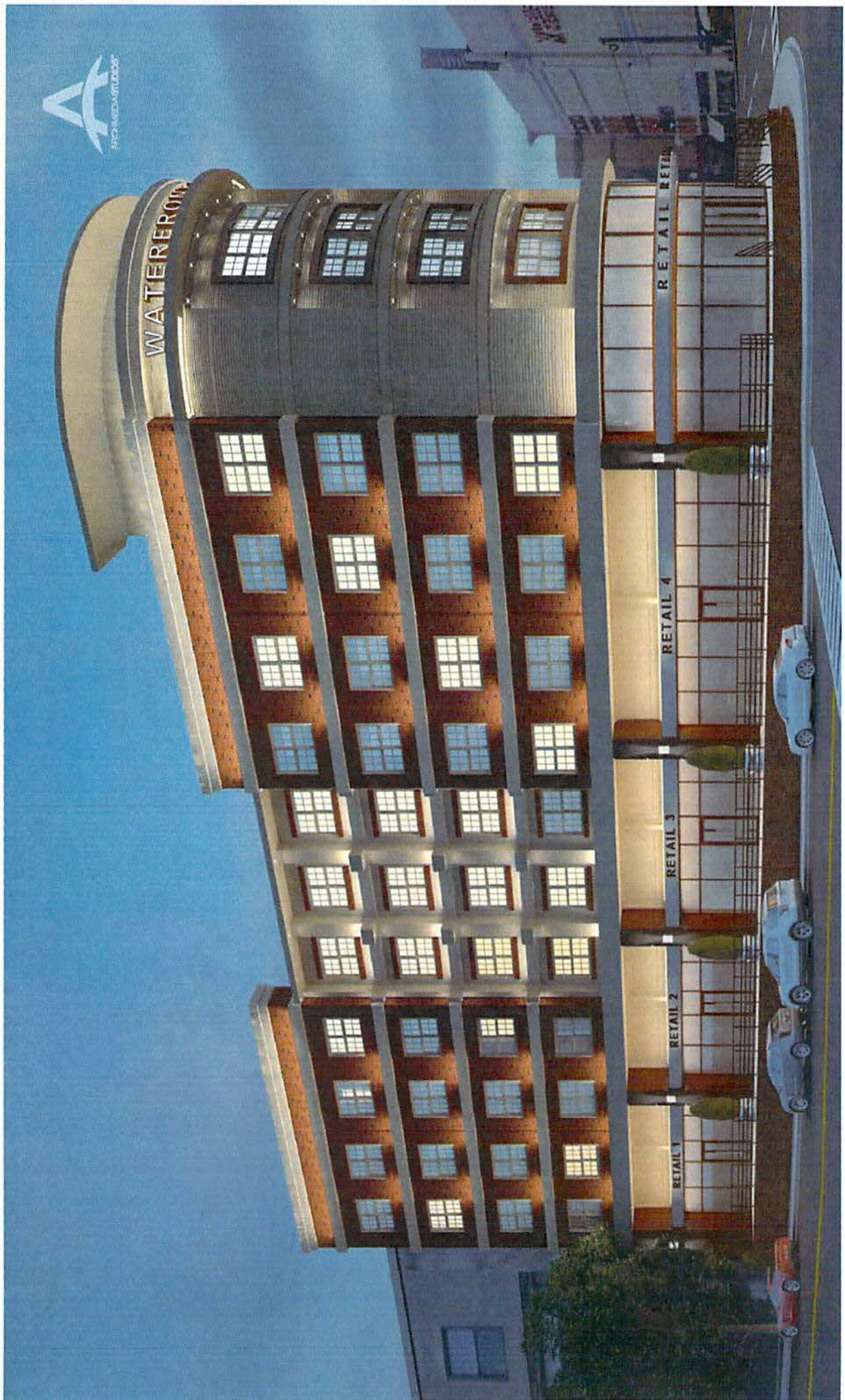
RETAIL RETAIL

RETAIL 4

RETAIL 3

RETAIL 2

RETAIL 1



**Full Environmental Assessment Form
Part 1 - Project and Setting**

Instructions for Completing Part 1

Part 1 is to be completed by the applicant or project sponsor. Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification.

Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information; indicate whether missing information does not exist, or is not reasonably available to the sponsor; and, when possible, generally describe work or studies which would be necessary to update or fully develop that information.

Applicants/sponsors must complete all items in Sections A & B. In Sections C, D & E, most items contain an initial question that must be answered either “Yes” or “No”. If the answer to the initial question is “Yes”, complete the sub-questions that follow. If the answer to the initial question is “No”, proceed to the next question. Section F allows the project sponsor to identify and attach any additional information. Section G requires the name and signature of the project sponsor to verify that the information contained in Part 1 is accurate and complete.

A. Project and Sponsor Information.

Name of Action or Project:		
Project Location (describe, and attach a general location map):		
Brief Description of Proposed Action (include purpose or need):		
Name of Applicant/Sponsor:		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:
Project Contact (if not same as sponsor; give name and title/role):		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:
Property Owner (if not same as sponsor):		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:

B. Government Approvals

B. Government Approvals, Funding, or Sponsorship. (“Funding” includes grants, loans, tax relief, and any other forms of financial assistance.)

Government Entity	If Yes: Identify Agency and Approval(s) Required	Application Date (Actual or projected)
a. City Council, Town Board, or Village Board of Trustees <input type="checkbox"/> Yes <input type="checkbox"/> No		
b. City, Town or Village Planning Board or Commission <input type="checkbox"/> Yes <input type="checkbox"/> No		
c. City Council, Town or Village Zoning Board of Appeals <input type="checkbox"/> Yes <input type="checkbox"/> No		
d. Other local agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
e. County agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
f. Regional agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
g. State agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
h. Federal agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
<p>i. Coastal Resources.</p> <p><i>i.</i> Is the project site within a Coastal Area, or the waterfront area of a Designated Inland Waterway? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p><i>ii.</i> Is the project site located in a community with an approved Local Waterfront Revitalization Program? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p><i>iii.</i> Is the project site within a Coastal Erosion Hazard Area? <input type="checkbox"/> Yes <input type="checkbox"/> No</p>		

C. Planning and Zoning

C.1. Planning and zoning actions.

Will administrative or legislative adoption, or amendment of a plan, local law, ordinance, rule or regulation be the only approval(s) which must be granted to enable the proposed action to proceed? Yes No

- **If Yes**, complete sections C, F and G.
- **If No**, proceed to question C.2 and complete all remaining sections and questions in Part 1

C.2. Adopted land use plans.

a. Do any municipally- adopted (city, town, village or county) comprehensive land use plan(s) include the site where the proposed action would be located? Yes No

If Yes, does the comprehensive plan include specific recommendations for the site where the proposed action would be located? Yes No

b. Is the site of the proposed action within any local or regional special planning district (for example: Greenway Brownfield Opportunity Area (BOA); designated State or Federal heritage area; watershed management plan; or other?) Yes No

If Yes, identify the plan(s):

c. Is the proposed action located wholly or partially within an area listed in an adopted municipal open space plan, or an adopted municipal farmland protection plan? Yes No

If Yes, identify the plan(s):

C.3. Zoning

a. Is the site of the proposed action located in a municipality with an adopted zoning law or ordinance. Yes No
If Yes, what is the zoning classification(s) including any applicable overlay district?

b. Is the use permitted or allowed by a special or conditional use permit? Yes No

c. Is a zoning change requested as part of the proposed action? Yes No

If Yes,

i. What is the proposed new zoning for the site? _____

C.4. Existing community services.

a. In what school district is the project site located? _____

b. What police or other public protection forces serve the project site?

c. Which fire protection and emergency medical services serve the project site?

d. What parks serve the project site?

D. Project Details

D.1. Proposed and Potential Development

a. What is the general nature of the proposed action (e.g., residential, industrial, commercial, recreational; if mixed, include all components)?

b. a. Total acreage of the site of the proposed action? _____ acres
b. Total acreage to be physically disturbed? _____ acres
c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor? _____ acres

c. Is the proposed action an expansion of an existing project or use? Yes No
i. If Yes, what is the approximate percentage of the proposed expansion and identify the units (e.g., acres, miles, housing units, square feet)? % _____ Units: _____

d. Is the proposed action a subdivision, or does it include a subdivision? Yes No

If Yes,

i. Purpose or type of subdivision? (e.g., residential, industrial, commercial; if mixed, specify types)

ii. Is a cluster/conservation layout proposed? Yes No

iii. Number of lots proposed? _____

iv. Minimum and maximum proposed lot sizes? Minimum _____ Maximum _____

e. Will proposed action be constructed in multiple phases? Yes No

i. If No, anticipated period of construction: _____ months

ii. If Yes:

- Total number of phases anticipated _____
- Anticipated commencement date of phase 1 (including demolition) _____ month _____ year
- Anticipated completion date of final phase _____ month _____ year

• Generally describe connections or relationships among phases, including any contingencies where progress of one phase may determine timing or duration of future phases: _____

f. Does the project include new residential uses? Yes No
 If Yes, show numbers of units proposed.

	<u>One Family</u>	<u>Two Family</u>	<u>Three Family</u>	<u>Multiple Family (four or more)</u>
Initial Phase	_____	_____	_____	_____
At completion	_____	_____	_____	_____
of all phases	_____	_____	_____	_____

g. Does the proposed action include new non-residential construction (including expansions)? Yes No
 If Yes,

i. Total number of structures _____

ii. Dimensions (in feet) of largest proposed structure: _____ height; _____ width; and _____ length

iii. Approximate extent of building space to be heated or cooled: _____ square feet

h. Does the proposed action include construction or other activities that will result in the impoundment of any liquids, such as creation of a water supply, reservoir, pond, lake, waste lagoon or other storage? Yes No
 If Yes,

i. Purpose of the impoundment: _____

ii. If a water impoundment, the principal source of the water: Ground water Surface water streams Other specify: _____

iii. If other than water, identify the type of impounded/contained liquids and their source. _____

iv. Approximate size of the proposed impoundment. Volume: _____ million gallons; surface area: _____ acres

v. Dimensions of the proposed dam or impounding structure: _____ height; _____ length

vi. Construction method/materials for the proposed dam or impounding structure (e.g., earth fill, rock, wood, concrete): _____

D.2. Project Operations

a. Does the proposed action include any excavation, mining, or dredging, during construction, operations, or both? Yes No
 (Not including general site preparation, grading or installation of utilities or foundations where all excavated materials will remain onsite)
 If Yes:

i. What is the purpose of the excavation or dredging? _____

ii. How much material (including rock, earth, sediments, etc.) is proposed to be removed from the site?

- Volume (specify tons or cubic yards): _____
- Over what duration of time? _____

iii. Describe nature and characteristics of materials to be excavated or dredged, and plans to use, manage or dispose of them. _____

iv. Will there be onsite dewatering or processing of excavated materials? Yes No
 If yes, describe. _____

v. What is the total area to be dredged or excavated? _____ acres

vi. What is the maximum area to be worked at any one time? _____ acres

vii. What would be the maximum depth of excavation or dredging? _____ feet

viii. Will the excavation require blasting? Yes No

ix. Summarize site reclamation goals and plan: _____

b. Would the proposed action cause or result in alteration of, increase or decrease in size of, or encroachment into any existing wetland, waterbody, shoreline, beach or adjacent area? Yes No
 If Yes:

i. Identify the wetland or waterbody which would be affected (by name, water index number, wetland map number or geographic description): _____

ii. Describe how the proposed action would affect that waterbody or wetland, e.g. excavation, fill, placement of structures, or alteration of channels, banks and shorelines. Indicate extent of activities, alterations and additions in square feet or acres:

iii. Will proposed action cause or result in disturbance to bottom sediments? Yes No

If Yes, describe: _____

iv. Will proposed action cause or result in the destruction or removal of aquatic vegetation? Yes No

If Yes:

- acres of aquatic vegetation proposed to be removed: _____
- expected acreage of aquatic vegetation remaining after project completion: _____
- purpose of proposed removal (e.g. beach clearing, invasive species control, boat access): _____
- proposed method of plant removal: _____
- if chemical/herbicide treatment will be used, specify product(s): _____

v. Describe any proposed reclamation/mitigation following disturbance: _____

c. Will the proposed action use, or create a new demand for water? Yes No

If Yes:

i. Total anticipated water usage/demand per day: _____ gallons/day

ii. Will the proposed action obtain water from an existing public water supply? Yes No

If Yes:

- Name of district or service area: _____
- Does the existing public water supply have capacity to serve the proposal? Yes No
- Is the project site in the existing district? Yes No
- Is expansion of the district needed? Yes No
- Do existing lines serve the project site? Yes No

iii. Will line extension within an existing district be necessary to supply the project? Yes No

If Yes:

- Describe extensions or capacity expansions proposed to serve this project: _____
- Source(s) of supply for the district: _____

iv. Is a new water supply district or service area proposed to be formed to serve the project site? Yes No

If Yes:

- Applicant/sponsor for new district: _____
- Date application submitted or anticipated: _____
- Proposed source(s) of supply for new district: _____

v. If a public water supply will not be used, describe plans to provide water supply for the project: _____

vi. If water supply will be from wells (public or private), maximum pumping capacity: _____ gallons/minute.

d. Will the proposed action generate liquid wastes? Yes No

If Yes:

i. Total anticipated liquid waste generation per day: _____ gallons/day

ii. Nature of liquid wastes to be generated (e.g., sanitary wastewater, industrial; if combination, describe all components and approximate volumes or proportions of each): _____

iii. Will the proposed action use any existing public wastewater treatment facilities? Yes No

If Yes:

- Name of wastewater treatment plant to be used: _____
- Name of district: _____
- Does the existing wastewater treatment plant have capacity to serve the project? Yes No
- Is the project site in the existing district? Yes No
- Is expansion of the district needed? Yes No

- Do existing sewer lines serve the project site? Yes No
- Will line extension within an existing district be necessary to serve the project? Yes No

 If Yes:

- Describe extensions or capacity expansions proposed to serve this project: _____

iv. Will a new wastewater (sewage) treatment district be formed to serve the project site? Yes No
 If Yes:

- Applicant/sponsor for new district: _____
- Date application submitted or anticipated: _____
- What is the receiving water for the wastewater discharge? _____

v. If public facilities will not be used, describe plans to provide wastewater treatment for the project, including specifying proposed receiving water (name and classification if surface discharge, or describe subsurface disposal plans):

vi. Describe any plans or designs to capture, recycle or reuse liquid waste: _____

e. Will the proposed action disturb more than one acre and create stormwater runoff, either from new point sources (i.e. ditches, pipes, swales, curbs, gutters or other concentrated flows of stormwater) or non-point source (i.e. sheet flow) during construction or post construction? Yes No
 If Yes:

- How much impervious surface will the project create in relation to total size of project parcel?
 _____ Square feet or _____ acres (impervious surface)
 _____ Square feet or _____ acres (parcel size)
- Describe types of new point sources. _____

- Where will the stormwater runoff be directed (i.e. on-site stormwater management facility/structures, adjacent properties, groundwater, on-site surface water or off-site surface waters)?

 - If to surface waters, identify receiving water bodies or wetlands: _____

 - Will stormwater runoff flow to adjacent properties? Yes No

iv. Does proposed plan minimize impervious surfaces, use pervious materials or collect and re-use stormwater? Yes No

f. Does the proposed action include, or will it use on-site, one or more sources of air emissions, including fuel combustion, waste incineration, or other processes or operations? Yes No
 If Yes, identify:

- Mobile sources during project operations (e.g., heavy equipment, fleet or delivery vehicles)

- Stationary sources during construction (e.g., power generation, structural heating, batch plant, crushers)

- Stationary sources during operations (e.g., process emissions, large boilers, electric generation)

g. Will any air emission sources named in D.2.f (above), require a NY State Air Registration, Air Facility Permit, or Federal Clean Air Act Title IV or Title V Permit? Yes No
 If Yes:

- Is the project site located in an Air quality non-attainment area? (Area routinely or periodically fails to meet ambient air quality standards for all or some parts of the year) Yes No
- In addition to emissions as calculated in the application, the project will generate:
 - _____ Tons/year (short tons) of Carbon Dioxide (CO₂)
 - _____ Tons/year (short tons) of Nitrous Oxide (N₂O)
 - _____ Tons/year (short tons) of Perfluorocarbons (PFCs)
 - _____ Tons/year (short tons) of Sulfur Hexafluoride (SF₆)
 - _____ Tons/year (short tons) of Carbon Dioxide equivalent of Hydroflouorocarbons (HFCs)
 - _____ Tons/year (short tons) of Hazardous Air Pollutants (HAPs)

h. Will the proposed action generate or emit methane (including, but not limited to, sewage treatment plants, landfills, composting facilities)? Yes No

If Yes:

i. Estimate methane generation in tons/year (metric): _____

ii. Describe any methane capture, control or elimination measures included in project design (e.g., combustion to generate heat or electricity, flaring): _____

i. Will the proposed action result in the release of air pollutants from open-air operations or processes, such as quarry or landfill operations? Yes No

If Yes: Describe operations and nature of emissions (e.g., diesel exhaust, rock particulates/dust): _____

j. Will the proposed action result in a substantial increase in traffic above present levels or generate substantial new demand for transportation facilities or services? Yes No

If Yes:

i. When is the peak traffic expected (Check all that apply): Morning Evening Weekend
 Randomly between hours of _____ to _____.

ii. For commercial activities only, projected number of semi-trailer truck trips/day: _____

iii. Parking spaces: Existing _____ Proposed _____ Net increase/decrease _____

iv. Does the proposed action include any shared use parking? Yes No

v. If the proposed action includes any modification of existing roads, creation of new roads or change in existing access, describe: _____

vi. Are public/private transportation service(s) or facilities available within 1/2 mile of the proposed site? Yes No

vii. Will the proposed action include access to public transportation or accommodations for use of hybrid, electric or other alternative fueled vehicles? Yes No

viii. Will the proposed action include plans for pedestrian or bicycle accommodations for connections to existing pedestrian or bicycle routes? Yes No

k. Will the proposed action (for commercial or industrial projects only) generate new or additional demand for energy? Yes No

If Yes:

i. Estimate annual electricity demand during operation of the proposed action: _____

ii. Anticipated sources/suppliers of electricity for the project (e.g., on-site combustion, on-site renewable, via grid/local utility, or other): _____

iii. Will the proposed action require a new, or an upgrade to, an existing substation? Yes No

l. Hours of operation. Answer all items which apply.

<p>i. During Construction:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ • Saturday: _____ • Sunday: _____ • Holidays: _____ 	<p>ii. During Operations:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ • Saturday: _____ • Sunday: _____ • Holidays: _____
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<p>m. Will the proposed action produce noise that will exceed existing ambient noise levels during construction, operation, or both? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If yes:</p> <p>i. Provide details including sources, time of day and duration:</p> <p>_____</p> <p>_____</p>	
<p>ii. Will proposed action remove existing natural barriers that could act as a noise barrier or screen? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>Describe: _____</p> <p>_____</p>	
<p>n. Will the proposed action have outdoor lighting? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If yes:</p> <p>i. Describe source(s), location(s), height of fixture(s), direction/aim, and proximity to nearest occupied structures:</p> <p>_____</p> <p>_____</p>	
<p>ii. Will proposed action remove existing natural barriers that could act as a light barrier or screen? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>Describe: _____</p> <p>_____</p>	
<p>o. Does the proposed action have the potential to produce odors for more than one hour per day? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If Yes, describe possible sources, potential frequency and duration of odor emissions, and proximity to nearest occupied structures: _____</p> <p>_____</p> <p>_____</p>	
<p>p. Will the proposed action include any bulk storage of petroleum (combined capacity of over 1,100 gallons) or chemical products 185 gallons in above ground storage or any amount in underground storage? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Product(s) to be stored _____</p> <p>ii. Volume(s) _____ per unit time _____ (e.g., month, year)</p> <p>iii. Generally describe proposed storage facilities: _____</p> <p>_____</p>	
<p>q. Will the proposed action (commercial, industrial and recreational projects only) use pesticides (i.e., herbicides, insecticides) during construction or operation? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Describe proposed treatment(s):</p> <p>_____</p> <p>_____</p> <p>_____</p>	
<p>ii. Will the proposed action use Integrated Pest Management Practices? <input type="checkbox"/> Yes <input type="checkbox"/> No</p>	
<p>r. Will the proposed action (commercial or industrial projects only) involve or require the management or disposal of solid waste (excluding hazardous materials)? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Describe any solid waste(s) to be generated during construction or operation of the facility:</p> <ul style="list-style-type: none"> • Construction: _____ tons per _____ (unit of time) • Operation : _____ tons per _____ (unit of time) <p>ii. Describe any proposals for on-site minimization, recycling or reuse of materials to avoid disposal as solid waste:</p> <ul style="list-style-type: none"> • Construction: _____ • Operation: _____ <p>iii. Proposed disposal methods/facilities for solid waste generated on-site:</p> <ul style="list-style-type: none"> • Construction: _____ • Operation: _____ 	

s. Does the proposed action include construction or modification of a solid waste management facility? Yes No
 If Yes:
 i. Type of management or handling of waste proposed for the site (e.g., recycling or transfer station, composting, landfill, or other disposal activities): _____
 ii. Anticipated rate of disposal/processing:
 • _____ Tons/month, if transfer or other non-combustion/thermal treatment, or
 • _____ Tons/hour, if combustion or thermal treatment
 iii. If landfill, anticipated site life: _____ years

t. Will proposed action at the site involve the commercial generation, treatment, storage, or disposal of hazardous waste? Yes No
 If Yes:
 i. Name(s) of all hazardous wastes or constituents to be generated, handled or managed at facility: _____

 ii. Generally describe processes or activities involving hazardous wastes or constituents: _____

 iii. Specify amount to be handled or generated _____ tons/month
 iv. Describe any proposals for on-site minimization, recycling or reuse of hazardous constituents: _____

 v. Will any hazardous wastes be disposed at an existing offsite hazardous waste facility? Yes No
 If Yes: provide name and location of facility: _____

 If No: describe proposed management of any hazardous wastes which will not be sent to a hazardous waste facility:

E. Site and Setting of Proposed Action

E.1. Land uses on and surrounding the project site

a. Existing land uses.
 i. Check all uses that occur on, adjoining and near the project site.
 Urban Industrial Commercial Residential (suburban) Rural (non-farm)
 Forest Agriculture Aquatic Other (specify): _____
 ii. If mix of uses, generally describe:

b. Land uses and covertypes on the project site.

Land use or Covertypes	Current Acreage	Acreage After Project Completion	Change (Acres +/-)
• Roads, buildings, and other paved or impervious surfaces			
• Forested			
• Meadows, grasslands or brushlands (non-agricultural, including abandoned agricultural)			
• Agricultural (includes active orchards, field, greenhouse etc.)			
• Surface water features (lakes, ponds, streams, rivers, etc.)			
• Wetlands (freshwater or tidal)			
• Non-vegetated (bare rock, earth or fill)			
• Other Describe: _____ _____			

c. Is the project site presently used by members of the community for public recreation? Yes No
i. If Yes: explain: _____

d. Are there any facilities serving children, the elderly, people with disabilities (e.g., schools, hospitals, licensed day care centers, or group homes) within 1500 feet of the project site? Yes No
If Yes,
i. Identify Facilities:

e. Does the project site contain an existing dam? Yes No
If Yes:
i. Dimensions of the dam and impoundment:

- Dam height: _____ feet
- Dam length: _____ feet
- Surface area: _____ acres
- Volume impounded: _____ gallons OR acre-feet

ii. Dam's existing hazard classification: _____
iii. Provide date and summarize results of last inspection:

f. Has the project site ever been used as a municipal, commercial or industrial solid waste management facility, or does the project site adjoin property which is now, or was at one time, used as a solid waste management facility? Yes No
If Yes:
i. Has the facility been formally closed? Yes No

- If yes, cite sources/documentation: _____

ii. Describe the location of the project site relative to the boundaries of the solid waste management facility:

g. Have hazardous wastes been generated, treated and/or disposed of at the site, or does the project site adjoin property which is now or was at one time used to commercially treat, store and/or dispose of hazardous waste? Yes No
If Yes:
i. Describe waste(s) handled and waste management activities, including approximate time when activities occurred:

h. Potential contamination history. Has there been a reported spill at the proposed project site, or have any remedial actions been conducted at or adjacent to the proposed site? Yes No
If Yes:
i. Is any portion of the site listed on the NYSDEC Spills Incidents database or Environmental Site Remediation database? Check all that apply: Yes No
 Yes – Spills Incidents database Provide DEC ID number(s): _____
 Yes – Environmental Site Remediation database Provide DEC ID number(s): _____
 Neither database
ii. If site has been subject of RCRA corrective activities, describe control measures: _____

iii. Is the project within 2000 feet of any site in the NYSDEC Environmental Site Remediation database? Yes No
If yes, provide DEC ID number(s): _____
iv. If yes to (i), (ii) or (iii) above, describe current status of site(s):

v. Is the project site subject to an institutional control limiting property uses? <input type="checkbox"/> Yes <input type="checkbox"/> No <ul style="list-style-type: none"> • If yes, DEC site ID number: _____ • Describe the type of institutional control (e.g., deed restriction or easement): _____ • Describe any use limitations: _____ • Describe any engineering controls: _____ • Will the project affect the institutional or engineering controls in place? <input type="checkbox"/> Yes <input type="checkbox"/> No • Explain: _____ _____ _____ 						
E.2. Natural Resources On or Near Project Site						
a. What is the average depth to bedrock on the project site? _____ feet						
b. Are there bedrock outcroppings on the project site? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, what proportion of the site is comprised of bedrock outcroppings? _____ %						
c. Predominant soil type(s) present on project site: <table style="width: 100%; border: none;"> <tr> <td style="border: none;">_____</td> <td style="border: none; text-align: right;">_____ %</td> </tr> <tr> <td style="border: none;">_____</td> <td style="border: none; text-align: right;">_____ %</td> </tr> <tr> <td style="border: none;">_____</td> <td style="border: none; text-align: right;">_____ %</td> </tr> </table>	_____	_____ %	_____	_____ %	_____	_____ %
_____	_____ %					
_____	_____ %					
_____	_____ %					
d. What is the average depth to the water table on the project site? Average: _____ feet						
e. Drainage status of project site soils: <table style="width: 100%; border: none;"> <tr> <td style="border: none;"><input type="checkbox"/> Well Drained:</td> <td style="border: none; text-align: right;">_____ % of site</td> </tr> <tr> <td style="border: none;"><input type="checkbox"/> Moderately Well Drained:</td> <td style="border: none; text-align: right;">_____ % of site</td> </tr> <tr> <td style="border: none;"><input type="checkbox"/> Poorly Drained</td> <td style="border: none; text-align: right;">_____ % of site</td> </tr> </table>	<input type="checkbox"/> Well Drained:	_____ % of site	<input type="checkbox"/> Moderately Well Drained:	_____ % of site	<input type="checkbox"/> Poorly Drained	_____ % of site
<input type="checkbox"/> Well Drained:	_____ % of site					
<input type="checkbox"/> Moderately Well Drained:	_____ % of site					
<input type="checkbox"/> Poorly Drained	_____ % of site					
f. Approximate proportion of proposed action site with slopes: <table style="width: 100%; border: none;"> <tr> <td style="border: none;"><input type="checkbox"/> 0-10%:</td> <td style="border: none; text-align: right;">_____ % of site</td> </tr> <tr> <td style="border: none;"><input type="checkbox"/> 10-15%:</td> <td style="border: none; text-align: right;">_____ % of site</td> </tr> <tr> <td style="border: none;"><input type="checkbox"/> 15% or greater:</td> <td style="border: none; text-align: right;">_____ % of site</td> </tr> </table>	<input type="checkbox"/> 0-10%:	_____ % of site	<input type="checkbox"/> 10-15%:	_____ % of site	<input type="checkbox"/> 15% or greater:	_____ % of site
<input type="checkbox"/> 0-10%:	_____ % of site					
<input type="checkbox"/> 10-15%:	_____ % of site					
<input type="checkbox"/> 15% or greater:	_____ % of site					
g. Are there any unique geologic features on the project site? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, describe: _____ _____						
h. Surface water features. <ul style="list-style-type: none"> i. Does any portion of the project site contain wetlands or other waterbodies (including streams, rivers, ponds or lakes)? <input type="checkbox"/> Yes <input type="checkbox"/> No ii. Do any wetlands or other waterbodies adjoin the project site? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes to either <i>i</i> or <i>ii</i> , continue. If No, skip to E.2.i. <ul style="list-style-type: none"> iii. Are any of the wetlands or waterbodies within or adjoining the project site regulated by any federal, state or local agency? <input type="checkbox"/> Yes <input type="checkbox"/> No iv. For each identified regulated wetland and waterbody on the project site, provide the following information: <ul style="list-style-type: none"> • Streams: Name _____ Classification _____ • Lakes or Ponds: Name _____ Classification _____ • Wetlands: Name _____ Approximate Size _____ • Wetland No. (if regulated by DEC) _____ 						
v. Are any of the above water bodies listed in the most recent compilation of NYS water quality-impaired waterbodies? <input type="checkbox"/> Yes <input type="checkbox"/> No If yes, name of impaired water body/bodies and basis for listing as impaired: _____ _____						
i. Is the project site in a designated Floodway? <input type="checkbox"/> Yes <input type="checkbox"/> No						
j. Is the project site in the 100 year Floodplain? <input type="checkbox"/> Yes <input type="checkbox"/> No						
k. Is the project site in the 500 year Floodplain? <input type="checkbox"/> Yes <input type="checkbox"/> No						
l. Is the project site located over, or immediately adjoining, a primary, principal or sole source aquifer? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <ul style="list-style-type: none"> i. Name of aquifer: _____ 						

<p>m. Identify the predominant wildlife species that occupy or use the project site: _____ _____ _____</p>	
<p>n. Does the project site contain a designated significant natural community? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> Describe the habitat/community (composition, function, and basis for designation): _____ _____ <i>ii.</i> Source(s) of description or evaluation: _____ <i>iii.</i> Extent of community/habitat: • Currently: _____ acres • Following completion of project as proposed: _____ acres • Gain or loss (indicate + or -): _____ acres</p>	
<p>o. Does project site contain any species of plant or animal that is listed by the federal government or NYS as endangered or threatened, or does it contain any areas identified as habitat for an endangered or threatened species? <input type="checkbox"/> Yes <input type="checkbox"/> No</p>	
<p>p. Does the project site contain any species of plant or animal that is listed by NYS as rare, or as a species of special concern? <input type="checkbox"/> Yes <input type="checkbox"/> No</p>	
<p>q. Is the project site or adjoining area currently used for hunting, trapping, fishing or shell fishing? <input type="checkbox"/> Yes <input type="checkbox"/> No If yes, give a brief description of how the proposed action may affect that use: _____ _____</p>	
<p>E.3. Designated Public Resources On or Near Project Site</p>	
<p>a. Is the project site, or any portion of it, located in a designated agricultural district certified pursuant to Agriculture and Markets Law, Article 25-AA, Section 303 and 304? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, provide county plus district name/number: _____</p>	
<p>b. Are agricultural lands consisting of highly productive soils present? <input type="checkbox"/> Yes <input type="checkbox"/> No <i>i.</i> If Yes: acreage(s) on project site? _____ <i>ii.</i> Source(s) of soil rating(s): _____</p>	
<p>c. Does the project site contain all or part of, or is it substantially contiguous to, a registered National Natural Landmark? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> Nature of the natural landmark: <input type="checkbox"/> Biological Community <input type="checkbox"/> Geological Feature <i>ii.</i> Provide brief description of landmark, including values behind designation and approximate size/extent: _____ _____ _____</p>	
<p>d. Is the project site located in or does it adjoin a state listed Critical Environmental Area? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> CEA name: _____ <i>ii.</i> Basis for designation: _____ <i>iii.</i> Designating agency and date: _____</p>	

e. Does the project site contain, or is it substantially contiguous to, a building, archaeological site, or district which is listed on, or has been nominated by the NYS Board of Historic Preservation for inclusion on, the State or National Register of Historic Places?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Nature of historic/archaeological resource: <input type="checkbox"/> Archaeological Site <input type="checkbox"/> Historic Building or District	
<i>ii.</i> Name: _____	
<i>iii.</i> Brief description of attributes on which listing is based: _____	
f. Is the project site, or any portion of it, located in or adjacent to an area designated as sensitive for archaeological sites on the NY State Historic Preservation Office (SHPO) archaeological site inventory?	<input type="checkbox"/> Yes <input type="checkbox"/> No
g. Have additional archaeological or historic site(s) or resources been identified on the project site?	
If Yes:	
<i>i.</i> Describe possible resource(s): _____	
<i>ii.</i> Basis for identification: _____	
h. Is the project site within five miles of any officially designated and publicly accessible federal, state, or local scenic or aesthetic resource?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Identify resource: _____	
<i>ii.</i> Nature of, or basis for, designation (e.g., established highway overlook, state or local park, state historic trail or scenic byway, etc.): _____	
<i>iii.</i> Distance between project and resource: _____ miles.	
i. Is the project site located within a designated river corridor under the Wild, Scenic and Recreational Rivers Program 6 NYCRR 666?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Identify the name of the river and its designation: _____	
<i>ii.</i> Is the activity consistent with development restrictions contained in 6NYCRR Part 666?	
	<input type="checkbox"/> Yes <input type="checkbox"/> No

F. Additional Information

Attach any additional information which may be needed to clarify your project.

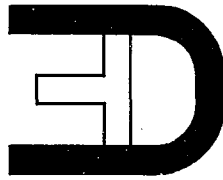
If you have identified any adverse impacts which could be associated with your proposal, please describe those impacts plus any measures which you propose to avoid or minimize them.

G. Verification

I certify that the information provided is true to the best of my knowledge.

Applicant/Sponsor Name _____ Date _____

Signature _____ Title _____



Dunn Engineering Associates, P.C.

Consulting Engineers

66 Main Street

Westhampton Beach, N.Y. 11978

631-288-2480

631-288-2544 Fax

November 12, 2015

Douglas Riley, Esq.
G&S Port Chester, LLC
211 East 43rd Street
25th Floor
New York, NY 10017

Re: Traffic Engineering Letter Report
Change of Zone Petition
Mixed Use Building
Village of Port Chester, NY

Dear Mr. Riley:

In accordance with your request, Dunn Engineering Associates, P.C. has prepared this letter report to examine the traffic engineering impacts of the proposed change of use on the subject parcel shown in Figure 1. The current approved use is for 40,000 S.F. of retail. The proposed use is for 12,000 S.F. of retail with 79 residential apartments.

We initially addressed the traffic engineering aspects of the petition when G&S Port Chester LLC was proposing that the amended zoning ordinance would allow for construction of a 90,000 square foot structure. We understand that as a result of a review of the application by the Village Planning Commission, the applicant has scaled back the size of the maximum allowed structure to approximately 72,000 square feet. Our analysis below takes that reduction into consideration.

The major traffic engineering issues that shall be addressed are 1) Comparison of Site Generated Traffic of the Permitted Use with the Proposed Use, 2) Comparison of Parking Requirements for the Existing Permitted Use Versus the Proposed Use, 3) Consideration of Shared Parking Concept and Smart Growth, 4) Effects of Public Transportation, 5) Availability of Off-Site Parking That Can Be Managed by G&S, 6) Conclusions, and 7) Recommendations.

These topics are discussed in detail in the following sections of this letter report.

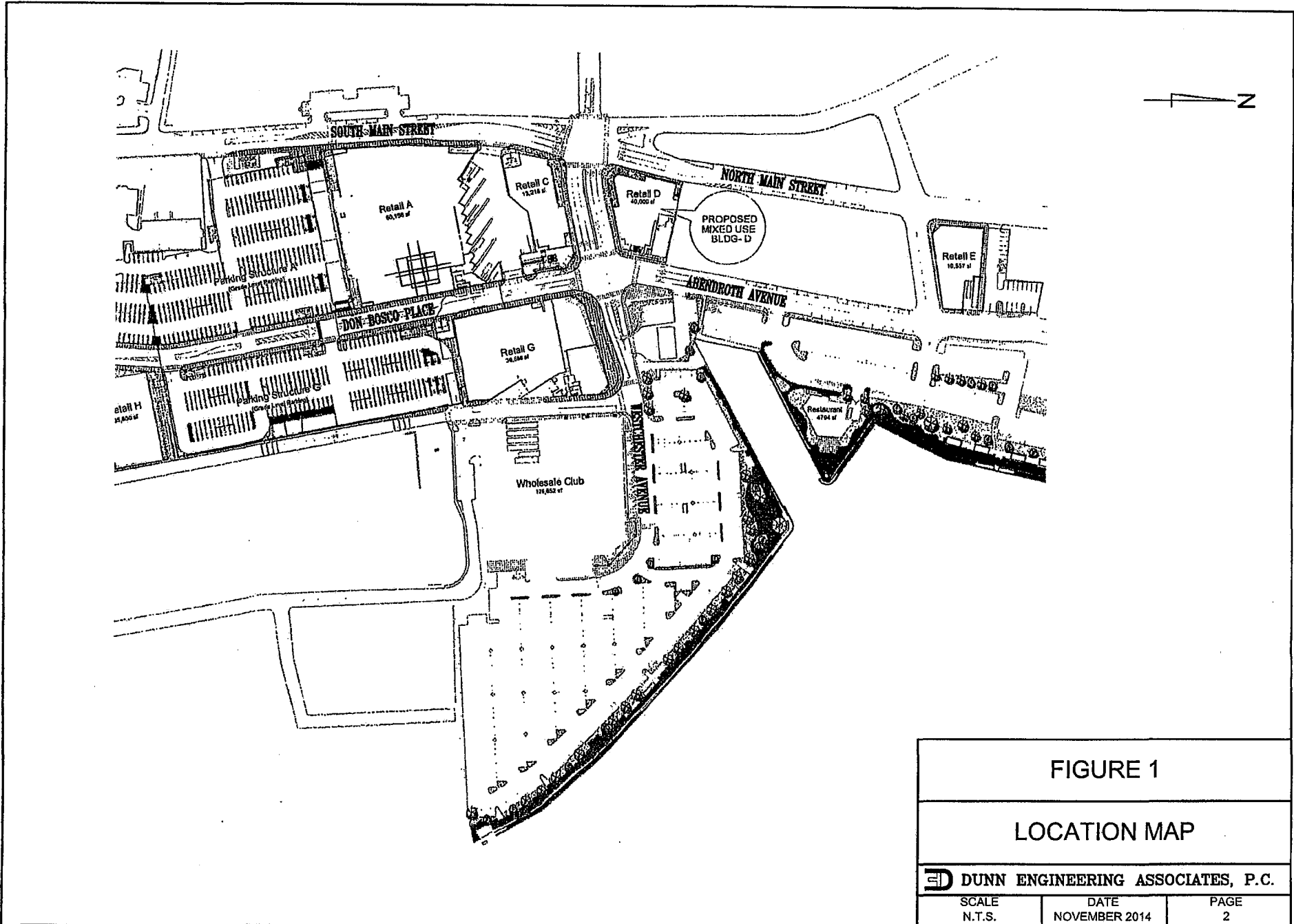



FIGURE 1

LOCATION MAP

 DUNN ENGINEERING ASSOCIATES, P.C.		
SCALE	DATE	PAGE
N.T.S.	NOVEMBER 2014	2

Comparison of Site Generated Traffic of the Permitted Use with the Proposed Use

As part of this examination, trip generation estimates were prepared for 1) the proposed 12,000 square feet of retail space and 79 residential apartments, and 2) the currently approved retail use of 40,000 S.F. Information on trip generation rates for retail uses and residential apartments is contained in the latest (9th) edition of "Trip Generation", a report published by the Institute of Transportation Engineers (ITE). This is a nationally recognized authoritative source for trip generation information for various land uses based on field surveys.

Table 1 presents a comparison of site generated traffic of the proposed use versus the current approved use. As shown in Table 1, the proposed use generates less traffic than the existing use.

Proposed Use	A.M.*		P.M.*		Saturday**	
	.44 Enter	.56 Exit	.56 Enter	.44 Exit	.52 Enter	.48 Exit
Retail Use: 12,000 S.F. Specialty Retail Center Land Use Code 826	14	18	35	25	30	28
	.31 Enter	.69 Exit	.58 Enter	.42 Exit	.54 Enter	.46 Exit
Residential Apartments: 79 Units Land Use Code 223 (Mid-Rise Apartment)	7	17	18	13	25***	21***
Total	21	35	53	38	55	49
Previously Approved Use						
Retail Use: 40,000 S.F. Specialty Retail Center Land Use Code 826	48	60	113	88	100	93
Reduction in Site Generated Traffic	27	25	60	50	45	44

*Peak Hours of Adjacent Street Traffic

**Peak Hour of Generator

***Land Use 221 Low-Rise Apartment Data

Table 1
Comparison of Site Generated Traffic
Proposed Use Versus Previously Approved Use

In order to predict the amount of traffic generated by the currently approved retail use and the proposed building, trip generation estimates were calculated under ITE Land Use Code 826 (Specialty Retail Center) and ITE Land Use Code 223 (Mid-Rise Apartment).

Table 1, shows the estimated site-generated traffic for the currently approved retail use of 40,000 S.F. that is anticipated during typical peak hours based on the ITE trip generation rates (Land Use Code 826) per 1,000 square feet.

Comparing the traffic generated by the current permitted use of 40,000 S.F. of retail with the traffic generated by the proposed 12,000 S.F. of retail use (Land Use Code 826, Specialty Retail Center) and 79 residential apartments (Land Use Code 223, Mid-Rise Apartment per apartment unit indicates that the current 40,000 S.F. retail use generates substantially more traffic during the A.M. and P.M. peak hours as well as the Saturday midday peak hours (108 > 56 in the A.M., 201 > 91 in the P.M., and 193 > 104 in the Saturday Midday).

From a traffic engineering standpoint, a general change of zone requirement is that the proposed use should generate equal or less traffic than a permitted use. As a result, the proposed use has less of a traffic impact than the permitted use and thus satisfies this requirement.

Comparison of Parking Requirements for the Existing Permitted Uses Versus the Proposed Use

Information provided in the report Parking Generation, 4th Edition, published by the Institute of Transportation Engineers (ITE) was consulted. This report provides information regarding parking demand at a variety of land uses, based on data collected at numerous sites throughout North America, including shopping centers (ITE Land Use Code 820) and mid-rise apartments (ITE Land Use Code 221), and is commonly used in the transportation engineering industry for investigations of this kind. Parking demand projections were examined for the weekday and Saturday peak demand periods. Also, Tables 2 and 3 present time-of-day distribution of parking demand of low/mid rise apartments and retail center, respectively. These two tables may be helpful to the Village in their examination of a standard for “multi-family dwellings”.

It should be recognized that the current approved retail building is to be served by existing project parking and is not required to provide on site parking.

Like the current approved retail building, the proposed building shall be served by existing project parking.

As noted in the original studies, there is ample existing parking nearby to accommodate the proposed residential use of the Building.

<i>Based on Vehicles per Dwelling Unit (Suburban)</i>	<i>Weekday</i>
Hours Beginning	Percent of Peak Period
12:00-4:00 a.m.	100
5:00 a.m.	96
6:00 a.m.	92
7:00 a.m.	74
8:00 a.m.	64
9:00 a.m.	--
10:00 a.m.	--
11:00 a.m.	--
12:00 a.m.	--
1:00 p.m.	--
2:00 p.m.	--
3:00 p.m.	--
4:00 p.m.	44
5:00 p.m.	59
6:00 p.m.	69
7:00 p.m.	66
8:00 p.m.	75
9:00 p.m.	77
10:00 p.m.	92
11:00 p.m.	94

Table 2
Time-of-Day Distribution Of Parking Demand
Low/Mid Rise Apartments

	<i>Non-Friday Weekday</i>	<i>Friday</i>	<i>Saturday</i>
Hours Beginning	Percent of Peak Period	Percent of Peak Period	Percent of Peak Period
12:00-4:00 a.m.	--	--	--
5:00 a.m.	--	--	--
6:00 a.m.	--	--	--
7:00 a.m.	5	--	13
8:00 a.m.	18	--	27
9:00 a.m.	38	--	60
10:00 a.m.	68	63	75
11:00 a.m.	91	79	90
12:00 a.m.	100	100	100
1:00 p.m.	97	92	100
2:00 p.m.	95	83	98
3:00 p.m.	88	76	91
4:00 p.m.	78	70	76
5:00 p.m.	62	73	67
6:00 p.m.	64	77	72
7:00 p.m.	77	92	51
8:00 p.m.	70	89	52
9:00 p.m.	42	--	44
10:00 p.m.	--	--	29
11:00 p.m.	--	--	--

Table 3
Time-of-Day Distribution for Parking Demand Retail Center

Consideration of Shared Parking Concept and Smart Growth

The concept of “Shared Parking” is well recognized in the transportation engineering industry. A number of trips to this section of the Village will be multi-purpose trips, where in the same parking space is utilized for a trip that encompasses visits to the proposed retail stores in the proposed Building combined with a visit to restaurants and other stores along Main Street, Westchester Avenue and Abendroth Avenue.

Smart Growth incorporates a combination of techniques that include increasing housing density along transit nodes and mixing residential and commercial use areas. An excellent example of Smart Growth is in the proposed use of the Building. A mixed use of retail on the first floor with apartments on the other floors fosters the residents walking to the retail stores, restaurants and entertainment within the Village without using a car or requiring additional parking spaces.

Shared parking and Smart Growth result in less site-generated traffic and the need for fewer parking spaces.

Effects of Public Transportation

Based on the proximity of the bus route immediately adjacent to the site and the nearby railroad, which would allow patrons and employees to use the buses to travel to and from the development, it is expected that some patrons, employees, residents and visitors may utilize the public transportation system. In this study, however, no credit was applied for use of public transportation, and the traffic destined to and from the proposed site was based on the use of passenger cars only. However, the use of public transportation by patrons, employees, residents and visitors would further reduce the traffic impact and the parking needs.

Availability of Off-Site Parking That Can Be Managed by G&S

The proposed use shall be developed with no on-site parking. As a result, a parking demand management plan shall be prepared that addresses the Village's and the site's parking needs. This plan shall be created by utilizing G&S Port Chester's ability to designate, control, assign and manage parking spaces for use by residents of the apartments and by patrons and employees of the retail stores. The parking demand shall be determined and parking lot occupancy data shall be utilized to comply with the requirements and codes of the Village.

Conclusions

1. A comparison of the site generated traffic of the proposed use versus the current permitted use indicates that the proposed use would generate equal or less traffic than a permitted use.
2. The proposed use has less of a traffic impact than the permitted use.
3. Like the current approved retail building, the proposed building shall be served by existing project parking and is not required to provide on-site parking.
4. The Village of Port Chester should amend the Zoning Regulations to include a parking standard for "multi-family dwellings".
5. Shared parking, smart growth and use of public transportation (bus and railroad) result in less site generated traffic and the need for fewer parking spaces.
6. If the Village grants the change of use, the following details and engineering documents shall be prepared for the appropriate Village Boards and Departments: 1) site plan with

- access locations, and 2) off-site parking demand management plan that includes parking lots along Abendroth Avenue and nearby existing parking structures.
7. Based on the above noted conclusions, no significant adverse traffic impacts, or any potential traffic impacts already considered and addressed in the original Traffic Impact Study and SEQRA findings will occur as a result of the construction of the proposed building in lieu of the current approved building.

Recommendations

Based on the traffic engineering information presented above, it is recommended that the change of use petition be approved.

As presented in this letter report, the traffic impact of the proposed use is expected to be less than the current permitted use that was included in our original Traffic Impact Study. Thus, no new Traffic Impact Study or additional roadway improvements would be required.

If you have any questions or require additional information, please do not hesitate to contact me.

Sincerely,



WALTER M. DUNN, JR., P.E.
President

WMD:as
L2015113

PETITION of G&S PORT CHESTER UNIT 2B LLC

ANALYSIS OF SCHOOL CHILD POPULATION IN MULTI-UNIT RESIDENTIAL STRUCTURES

NOVEMBER , 2014

Petitioner summarizes below its findings related to the probable number of school children who would be living from time to time in a multi-unit residential building located in the “downtown” section of the Village of Port Chester.

Cleary Consulting reviewed the 2012 Rutgers University data for market rate rental apartments for Port Chester, which are as follows:

Bedroom Count	Number of School Children
Studio	-0-
1 Bedroom	0.07
2 Bedroom	0.16
3 Bedroom	0.63

Cleary Consulting also reviewed the census data for market rate rentals in Port Chester published in American Community Survey (2012), which are as follows:

Bedroom Count	Number of School Children
Studio	-0-
1 Bedroom	0.03
2 Bedroom	0.31
3 Bedroom	1.47

Using the Rutgers data as a multiplier, the recently-constructed “Mariner” complex would produce 11 school aged children.

Using the American Community Survey data as a multiplier, the Mariner would produce 14 school aged children.

The actual number for the Mariner is believed to be 18 school aged children.

For purposes of the Parcel “D” Project, the following would be the projection depending upon which multiplier is used:

Rutgers Methodology:

Number of Apartments	Bedroom Count	Number of School Children
7	Studio	-0-
56	1 Bedroom	3.92
16	2 Bedroom	2.56
	TOTAL	6.48

American Community Survey Methodology:

Number of Apartments	Bedroom Count	Number of School Children
7	Studio	-0-
56	1 Bedroom	1.68
16	2 Bedroom	4.96
	TOTAL	6.64

Accordingly, for the proposed 79 units, fewer than 7 would be likely to have a school-aged child in residence on a regular basis.



Village of Port Chester Waterfront Commission

222 Grace Church Street
Port Chester, NY 10573
Phone 914-937-6780



LWRP COASTAL ASSESSMENT FORM

An applicant seeking an approval for a non-excluded action from any agency of the Village of Port Chester which is subject to the Village of Port Chester Local Waterfront Revitalization Program (LWRP) must complete this form. This form is intended to assist the Village Waterfront Commission in determining if the proposed action is consistent with the policies and purposes of the LWRP.

SITE IDENTIFICATION INFORMATION		
Application Name: MUR Amendment	Application #:	Date Submitted:
Site Address: Section 142.031, Block 1, Lots 3, 4, 5, 6, 20, 21, 22, 23, and 24 <i>No. Street:</i>		
Property Location: <i>(Identify landmarks, distance from intersections, etc.)</i> Bounded: West by N. Main Street, East by Abendoth Avenue and South by Westchester Avenue		
Village of Port Chester Tax Map Designation: <i>Section 142.031 Block 1 Lot(s) 3,4,5,6,20,21,22,23 and 24</i>	Zoning Designation of Site: MUR	
OWNER/APPLICANT INFORMATION		
Property Owner: G & S PORT CHESTER LLC	Phone #: 914-481-1880 Fax #: 914-206-4858	Email: ddt@tartagliaalawgroup.com
Owner Address: <i>No. 211 Street: East 43rd Street, 25th floor Town: New York State: NY Zip:</i>		
Applicant (If different than owner):	Phone #: Fax #:	Email:
Applicant Address (If different than owner): <i>No. Street: Town: State: Zip:</i>		
PROJECT DESCRIPTION		
Describe the project, proposed use and operation thereof: the MMRP Urban Renewal Plan and the regulations of the MUR District be amended to create a new district to be referred to as the "MUR-Mixed Use District" specifically limited to Land Use Area 2, that will principally permit: (i) multi-family dwellings (floors above first floor); and require (ii) maximum building height of five (5) stories or 70 feet; (iii) maximum Floor Area Ratio of 3.7 (iii) minimum lot area per dwelling unit of 240 sq. ft.; (iv) no requirement for lot width or lot depth; (v) no minimum yard requirements; and (vi) no minimum usable open space per dwelling unit		

CONSISTANCY WITH LWRP POLICIES

Actions to be undertaken within the Village's coastal area shall be evaluated for consistency in accordance with the following LWRP policy standards and conditions, which are derived from and further explained and described in Section III of the Village of Port Chester LWRP, a copy of which is on file in the Village Clerk's office and available for inspection during normal business hours. Agencies that undertake direct actions shall also consult with Section IV of the LWRP in making their consistency determination.

YES indicates that the proposed project/action is consistent with the applicable policy,

NO indicates that the proposed project/action is inconsistent with the applicable policy,

NOT APPLICABLE indicates that it does not apply.

<p>Restore, revitalize and redevelop deteriorated and underutilized waterfront areas for commercial, industrial, cultural, recreational, public access and other compatible uses (Policies 1, 1A, 1B, 1C, 1D and 1E).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Facilitate the siting of water dependent uses on or near coastal waters (Policy 2).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Encourage the development and enhancement of those traditional uses and activities which have provided the Village a unique maritime identity (Policy 4).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Ensure that development occurs where adequate public infrastructure is available (Policy 5).</p>	<p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable</p>
<p>Streamline development permit procedures (Policy 6).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Protect fish and wildlife resources from hazardous wastes and other contaminating pollutants (Policy 8).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Maintain and expand the recreational use of existing fish and wildlife resources while ensuring protection of renewable fish and wildlife resources (Policy 9).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Maintain and expand commercial fishing resources to promote commercial fishing opportunities (Policy 10).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>

<p>Minimize flooding and erosion hazards through proper siting of buildings and structures; construction of carefully selected, long-term structural measures; and the use of appropriate nonstructural means (Policies 11, 13, 14, 15 and 17).</p>	<p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable</p>
<p>Public funds shall be used for erosion protection structures only where necessary and in an appropriate manner (Policy 16).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Safeguard vital economic, social and environmental interests in the coastal area when major actions are undertaken (Policy 18).</p>	<p><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable</p>
<p>Protect, maintain, and improve public access to the shoreline and to water-related recreational resources, while protecting natural and historic resources and adjacent land uses and providing a continuous public walkway along the foreshore of the Byram River (Policies 19, 20 and 20A).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Encourage, facilitate and prioritize water-dependent and water-enhanced recreational resources and facilities near coastal waters, with priority given to those near access to existing public transportation and areas where the use of the shore is severely restricted by existing development (Policy 21).</p>	<p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable</p>
<p>Encourage the development of water-related recreational resources and facilities, as multiple uses, in appropriate locations within the shore zone (Policy 22).</p>	<p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable</p>
<p>Protect, restore and enhance historic resources (Policy 23).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>
<p>Protect and enhance scenic and aesthetic resources (Policy 25).</p>	<p><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable</p>
<p>Site and construct energy facilities in a manner which will be compatible with the environment and contingent upon the need for a shorefront location and in such a manner as to avoid adverse environmental impacts when in operation (Policies 27 and 29).</p>	<p><input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable</p>

Protect coastal waters from direct and indirect discharge of pollutants (Policies 30, 31, 33, 34, 35, 36 and 37).	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
Ensure that dredging and dredge spoil disposal are undertaken in a manner protective of natural resources (Policies 15 and 35).	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
Ensure that any transportation, handling or disposal of hazardous wastes and effluent is undertaken in a manner which will not adversely affect the environment (Policy 39).	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
Protect air quality in accordance with state and federal air quality standards (Policies 41 and 42).	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
Preserve and protect tidal and freshwater wetlands (Policy 44).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable

DETERMINATION OF CONSISTENCY

To be filled out by the Waterfront Commission

- This project/action is consistent with LWRP Policies and Conditions
- This project/action is NOT consistent with LWRP Policies and Conditions

The following changes to the project/action are recommended to ensure consistency with the LWRP:

Waterfront Commission Chairman

Date



VILLAGE OF
PORT CHESTER

222 Grace Church Street, Port Chester, New York 10573

RES-02
 BOT 3-7 -2016

AGENDA MEMO

Department: Planning and Development Department
Department: Office of the Village Attorney

BOT Meeting Date: 3/7/2016

Item Type: Resolution

Sponsor's Name:	Anthony Cerreto, Village Attorney
Sponsor's Name:	Eric Zamft, Director of Planning & Economic Development

Description	Yes	No	Description	Yes	No
Fiscal Impact	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Public Hearing Required	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Funding Source:			BID #		
Account #:			Strategic Plan Priority Area		
			Transit Oriented Development		
	Yes	No			
Agreement	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Manager Priorities		
Strategic Plan Related	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Planning & Zoning		

Agenda Heading Title
(Will appear as indicated below on Agenda)

Port North Main Street Rezoning: Declaration of Lead Agency and Scheduling of Public Hearing

Summary

On October 6, 2015, Port North Main Street LLC (the "Applicant") filed site plan applications for 531 and 601 North Main Street to convert the existing buildings to retail use. Recognizing that retail is currently not a permitted use in the C4 General Commercial Zoning District in which the properties are located, the Applicant concurrently filed a petition on December 15, 2015 for a proposed zoning text amendment to Village Code Chapter 345, to allow retail as a special exception use within all or part of the C4 District, to provide a parking standard for retail use, and to provide specific special exception

criteria (the "Original Petition"). The Original Petition also offered an alternative to create a new zoning district in the North Main Street area of the Village. The Applicant made a presentation to the Board of Trustees on the Original Petition at the January 19, 2016 Board meeting. At the January 19, 2016 Board meeting, the Board accepted the Original Petition for consideration and review, referred the matter to the Planning Commission and Waterfront Commission for their review and comments, and declared their intent to be Lead Agency under the New York State Environmental Quality Review Act ("SEQRA").

At its January 25, 2016 meeting, the Planning Commission held a public meeting on the zoning and provided comments, as encapsulated in the January 25, 2016 Planning Commission meeting minutes. The Planning Commission did not comment on the site plans, as the zoning change is necessary first for that review to occur.

Based upon the Planning Commission's comments and an initial review by the Department of Planning & Economic Development, the Applicant has since filed an Amended Petition to specifically request a proposed zoning text amendment to Village Code Chapter 345, to create a new C4-R Gateway Retail District from Terrace Avenue north along North Main Street to the Connecticut border, create a retail parking standard for the C4-R District, designate permitted and special exception uses and lot and bulk controls for such district, and establish special exception criteria for retail uses (the "Amended Petition"). As part of the Amended Petition, the Applicant has provided a Long Environmental Assessment Form (Long "EAF") for SEQRA purposes and a Coastal Assessment Form ("CAF") for Local Waterfront Revitalization Program ("LWRP") consistency purposes.

By virtue of the Amended Petition, the Applicant has responded to the Planning Commission's and staff's comments.

The CAF has been provided to the Waterfront Commission for their review. The Amended Petition is scheduled for discussion at the Waterfront Commission's next meeting on March 30, 2016.

Planning staff is currently reviewing the proposal for SEQRA review on the zoning. A copy of the site plan application and Amended Petition have been forwarded to Adler Consulting for their review with regards to the potential traffic impacts. We anticipate having a full review completed prior to the date of the public hearing. There has and will continue to be interaction and cooperation with the Applicant to evaluate the potential impacts and benefits of the proposal.

The Village has not received any objection from any involved agencies objecting to the Board of Trustees acting as lead agency. Therefore, the Board may act to declare itself lead agency for the SEQRA review.

Therefore, should the Board wish to proceed, the attached resolution undertakes the following actions:

1. Accepts the Amended Petition
2. Declares the Board as Lead Agency under SEQRA
3. Schedules a Public Hearing on the Petition for the April 4, 2016 Board meeting

Attachments
<ul style="list-style-type: none">• Amended Petition• Coastal Assessment Form (CAF)• Environmental Assessment Form (EAF)• Planning Commission minutes from January 25, 2016

RESOLUTION

ACCEPTING THE PORT NORTH MAIN STREET LLC AMENDED PETITION TO AMEND THE ZONING CODE OF THE VILLAGE OF PORT CHESTER TO INCLUDE A C4-R GATEWAY RETAIL ZONING DISTRICT FOR REVIEW AND CONSIDERATION, DECLARING THE VILLAGE OF PORT CHESTER BOARD OF TRUSTEES AS LEAD AGENCY FOR THE PORT NORTH MAIN STREET PROPOSED ACTION UNDER THE STATE ENVIRONMENTAL QUALITY REVIEW ACT (SEQRA) AND SETTING A PUBLIC HEARING ON THE PROPOSED ZONING AMENDMENT

On motion of Trustee _____, seconded by Trustee _____,

the following resolution was adopted by the Board of Trustees of the Village of Port Chester, New York:

WHEREAS, On October 6, 2015, Port North Main Street LLC (the “Applicant”) filed site plan applications for 531 and 601 North Main Street to convert the existing buildings to retail use; and

WHEREAS, recognizing that retail is currently not a permitted use in the C4 General Commercial Zoning District in which the properties are located, the Applicant concurrently filed a petition on December 15, 2015 for a proposed zoning text amendment to Village Code Chapter 345, to allow retail as a special exception use within all or part of the C4 District, to provide a parking standard for retail use, and to provide specific special exception criteria (the “Original Petition”); and

WHEREAS, the Applicant made a presentation to the Board of Trustees on the Original Petition at the January 19, 2016 Board meeting; and

WHEREAS, at the January 19, 2016 Board meeting, the Board accepted the Original Petition for consideration and review, referred the matter to the Planning Commission and Waterfront Commission for their review and comments, and declared their intent to be Lead Agency under the New York State Environmental Quality Review Act (“SEQRA”); and

WHEREAS, the Board of Trustees referred the Proposal to the Westchester County Planning Board pursuant to §345-34 H for review and comment in accordance with of the Village Zoning Code; and

WHEREAS, At its January 25, 2016 meeting, the Planning Commission held a public meeting on the zoning and provided comments, as encapsulated in the January 25, 2016 Planning Commission meeting minutes; and

WHEREAS, based upon the Planning Commission's comments and an initial review by the Department of Planning & Economic Development, the Applicant filed an Amended Petition to specifically request a proposed zoning text amendment to Village Code Chapter 345, to create a new C4-R Gateway Retail District from Terrace Avenue north along North Main Street to the Connecticut border, create a retail parking standard for the C4-R District, designate permitted and special exception uses and lot and bulk controls for such district, and establish special exception criteria for retail uses (the "Amended Petition"); and

WHEREAS, The Village has not received any objection from any involved agencies objecting to the Board of Trustees acting as lead agency; and

WHEREAS, the proposal is an Unlisted Action pursuant SEQRA regulations and will require determination of significance of potential environmental impacts prior to adoption.

NOW, THEREFORE, be it

RESOLVED, that the Village of Port Chester Board of Trustees accepts the Amended Petition as presented for review and consideration; and be it further

RESOLVED, that the Village of Port Chester Board of Trustees declares its intent to serve as Lead Agency for the proposed Unlisted Action described herein and as defined in Part 617 of SEQRA regulations; and be it further

RESOLVED, that the Board of Trustees sets a public hearing on this matter on April 4, 2016 at 7:00 p.m. or shortly thereafter at the Village Justice Court Room, 350 North Main Street, Port Chester NY 10573.

Approved as to Form:

Village Attorney, Anthony Cerreto

ROLL CALL

AYES:

NOES:

ABSENT:

DATE: March 7, 2016

BOARD OF TRUSTEES: VILLAGE OF PORT CHESTER
COUNTY OF WESTCHESTER: STATE OF NEW YORK
-----X

In the Matter of the Application of

PORT NORTH MAIN STREET LLC

AMENDED
PETITION

For amendments to §§ 345-14.C.(2), 345-50, 345-61 and 345
Attachment 3B of the Village of Port Chester Zoning Code.

-----X

Port North Main Street LLC (the “Petitioner”), by its attorneys Tartaglia Law Group, LLC,
hereby petitions the Board of Trustees of the Village of Port Chester, as follows:

REQUESTED ZONING AMENDMENTS

1. To accommodate the Proposed Project, the Petitioner is requesting that a new Zoning District be created (“C4-R”) entitled “Gateway Retail,” and the C4-R be mapped to include the portions of the current C-4 District corridor along North Main Street that are from Terrace Avenue to the northern border of the District at or about Hillside Avenue and Riverside Avenue.

2. The following are the requested Zoning Amendments:

SECTION 1: The Code of the Village of Port Chester, Article IV, Section 345-14.C.(2), is hereby amended to read as follows:

Uses	Number of Spaces Required
Retail stores or personal service stores within the C4-R District floor area.	1 per 300 square feet of gross

SECTION 2: The Code of the Village of Port Chester, Article IX, is hereby amended to include § 345-50.3. C4-R Gateway Retail District to read as follows:

§ 345-50.3. C4-R Gateway Retail District

See the Schedule of Regulations for Nonresidence Districts (Attachments 3A and 3B)

SECTION 3: The Code of the Village of Port Chester, Article X, § 345-61 is hereby amended to include § 345-61.Y Retail store or personal service shop within the C4-R District to read as follows:

Y. Retail store or personal service shop within the C4-R District

- (1) In addition to the general standards, set forth in §§ 345-59 and 345-60, for the granting of a special exception use, the Planning Commission shall also consider the following criteria:
 - (a) On parcels adjacent to the Byram River, access shall be provided and/or the site plan and on-site landscaping shall be designed to provide a view scape of the river, to the extent practicable, via viewsheds through parking lots and other non-improved areas of the site.

SECTION 4: The Code of the Village of Port Chester, 345 Attachment 3A, is hereby amended to include:

Type of Use	C4-R Gateway Retail §345-50.3
Residential Uses	
1 Family Dwelling	X
2 Family Dwelling	X
Multifamily Dwelling	X
Multifamily Dwelling (floors above first floor)	X
Residential Community Facilities	
Church or other Place of Worship, Parish House, Rectory, Sunday School, Convent, Seminary	SE
General Community Facilities	
Assembly Hall	SE
Convalescent home or nursing home	X
Hospital	X
Medical and dental offices, not including operating room or community X-ray or therapy room	X
Membership club, fraternal organization or similar social institution not operated for a profit	SE
Nursery school, day camp or day care center	X
Public utility facility	SE
School, elementary or high, public, private, or parochial, having a curriculum equivalent to that ordinarily given in public schools.	X
Medical and dental offices including X-ray and therapy room	X
Business Uses	
Automobile repair garage	SE
Bank, excluding drive-in	P
Bar or Tavern	X
Bowling Alley	SE
Cabaret	X
Catering and Events Establishment	X
Commercial Indoor Athletic Training Facility	X
Drive-in establishments other than restaurant, or circus, carnival, or other outdoor amusements	SE
Drive-in and fast-food restaurant	SE

Food processing shop	SE
Funeral Home	SE
Gasoline Station	SE
Health Club, including racquetball facilities and indoor swimming pools	P
Heating, air conditioning, plumbing, electrical, and similar construction businesses, excluding open storage of materials	P
Hotel or Motel	X
Hotel, Motel (floors above first floor)	X
Hotel, limited service	X
Marina or yacht club	X
Theater	X
Motor Vehicle Sales lot, motor vehicle salesroom and accessory repair shop	SE
Office, Office Building	P
Office, Office Building (floors above first floor)	P
Off-street parking lot or garage for motor vehicles, but not including storage of used or new motor vehicles for sale or hire; minimum requirements shall be in accordance with § 345-14.	P
Pawnshops	X
Radio or television station studio, excluding transmission tower	P
Radio or television station studio excluding transmission tower (floors above first floor)	P
Shooting ranges with accessory sales of guns and equipment	X
Table-service restaurant, no drive-in, open front, fast food, or curb-service types	P
Tax Preparation Office	X
Retail store or personal service shop, services clearly incidental to retail sales on the premises or to personal services	SE
Veterinary hospital or board and care of small animals	SE
Wholesale business, storage building or warehouse	P
Industrial uses	
Cold storage plant, ice plant, bottling central distribution station, light or power plant, or garbage or sewage disposal facility	X
Creamery, ice cream plant or bakery plant	P
Laundry or dry-cleaning plant	X
Nonnuisance industry, provided that in nonindustrial districts equipment is used that has a rating of no more than 5 horsepower	P
Open storage of equipment or materials	X
Printing plant	P
Research laboratory, provided that is shall not be obnoxious by reason of dissemination of smoke, dust, fumes, noise or vibration, or hazardous from fire waste materials or the creation of excessive demands upon municipal services	SE
Accessory Uses	
CD Accessory garden center	X
Customary accessory structure or use, including cultural, recreational, or athletic facility, meeting room or similar accessory structure or use related to a school, church, or other place of worship	P
Ethical Pharmacy	X
Ground-floor office as accessory use to multifamily development	X

Private garage or private off-street parking area, in accordance with § 345-14	P
Sign, in accordance with § 345-15	P

Notes:

- P = permitted use
- SE = special exception use
- X = prohibited use

SECTION 5: The Code of the Village of Port Chester, 345 Attachment 3B, is hereby amended to include:

Type of Use	C4-R Gateway Retail §345-50.3
Maximum Floor Area Ratio (See definition, § 345-2)	2.0
Maximum Floor Area Ratio For 1 Story	0.5
Minimum Size of Lot:	
Area, nonresidential (square feet)	NR
Area per dwelling unit (square feet)	NR
Width (feet) (e)	40
Depth (feet)	NR
Minimum Yard Dimensions:	
Front (feet)	NR
Side	
One (feet)	NR(a)
Total of 2 on interior lot (feet)	NR(a)
Rear (feet)	NR
Maximum Height of Building:	
In stories	3
In feet	45
Minimum Usable Open Space on Lot:	
For each dwelling unit (square feet)	NR

NOTES:

- (a) If provided at least ten (10) feet per yard.
 - (b) One-half (½) the height of the building to which the yard is related, but not greater than twenty (20) feet.
 - (c) Each twenty-five (25) feet or one-half (½) the height of the building, whichever is greater.
 - (d) Twenty (20) square feet for each patient bed in a sanatorium, convalescent home or nursing home.
 - (e) Must be maintained from the minimum front yard depth to the rear lot line.
- 1 A floor area ratio (FAR) bonus of 0.8 is available in accordance with §345-16.
 - 2 A minimum of 575 sq. ft. of lot area per dwelling unit can be achieved through a Building Height and Floor Area Bonus Program in accordance with §345-16.
 - 3 A Floor Area Bonus of 0.5 available in accordance with §345-16.
 - 4 A minimum of 250 sq. ft. of lot area per dwelling unit can be achieved through a Building Height and Floor Area Bonus Program in accordance with §345-16.
 - 5 A minimum of 400 sq. ft. of lot area per dwelling unit can be achieved through a Building Height and Floor Area Bonus Program in accordance with §345-16.
 - 6 A building height bonus of 2 stories (30 feet) is available in accordance with §345-16.
 - 7 A building height bonus of 1 story (10 feet) is available in accordance with §345-16.

8 See §345-7.E.

9 A floor area bonus of 0.20 is available in accordance with §345-16. For any mixed use (commercial/residential) structure, a building height bonus of 2 stories (15 feet) is available in accordance with §345-16.

10 A Floor Area Bonus of 0.2 is available in accordance with §345-16.

11 A minimum of 500 sq. ft. of lot area per dwelling unit can be achieved through a Building Height and Floor Area Bonus Program in accordance with §345-16.

NR = none required

SECTION 6: The Official Zoning Map of the Village of Port Chester is hereby amended to include the C-4R Gateway Retail District, comprising Section 136.64, Block 1, Lots 30, 33, 34, 35, 36, 38, 39, 40, and 61 of the Town of Rye Official Tax Map.

WHEREFORE, the Petitioner respectfully requests that this Amended Petition be granted as set forth herein.

Respectfully submitted,
TARTAGLIA LAW GROUP, LLC
Attorneys for Petitioner
800 Westchester Avenue
Rye Brook, NY 10573
(914) 481-1880

Dated: Rye Brook, New York
February 25, 2016

LOCAL LAW NO. I-1 of 2016

A LOCAL LAW AMENDING THE CODE OF THE VILLAGE OF PORT CHESTER, CHAPTER 345, "ZONING", TO ESTABLISH A NEW GATEWAY RETAIL ("C4-R") ZONING DISTRICT AND TO CHANGE THE OFFICIAL ZONING MAP THEREFOR

Be it enacted by the Board of Trustees of the Village of Port Chester, New York as follows:

SECTION 1: Purpose and Intent

The current C4 General Commercial District designation for the northern portion of North Main Street presents a limitation on realizing the area as a gateway to the Village of Port Chester. The new C4-R Gateway Retail District will facilitate a greater mix of uses, including retail., and promote the continued economic revitalization of the Village. The C4-R District regulations are designed to provide better visual and/or physical access to the Byram River, which is one of the defining characteristics of the area and currently underutilized as a public asset.

SECTION 2: The Code of the Village of Port Chester, Chapter 345, Article IV, Section 345-14 is hereby amended as follows:

Section 345-14C Schedule of Off-Street Parking Space Requirements.

..

(2) For Nonresidential Uses

Uses	Number of Spaces Required
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....

<u>Retail stores or personal service stores within the C4-R District</u>	<u>1 per 300 square feet of gross floor area.</u>
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SECTION 3: The Code of the Village of Port Chester, Chapter 345. Article IX, is hereby amended as follows::

Section 345-50.3. C4-R Gateway Retail District

See the Schedule of Regulations for Nonresidence Districts (Attachments 3A and 3B)

SECTION 4: The Code of the Village of Port Chester, Chapter 345, Article X, Section 345-59 is hereby amended

Section 345-59.D. Jurisdiction to hear specific applications is as follows:

....

(2) Planning Commission

...

(u) Retail store or personal service shop within the C4-R District

SECTION 5: The Code of the Village of Port Chester, Chapter 345, Article X, Section 345-61, is hereby amended as follows:

...

Y. Retail store or personal service shop within the C4-R District

In addition to the general standards, set forth in Sections 345-59 and 345-60, for the granting of a special exception use, the Planning Commission shall also consider the following: On parcels adjacent to the Byram River, access shall be provided and/or the site plan and on-site landscaping shall be designed to provide a view scape of the river, to the extent practicable, via viewsheds through parking lots and other non-improved areas of the site.

SECTION 6: The Code of the Village of Port Chester, Chapter 345, Attachment 3A, Schedule of Regulations for Nonresidence Districts, Part 1, Use Regulations and Attachment 3B Schedule of Regulations for Nonresidence Districts Part 2, Dimensional Regulations. is hereby amended as follows per the annexed:

ZONING

345 Attachment 3A

Village of Port Chester
 Schedule of Regulations for Nonresidence Districts
 Part 1, Use Regulations

Type of Use	Districts														
	C1 Neighborhood Retail §345-47	C2 Main Street Business §345-48	C3 Office and Commercial §345-49	C4 General Commercial §345-50	C4-R Gateway Retail §345-50.3	C5 Train Station Mixed Use §345-50.1	C5T Downtown Mixed Use Transitional §345-50.2	CD Design Shopping Center §345-51	CDS Special Designed Commercial §345-52	PD Design Professional Building §345-53	DW Design Waterfront §345-54	DW2 Downtown Design Waterfront §345-54.1	M1 Light Industrial §345-55	M2 General Industrial §345-561	PMU Planned Mixed Use §345-57
Residential Uses															
1 Family Dwelling	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
2 Family Dwelling	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Multifamily Dwelling	X	SE	X	X	X	SE	SE	X	X	X	SE	SE	X	X	P
Multifamily Dwelling (floors above first floor)	X	SE	X	X	X	P	P	X	X	X	SE	SE	X	X	P
Residential Community Facilities															
Church or other Place of Worship, Parish House, Rectory, Sunday School, Convent, Seminary	SE	SE	SE	SE	SE	SE	SE	SE	X	X	SE	SE	SE	SE	SE
General Community Facilities															
Assembly Hall	X	SE	SE	SE	SE	SE	SE	X	X	X	X	X	X	X	P
Convalescent home or nursing home	X	X	SE	X	X	X	X	X	X	X	SE	SE	X	X	P
Hospital	X	SE	SE	X	X	SE	SE	X	X	X	X	X	X	X	SE
Medical and dental offices, not including operating room or community X-ray or therapy room	X	SE	P	X	X	SE	SE	P	P	P	X	X	X	X	SE
Membership club, fraternal organization or similar social institution not operated for a profit	X	P	SE	SE	SE	P	P	X	X	X	SE	SE	X	X	P
Nursery school, day camp or day care center	SE	SE	SE	X	X	SE	SE	X	X	X	X	X	X	X	SE
Public utility facility	X	SE	SE	SE	SE	SE	SE	X	X	X	SE	SE	P	P	SE
School, elementary or high, public, private, or parochial, having a curriculum equivalent to that ordinarily given in public schools.	X	SE	X	X	X	SE	SE	X	X	X	SE	SE	X	X	SE
Medical and dental offices including X-ray and therapy room	X	SE	X	X	X	SE	SE	X	X	X	X	X	X	X	SE
Business Uses															
Automobile repair garage	X	X	X	SE	SE	X	X	X	X	X	X	X	X	X	X
Bank, excluding drive-in	P	P	P	P	P	P	P	P	P	P	SE	SE	P	P	P
Bar or Tavern	X	P	X	X	X	P	P	X	X	X	X	X	X	X	P
Bowling Alley	X	SE	SE	SE	SE	SE	SE	P	P	X	X	X	X	X	P
Cabaret	X	P	X	X	X	P	P	X	X	X	X	X	X	X	P
Catering and Events Establishment	X	P	X	X	X	P	P	X	X	X	X	X	P	P	P
Commercial Indoor Athletic Training Facility	X	SE	X	X	X	SE	SE	X	X	X	X	X	X	P	P
Drive-in establishments other than restaurant, or circus, carnival, or other outdoor amusements	X	X	SE	SE	SE	X	X	SE	SE	X	X	X	X	X	X
Drive-in and fast-food restaurant	X	X	X	SE	SE	X	X	X	X	X	X	X	X	X	X
Food processing shop	X	X	X	SE	SE	X	X	X	X	X	X	X	X	X	X
Funeral Home	SE	SE	SE	SE	SE	SE	SE	X	X	X	X	X	X	X	SE
Gasoline Station	X	X	X	SE	SE	X	X	X	X	X	X	X	SE	SE	X
Health Club, including racquetball facilities and indoor swimming pools	X	P	P	P	P	P	P	P	P	X	X	X	X	X	P
Heating, air conditioning, plumbing, electrical, and similar construction businesses, excluding open storage of materials	X	X	X	P	P	X	X	X	X	X	SE	X	P	P	X

**Village of Port Chester
Schedule of Regulations for Nonresidence Districts
Part 1, Use Regulations (Cont'd)**

Type of Use	Districts														
	C1 Neighborhood Retail §345-47	C2 Main Street Business §345-48	C3 Office and Commercial §345-49	C4 General Commercial §345-50	C4-R Gateway Retail §345-50.3	C5 Train Station Mixed Use §345-50.1	C5T Downtown Mixed Use Transitional §345-50.2	CD Design Shopping Center §345-51	CDS Special Designed Commercial §345-52	PD Design Professional Building §345-53	DW Design Waterfront §345-54	DW2 Downtown Design Waterfront §345-54.1	M1 Light Industrial §345-55	M2 General Industrial §345-561	PMU Planned Mixed Use §345-57
Hotel or Motel	X	P	P	X	X	P	P	X	X	SE	SE	X	X	P	
Hotel, Motel (floors above first floor)	X	P	P	X	X	P	P	X	X	SE	SE	X	X	P	
Hotel, limited service	X	P	SE	X	X	P	P	SE	X	X	SE	SE	X	X	P
Marina or yacht club	X	X	X	X	X	X	X	X	X	SE	SE	X	X	X	
Theater	X	P	P	X	X	P	P	X	X	SE	SE	P	P	P	
Motor Vehicle Sales lot, motor vehicle salesroom and accessory repair shop	X	X	X	SE	SE	X	X	X	X	X	X	X	X	X	
Office, Office Building	P	SE	P	P	P	SE	SE	P	P	P	SE	SE	P	P	P
Office, Office Building (floors above first floor)	P	P	P	P	P	P	P	P	P	SE	SE	P	P	P	
Off-street parking lot or garage for motor vehicles, but not including storage of used or new motor vehicles for sale or hire; minimum requirements shall be in accordance with § 345-14.	X	P	P	P	P	P	P	X	X	X	SE	SE	X	X	P
Pawnshops	X	X	X	X	X	X	X	X	X	X	X	SE	SE	X	
Radio or television station studio, excluding transmission tower	X	SE	P	P	P	SE	SE	X	X	X	SE	SE	P	P	SE
Radio or television station studio excluding transmission tower (floors above first floor)	X	P	P	P	P	P	P	X	X	X	SE	SE	P	P	SE
Shooting ranges with accessory sales of guns and equipment	X	X	X	X	X	X	X	X	X	X	X	X	P	X	X
Table-service restaurant, no drive-in, open front, fast food, or curb-service types	P	P	P	P	P	P	P	P	P	X	SE	SE	X	X	P
Tax Preparation Office	X	P	X	X	X	P	P	X	X	X	SE	SE	X	X	P
Retail store or personal service shop, services clearly incidental to retail sales on the premises or to personal services	P	P	P	X	SE	P	P	P	SE	X	SE	SE	X	X	P
Veterinary hospital or board and care of small animals	X	SE	X	SE	SE	SE	SE	X	X	X	SE	SE	X	X	SE
Wholesale business, storage building or warehouse	P	X	X	P	P	X	X	X	X	X	SE	SE	P	P	X
Industrial uses															
Cold storage plant, ice plant, bottling central distribution station, light or power plant, or garbage or sewage disposal facility	X	X	X	X	X	X	X	X	X	X	X	X	X	P	X
Creamery, ice cream plant or bakery plant	X	X	X	P	P	X	X	X	X	SE	SE	P	P	X	
Laundry or dry-cleaning plant	X	X	X	X	X	X	X	X	X	SE	SE	P	P	X	
Nonnuisance industry, provided that in nonindustrial districts equipment is used that has a rating of no more than 5 horsepower	X	X	X	P	P	X	X	X	X	SE	SE	P	P	X	
Open storage of equipment or materials	X	X	X	X	X	X	X	X	X	X	X	X	SE	X	X
Printing plant	X	X	P	P	P	X	X	X	X	SE	SE	P	P	X	
Research laboratory, provided that is shall not be obnoxious by reason of dissemination of smoke, dust, fumes, noise or vibration, or hazardous from fire waste materials or the creation of excessive demands upon municipal services	X	X	SE	SE	SE	X	X	X	X	SE	SE	P	P	X	
Accessory Uses															
X	X	P	X	X	X	X	X	P	X	X	X	X	X	X	X
Customary accessory structure or use, including cultural, recreational, or athletic facility, meeting room or similar accessory structure or use related to a school, church, or other place of worship	P	P	P	P	P	P	P	P	P	SE	SE	P	P	SE	
Ethical Pharmacy	X	SE	X	X	X	SE	SE	P	X	P	X	X	X	SE	
Ground-floor office as accessory use to multifamily development	X	SE	X	X	X	SE	SE	X	X	X	SE	SE	X	X	P
Private garage or private off-street parking area, in accordance with § 345-14	P	P	P	P	P	P	P	P	SE	P	SE	SE	P	P	P
Sign, in accordance with § 345-15	P	P	P	P	P	P	P	P	P	SE	SE	P	P	P	

Notes:
P = permitted use
SE = special exception use
X = prohibited use

Village of Port Chester
 Schedule of Regulations for Nonresidence Districts
 Part 2, Dimensional Regulations

	Districts															
	C1 Neighborhood Retail § 345-47	C2 Main Street Business § 345-48	C3 Office & Commercial § 345-49	C4 General Commercial § 345-50	<u>C4-R Gateway Retail § 345-50.3</u>	C5 Train Station Mixed Use § 345-50.1	C5T Downtown Mixed Use Transitional District § 345-50.2	CD Design Shopping Center § 345-51	CDS Special Designed Commercial § 345-52	PD Design Professional Building § 345-53	DW Design Waterfront § 345-54	DW2 Downtown Design Waterfront § 345-54.1	M1 Light Industrial § 345-55	M2 General Industrial § 345-56	PMU Planned Mixed Use § 345-57	
Maximum Floor Area Ratio (See definition, § 345-2)	1.0	3.2 ¹	3.0	2.0	<u>2.0</u>	4.0 ³	4.0 ³	1.0	1.0	1.0	1.6	2.4 ¹⁰	1.0	2.0	0.8 ⁹	
Maximum Floor Area Ratio For 1 Story	0.35	NR	0.60	NR	<u>0.50</u>	NR	NR	0.35	NR	NR	0.40	NR	0.50	0.70	See §345-62	
Minimum Size of Lot:																
Area, nonresidential (square feet)	NR	NR	NR	NR	<u>NR</u>	NR	NR	21,780	65,340	21,780	10,000	NR	NR	NR		
Area per dwelling unit (square feet)	NR	750 ²	NR	NR	<u>NR</u>	400 ⁴	575 ⁵	NR	NR	NR	750	600 ¹¹	NR	NR		
Width (feet) (e)	40	40	40	40	<u>40</u>	40	40	40	NR	40	40	40	40	40		
Depth (feet)	NR	NR	NR	NR	<u>NR</u>	NR	NR	NR	NR	NR	NR	NR	NR	NR		
Minimum Yard Dimensions:																
Front (feet)	NR	NR	10	10	<u>NR</u>	NR	NR	30	60	10	20	5	25	10		
Side (feet)										-						
1	NR(a)	NR(a)	(b)	NR(a)	<u>NR(a)</u>	NR(a)	NR(a)	30	50	20	(c)	5	20	10		
Total of 2 on interior lot	NR(a)	NR(a)	(b)	NR(a)	<u>NR(a)</u>	NR(a)	NR(a)	60	100	40	(c)	10	40	20		
Rear (feet)	30	20	20	20	<u>NR</u>	20	20	30	50	30	30	25	20	20		
Maximum Height of Building:																
In stories	2½	5	5	3	<u>3</u>	8 ⁶	5 ⁷	3	3	5	4	4	2	8		
In feet	35	60	60	45	<u>45</u>	90 ⁶	60 ⁷	45	45	60	50	50	40	70		
Minimum Usable Open Space on Lot:																
For each dwelling unit (square feet)	1,200	50 ⁸	NR(d)	NR	<u>NR</u>	50 ⁸	50 ⁸	NR	NR	100(d)	50 ⁸	50 ⁸	NR	NR	100	

NOTES:

- (a) If provided at least 10 feet per yard.
- (b) One-half the height of the building to which the yard is related, but not greater than 20 feet.
- (c) Each 25 feet or ½ the height of the building, whichever is greater.
- (d) For each patient bed in a sanatorium, convalescent home or nursing home: 20 feet.
- (e) Must be maintained from the minimum front yard depth to the rear lot line.

¹ A floor area ratio (FAR) bonus of 0.8 is available in accordance with §345-16.

² A minimum of 575 square feet of lot area per dwelling unit can be achieved through a building height and floor area bonus program in accordance with §345-16.

³ A floor area bonus of 0.5 is available in accordance with §345-16.

⁴ A minimum of 250 square feet of lot area per dwelling unit can be achieved through a building height and floor area bonus program in accordance with §345-16.

⁵ A minimum of 400 square feet of lot area per dwelling unit can be achieved through a building height and floor area bonus program in accordance with §345-16.

⁶ A building height bonus of two stories (30 feet) is available in accordance with §345-16.

⁷ A building height bonus of one story (10 feet) is available in accordance with §345-16.

⁸ See §345-7.E.

⁹ A floor area bonus of 0.20 is available in accordance with §345-16. For any mixed-use (commercial/residential) structure, a building height bonus of two stories (15 feet) is available in accordance with §345-16.

¹⁰ A floor area bonus of 0.2 is available in accordance with §345-16.

¹¹ A minimum of 500 square feet of lot area per dwelling unit can be achieved through a building height and floor area bonus program in accordance with §345-16.

NR = none required

SECTION 7: Official Zoning Map

The Official Zoning Map of the Village of Port Chester is hereby amended to permit the following properties to be changed from a C-4 Zoning District to a C-4R Gateway Retail District and be thereafter be subject to the requirements and regulations of such latter district as described in the Village Code, Chapter 345, and Zoning Regulation:

Section 136.64, Block 1, Lots 30, 33, 34, 35, 36, 38, 39, 40, and 61 of the Town of Rye Official Tax Map.

SECTION 8: Severability

If any provision of this local law is held to be invalid or unenforceable in whole or in part, such invalidity or unenforceability shall attach only to such provision or part thereof and the remaining part of such provision and all other provisions shall continue to be in full force and effect.

SECTION 9: Effective Date

This local law shall take effect immediately as provided by law upon due publication and filing with the Secretary of State.

Full Environmental Assessment Form
Part 1 - Project and Setting

Instructions for Completing Part 1

Part 1 is to be completed by the applicant or project sponsor. Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification.

Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information; indicate whether missing information does not exist, or is not reasonably available to the sponsor; and, when possible, generally describe work or studies which would be necessary to update or fully develop that information.

Applicants/sponsors must complete all items in Sections A & B. In Sections C, D & E, most items contain an initial question that must be answered either “Yes” or “No”. If the answer to the initial question is “Yes”, complete the sub-questions that follow. If the answer to the initial question is “No”, proceed to the next question. Section F allows the project sponsor to identify and attach any additional information. Section G requires the name and signature of the project sponsor to verify that the information contained in Part 1 is accurate and complete.

A. Project and Sponsor Information.

Name of Action or Project:		
Project Location (describe, and attach a general location map):		
Brief Description of Proposed Action (include purpose or need):		
Name of Applicant/Sponsor:		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:
Project Contact (if not same as sponsor; give name and title/role):		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:
Property Owner (if not same as sponsor):		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:

B. Government Approvals

B. Government Approvals, Funding, or Sponsorship. (“Funding” includes grants, loans, tax relief, and any other forms of financial assistance.)

Government Entity	If Yes: Identify Agency and Approval(s) Required	Application Date (Actual or projected)
a. City Council, Town Board, or Village Board of Trustees <input type="checkbox"/> Yes <input type="checkbox"/> No		
b. City, Town or Village Planning Board or Commission <input type="checkbox"/> Yes <input type="checkbox"/> No		
c. City Council, Town or Village Zoning Board of Appeals <input type="checkbox"/> Yes <input type="checkbox"/> No		
d. Other local agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
e. County agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
f. Regional agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
g. State agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
h. Federal agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
i. Coastal Resources. <ul style="list-style-type: none"> <li data-bbox="121 829 1485 861">i. Is the project site within a Coastal Area, or the waterfront area of a Designated Inland Waterway? <input type="checkbox"/> Yes <input type="checkbox"/> No <li data-bbox="121 892 1485 924">ii. Is the project site located in a community with an approved Local Waterfront Revitalization Program? <input type="checkbox"/> Yes <input type="checkbox"/> No <li data-bbox="121 924 1485 955">iii. Is the project site within a Coastal Erosion Hazard Area? <input type="checkbox"/> Yes <input type="checkbox"/> No 		

C. Planning and Zoning

C.1. Planning and zoning actions.

Will administrative or legislative adoption, or amendment of a plan, local law, ordinance, rule or regulation be the only approval(s) which must be granted to enable the proposed action to proceed? Yes No

- **If Yes**, complete sections C, F and G.
- **If No**, proceed to question C.2 and complete all remaining sections and questions in Part 1

C.2. Adopted land use plans.

a. Do any municipally- adopted (city, town, village or county) comprehensive land use plan(s) include the site where the proposed action would be located? Yes No

If Yes, does the comprehensive plan include specific recommendations for the site where the proposed action would be located? Yes No

b. Is the site of the proposed action within any local or regional special planning district (for example: Greenway Brownfield Opportunity Area (BOA); designated State or Federal heritage area; watershed management plan; or other?) Yes No

If Yes, identify the plan(s):

c. Is the proposed action located wholly or partially within an area listed in an adopted municipal open space plan, or an adopted municipal farmland protection plan? Yes No

If Yes, identify the plan(s):

C.3. Zoning

a. Is the site of the proposed action located in a municipality with an adopted zoning law or ordinance. Yes No
If Yes, what is the zoning classification(s) including any applicable overlay district?

b. Is the use permitted or allowed by a special or conditional use permit? Yes No

c. Is a zoning change requested as part of the proposed action? Yes No

If Yes,

i. What is the proposed new zoning for the site? _____

C.4. Existing community services.

a. In what school district is the project site located? _____

b. What police or other public protection forces serve the project site?

c. Which fire protection and emergency medical services serve the project site?

d. What parks serve the project site?

D. Project Details

D.1. Proposed and Potential Development

a. What is the general nature of the proposed action (e.g., residential, industrial, commercial, recreational; if mixed, include all components)?

b. a. Total acreage of the site of the proposed action? _____ acres
b. Total acreage to be physically disturbed? _____ acres
c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor? _____ acres

c. Is the proposed action an expansion of an existing project or use? Yes No
i. If Yes, what is the approximate percentage of the proposed expansion and identify the units (e.g., acres, miles, housing units, square feet)? % _____ Units: _____

d. Is the proposed action a subdivision, or does it include a subdivision? Yes No
If Yes,

i. Purpose or type of subdivision? (e.g., residential, industrial, commercial; if mixed, specify types)

ii. Is a cluster/conservation layout proposed? Yes No

iii. Number of lots proposed? _____

iv. Minimum and maximum proposed lot sizes? Minimum _____ Maximum _____

e. Will proposed action be constructed in multiple phases? Yes No

i. If No, anticipated period of construction: _____ months

ii. If Yes:

- Total number of phases anticipated _____
- Anticipated commencement date of phase 1 (including demolition) _____ month _____ year
- Anticipated completion date of final phase _____ month _____ year

• Generally describe connections or relationships among phases, including any contingencies where progress of one phase may determine timing or duration of future phases: _____

f. Does the project include new residential uses? Yes No
 If Yes, show numbers of units proposed.

	<u>One Family</u>	<u>Two Family</u>	<u>Three Family</u>	<u>Multiple Family (four or more)</u>
Initial Phase	_____	_____	_____	_____
At completion	_____	_____	_____	_____
of all phases	_____	_____	_____	_____

g. Does the proposed action include new non-residential construction (including expansions)? Yes No
 If Yes,

i. Total number of structures _____

ii. Dimensions (in feet) of largest proposed structure: _____ height; _____ width; and _____ length

iii. Approximate extent of building space to be heated or cooled: _____ square feet

h. Does the proposed action include construction or other activities that will result in the impoundment of any liquids, such as creation of a water supply, reservoir, pond, lake, waste lagoon or other storage? Yes No
 If Yes,

i. Purpose of the impoundment: _____

ii. If a water impoundment, the principal source of the water: Ground water Surface water streams Other specify: _____

iii. If other than water, identify the type of impounded/contained liquids and their source. _____

iv. Approximate size of the proposed impoundment. Volume: _____ million gallons; surface area: _____ acres

v. Dimensions of the proposed dam or impounding structure: _____ height; _____ length

vi. Construction method/materials for the proposed dam or impounding structure (e.g., earth fill, rock, wood, concrete): _____

D.2. Project Operations

a. Does the proposed action include any excavation, mining, or dredging, during construction, operations, or both? Yes No
 (Not including general site preparation, grading or installation of utilities or foundations where all excavated materials will remain onsite)
 If Yes:

i. What is the purpose of the excavation or dredging? _____

ii. How much material (including rock, earth, sediments, etc.) is proposed to be removed from the site?

- Volume (specify tons or cubic yards): _____
- Over what duration of time? _____

iii. Describe nature and characteristics of materials to be excavated or dredged, and plans to use, manage or dispose of them. _____

iv. Will there be onsite dewatering or processing of excavated materials? Yes No
 If yes, describe. _____

v. What is the total area to be dredged or excavated? _____ acres

vi. What is the maximum area to be worked at any one time? _____ acres

vii. What would be the maximum depth of excavation or dredging? _____ feet

viii. Will the excavation require blasting? Yes No

ix. Summarize site reclamation goals and plan: _____

b. Would the proposed action cause or result in alteration of, increase or decrease in size of, or encroachment into any existing wetland, waterbody, shoreline, beach or adjacent area? Yes No
 If Yes:

i. Identify the wetland or waterbody which would be affected (by name, water index number, wetland map number or geographic description): _____

ii. Describe how the proposed action would affect that waterbody or wetland, e.g. excavation, fill, placement of structures, or alteration of channels, banks and shorelines. Indicate extent of activities, alterations and additions in square feet or acres:

iii. Will proposed action cause or result in disturbance to bottom sediments? Yes No

If Yes, describe: _____

iv. Will proposed action cause or result in the destruction or removal of aquatic vegetation? Yes No

If Yes:

- acres of aquatic vegetation proposed to be removed: _____
- expected acreage of aquatic vegetation remaining after project completion: _____
- purpose of proposed removal (e.g. beach clearing, invasive species control, boat access): _____
- proposed method of plant removal: _____
- if chemical/herbicide treatment will be used, specify product(s): _____

v. Describe any proposed reclamation/mitigation following disturbance: _____

c. Will the proposed action use, or create a new demand for water? Yes No

If Yes:

i. Total anticipated water usage/demand per day: _____ gallons/day

ii. Will the proposed action obtain water from an existing public water supply? Yes No

If Yes:

- Name of district or service area: _____
- Does the existing public water supply have capacity to serve the proposal? Yes No
- Is the project site in the existing district? Yes No
- Is expansion of the district needed? Yes No
- Do existing lines serve the project site? Yes No

iii. Will line extension within an existing district be necessary to supply the project? Yes No

If Yes:

- Describe extensions or capacity expansions proposed to serve this project: _____
- Source(s) of supply for the district: _____

iv. Is a new water supply district or service area proposed to be formed to serve the project site? Yes No

If Yes:

- Applicant/sponsor for new district: _____
- Date application submitted or anticipated: _____
- Proposed source(s) of supply for new district: _____

v. If a public water supply will not be used, describe plans to provide water supply for the project: _____

vi. If water supply will be from wells (public or private), maximum pumping capacity: _____ gallons/minute.

d. Will the proposed action generate liquid wastes? Yes No

If Yes:

i. Total anticipated liquid waste generation per day: _____ gallons/day

ii. Nature of liquid wastes to be generated (e.g., sanitary wastewater, industrial; if combination, describe all components and approximate volumes or proportions of each): _____

iii. Will the proposed action use any existing public wastewater treatment facilities? Yes No

If Yes:

- Name of wastewater treatment plant to be used: _____
- Name of district: _____
- Does the existing wastewater treatment plant have capacity to serve the project? Yes No
- Is the project site in the existing district? Yes No
- Is expansion of the district needed? Yes No

- Do existing sewer lines serve the project site? Yes No
- Will line extension within an existing district be necessary to serve the project? Yes No

 If Yes:

- Describe extensions or capacity expansions proposed to serve this project: _____

iv. Will a new wastewater (sewage) treatment district be formed to serve the project site? Yes No
 If Yes:

- Applicant/sponsor for new district: _____
- Date application submitted or anticipated: _____
- What is the receiving water for the wastewater discharge? _____

v. If public facilities will not be used, describe plans to provide wastewater treatment for the project, including specifying proposed receiving water (name and classification if surface discharge, or describe subsurface disposal plans):

vi. Describe any plans or designs to capture, recycle or reuse liquid waste: _____

e. Will the proposed action disturb more than one acre and create stormwater runoff, either from new point sources (i.e. ditches, pipes, swales, curbs, gutters or other concentrated flows of stormwater) or non-point source (i.e. sheet flow) during construction or post construction? Yes No
 If Yes:

- How much impervious surface will the project create in relation to total size of project parcel?
 _____ Square feet or _____ acres (impervious surface)
 _____ Square feet or _____ acres (parcel size)
- Describe types of new point sources. _____

- Where will the stormwater runoff be directed (i.e. on-site stormwater management facility/structures, adjacent properties, groundwater, on-site surface water or off-site surface waters)?

 - If to surface waters, identify receiving water bodies or wetlands: _____

 - Will stormwater runoff flow to adjacent properties? Yes No

iv. Does proposed plan minimize impervious surfaces, use pervious materials or collect and re-use stormwater? Yes No

f. Does the proposed action include, or will it use on-site, one or more sources of air emissions, including fuel combustion, waste incineration, or other processes or operations? Yes No
 If Yes, identify:

- Mobile sources during project operations (e.g., heavy equipment, fleet or delivery vehicles)

- Stationary sources during construction (e.g., power generation, structural heating, batch plant, crushers)

- Stationary sources during operations (e.g., process emissions, large boilers, electric generation)

g. Will any air emission sources named in D.2.f (above), require a NY State Air Registration, Air Facility Permit, or Federal Clean Air Act Title IV or Title V Permit? Yes No
 If Yes:

- Is the project site located in an Air quality non-attainment area? (Area routinely or periodically fails to meet ambient air quality standards for all or some parts of the year) Yes No
- In addition to emissions as calculated in the application, the project will generate:
 - _____ Tons/year (short tons) of Carbon Dioxide (CO₂)
 - _____ Tons/year (short tons) of Nitrous Oxide (N₂O)
 - _____ Tons/year (short tons) of Perfluorocarbons (PFCs)
 - _____ Tons/year (short tons) of Sulfur Hexafluoride (SF₆)
 - _____ Tons/year (short tons) of Carbon Dioxide equivalent of Hydroflouorocarbons (HFCs)
 - _____ Tons/year (short tons) of Hazardous Air Pollutants (HAPs)

h. Will the proposed action generate or emit methane (including, but not limited to, sewage treatment plants, landfills, composting facilities)? Yes No

If Yes:

i. Estimate methane generation in tons/year (metric): _____

ii. Describe any methane capture, control or elimination measures included in project design (e.g., combustion to generate heat or electricity, flaring): _____

i. Will the proposed action result in the release of air pollutants from open-air operations or processes, such as quarry or landfill operations? Yes No

If Yes: Describe operations and nature of emissions (e.g., diesel exhaust, rock particulates/dust):

j. Will the proposed action result in a substantial increase in traffic above present levels or generate substantial new demand for transportation facilities or services? Yes No

If Yes:

i. When is the peak traffic expected (Check all that apply): Morning Evening Weekend
 Randomly between hours of _____ to _____.

ii. For commercial activities only, projected number of semi-trailer truck trips/day: _____

iii. Parking spaces: Existing _____ Proposed _____ Net increase/decrease _____

iv. Does the proposed action include any shared use parking? Yes No

v. If the proposed action includes any modification of existing roads, creation of new roads or change in existing access, describe:

vi. Are public/private transportation service(s) or facilities available within 1/2 mile of the proposed site? Yes No

vii. Will the proposed action include access to public transportation or accommodations for use of hybrid, electric or other alternative fueled vehicles? Yes No

viii. Will the proposed action include plans for pedestrian or bicycle accommodations for connections to existing pedestrian or bicycle routes? Yes No

k. Will the proposed action (for commercial or industrial projects only) generate new or additional demand for energy? Yes No

If Yes:

i. Estimate annual electricity demand during operation of the proposed action: _____

ii. Anticipated sources/suppliers of electricity for the project (e.g., on-site combustion, on-site renewable, via grid/local utility, or other):

iii. Will the proposed action require a new, or an upgrade to, an existing substation? Yes No

l. Hours of operation. Answer all items which apply.

<p><i>i.</i> During Construction:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ • Saturday: _____ • Sunday: _____ • Holidays: _____ 	<p><i>ii.</i> During Operations:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ • Saturday: _____ • Sunday: _____ • Holidays: _____
---	--

m. Will the proposed action produce noise that will exceed existing ambient noise levels during construction, operation, or both? Yes No
 If yes:
 i. Provide details including sources, time of day and duration:

ii. Will proposed action remove existing natural barriers that could act as a noise barrier or screen? Yes No
 Describe: _____

n. Will the proposed action have outdoor lighting? Yes No
 If yes:
 i. Describe source(s), location(s), height of fixture(s), direction/aim, and proximity to nearest occupied structures:

ii. Will proposed action remove existing natural barriers that could act as a light barrier or screen? Yes No
 Describe: _____

o. Does the proposed action have the potential to produce odors for more than one hour per day? Yes No
 If Yes, describe possible sources, potential frequency and duration of odor emissions, and proximity to nearest occupied structures: _____

p. Will the proposed action include any bulk storage of petroleum (combined capacity of over 1,100 gallons) or chemical products 185 gallons in above ground storage or any amount in underground storage? Yes No
 If Yes:
 i. Product(s) to be stored _____
 ii. Volume(s) _____ per unit time _____ (e.g., month, year)
 iii. Generally describe proposed storage facilities: _____

q. Will the proposed action (commercial, industrial and recreational projects only) use pesticides (i.e., herbicides, insecticides) during construction or operation? Yes No
 If Yes:
 i. Describe proposed treatment(s):

ii. Will the proposed action use Integrated Pest Management Practices? Yes No

r. Will the proposed action (commercial or industrial projects only) involve or require the management or disposal of solid waste (excluding hazardous materials)? Yes No
 If Yes:
 i. Describe any solid waste(s) to be generated during construction or operation of the facility:
 • Construction: _____ tons per _____ (unit of time)
 • Operation : _____ tons per _____ (unit of time)
 ii. Describe any proposals for on-site minimization, recycling or reuse of materials to avoid disposal as solid waste:
 • Construction: _____

 • Operation: _____

iii. Proposed disposal methods/facilities for solid waste generated on-site:
 • Construction: _____

 • Operation: _____

s. Does the proposed action include construction or modification of a solid waste management facility? Yes No
 If Yes:
 i. Type of management or handling of waste proposed for the site (e.g., recycling or transfer station, composting, landfill, or other disposal activities): _____
 ii. Anticipated rate of disposal/processing:
 • _____ Tons/month, if transfer or other non-combustion/thermal treatment, or
 • _____ Tons/hour, if combustion or thermal treatment
 iii. If landfill, anticipated site life: _____ years

t. Will proposed action at the site involve the commercial generation, treatment, storage, or disposal of hazardous waste? Yes No
 If Yes:
 i. Name(s) of all hazardous wastes or constituents to be generated, handled or managed at facility: _____

 ii. Generally describe processes or activities involving hazardous wastes or constituents: _____

 iii. Specify amount to be handled or generated _____ tons/month
 iv. Describe any proposals for on-site minimization, recycling or reuse of hazardous constituents: _____

 v. Will any hazardous wastes be disposed at an existing offsite hazardous waste facility? Yes No
 If Yes: provide name and location of facility: _____

 If No: describe proposed management of any hazardous wastes which will not be sent to a hazardous waste facility:

E. Site and Setting of Proposed Action

E.1. Land uses on and surrounding the project site

a. Existing land uses.
 i. Check all uses that occur on, adjoining and near the project site.
 Urban Industrial Commercial Residential (suburban) Rural (non-farm)
 Forest Agriculture Aquatic Other (specify): _____
 ii. If mix of uses, generally describe:

b. Land uses and covertypes on the project site.

Land use or Covertypes	Current Acreage	Acreage After Project Completion	Change (Acres +/-)
• Roads, buildings, and other paved or impervious surfaces			
• Forested			
• Meadows, grasslands or brushlands (non-agricultural, including abandoned agricultural)			
• Agricultural (includes active orchards, field, greenhouse etc.)			
• Surface water features (lakes, ponds, streams, rivers, etc.)			
• Wetlands (freshwater or tidal)			
• Non-vegetated (bare rock, earth or fill)			
• Other Describe: _____ _____			

c. Is the project site presently used by members of the community for public recreation? Yes No
i. If Yes: explain: _____

d. Are there any facilities serving children, the elderly, people with disabilities (e.g., schools, hospitals, licensed day care centers, or group homes) within 1500 feet of the project site? Yes No
If Yes,
i. Identify Facilities:

e. Does the project site contain an existing dam? Yes No
If Yes:
i. Dimensions of the dam and impoundment:

- Dam height: _____ feet
- Dam length: _____ feet
- Surface area: _____ acres
- Volume impounded: _____ gallons OR acre-feet

ii. Dam's existing hazard classification: _____
iii. Provide date and summarize results of last inspection:

f. Has the project site ever been used as a municipal, commercial or industrial solid waste management facility, or does the project site adjoin property which is now, or was at one time, used as a solid waste management facility? Yes No
If Yes:
i. Has the facility been formally closed? Yes No

- If yes, cite sources/documentation: _____

ii. Describe the location of the project site relative to the boundaries of the solid waste management facility:

g. Have hazardous wastes been generated, treated and/or disposed of at the site, or does the project site adjoin property which is now or was at one time used to commercially treat, store and/or dispose of hazardous waste? Yes No
If Yes:
i. Describe waste(s) handled and waste management activities, including approximate time when activities occurred:

h. Potential contamination history. Has there been a reported spill at the proposed project site, or have any remedial actions been conducted at or adjacent to the proposed site? Yes No
If Yes:
i. Is any portion of the site listed on the NYSDEC Spills Incidents database or Environmental Site Remediation database? Check all that apply: Yes No
 Yes – Spills Incidents database Provide DEC ID number(s): _____
 Yes – Environmental Site Remediation database Provide DEC ID number(s): _____
 Neither database
ii. If site has been subject of RCRA corrective activities, describe control measures: _____

iii. Is the project within 2000 feet of any site in the NYSDEC Environmental Site Remediation database? Yes No
If yes, provide DEC ID number(s): _____
iv. If yes to (i), (ii) or (iii) above, describe current status of site(s):

v. Is the project site subject to an institutional control limiting property uses? Yes No

- If yes, DEC site ID number: _____
- Describe the type of institutional control (e.g., deed restriction or easement): _____
- Describe any use limitations: _____
- Describe any engineering controls: _____
- Will the project affect the institutional or engineering controls in place? Yes No
- Explain: _____

E.2. Natural Resources On or Near Project Site

a. What is the average depth to bedrock on the project site? _____ feet

b. Are there bedrock outcroppings on the project site? Yes No
 If Yes, what proportion of the site is comprised of bedrock outcroppings? _____%

c. Predominant soil type(s) present on project site: _____ %
 _____ %
 _____ %

d. What is the average depth to the water table on the project site? Average: _____ feet

e. Drainage status of project site soils: Well Drained: _____ % of site
 Moderately Well Drained: _____ % of site
 Poorly Drained _____ % of site

f. Approximate proportion of proposed action site with slopes: 0-10%: _____ % of site
 10-15%: _____ % of site
 15% or greater: _____ % of site

g. Are there any unique geologic features on the project site? Yes No
 If Yes, describe: _____

h. Surface water features.

i. Does any portion of the project site contain wetlands or other waterbodies (including streams, rivers, ponds or lakes)? Yes No

ii. Do any wetlands or other waterbodies adjoin the project site? Yes No
 If Yes to either *i* or *ii*, continue. If No, skip to E.2.i.

iii. Are any of the wetlands or waterbodies within or adjoining the project site regulated by any federal, state or local agency? Yes No

iv. For each identified regulated wetland and waterbody on the project site, provide the following information:

- Streams: Name _____ Classification _____
- Lakes or Ponds: Name _____ Classification _____
- Wetlands: Name _____ Approximate Size _____
- Wetland No. (if regulated by DEC) _____

v. Are any of the above water bodies listed in the most recent compilation of NYS water quality-impaired waterbodies? Yes No
 If yes, name of impaired water body/bodies and basis for listing as impaired: _____

i. Is the project site in a designated Floodway? Yes No

j. Is the project site in the 100 year Floodplain? Yes No

k. Is the project site in the 500 year Floodplain? Yes No

l. Is the project site located over, or immediately adjoining, a primary, principal or sole source aquifer? Yes No
 If Yes:
 i. Name of aquifer: _____

<p>m. Identify the predominant wildlife species that occupy or use the project site: _____ _____ _____</p>	
<p>n. Does the project site contain a designated significant natural community? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> Describe the habitat/community (composition, function, and basis for designation): _____ _____ <i>ii.</i> Source(s) of description or evaluation: _____ <i>iii.</i> Extent of community/habitat: • Currently: _____ acres • Following completion of project as proposed: _____ acres • Gain or loss (indicate + or -): _____ acres</p>	
<p>o. Does project site contain any species of plant or animal that is listed by the federal government or NYS as endangered or threatened, or does it contain any areas identified as habitat for an endangered or threatened species? <input type="checkbox"/> Yes <input type="checkbox"/> No</p>	
<p>p. Does the project site contain any species of plant or animal that is listed by NYS as rare, or as a species of special concern? <input type="checkbox"/> Yes <input type="checkbox"/> No</p>	
<p>q. Is the project site or adjoining area currently used for hunting, trapping, fishing or shell fishing? <input type="checkbox"/> Yes <input type="checkbox"/> No If yes, give a brief description of how the proposed action may affect that use: _____ _____</p>	
E.3. Designated Public Resources On or Near Project Site	
<p>a. Is the project site, or any portion of it, located in a designated agricultural district certified pursuant to Agriculture and Markets Law, Article 25-AA, Section 303 and 304? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, provide county plus district name/number: _____</p>	
<p>b. Are agricultural lands consisting of highly productive soils present? <input type="checkbox"/> Yes <input type="checkbox"/> No <i>i.</i> If Yes: acreage(s) on project site? _____ <i>ii.</i> Source(s) of soil rating(s): _____</p>	
<p>c. Does the project site contain all or part of, or is it substantially contiguous to, a registered National Natural Landmark? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> Nature of the natural landmark: <input type="checkbox"/> Biological Community <input type="checkbox"/> Geological Feature <i>ii.</i> Provide brief description of landmark, including values behind designation and approximate size/extent: _____ _____ _____</p>	
<p>d. Is the project site located in or does it adjoin a state listed Critical Environmental Area? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> CEA name: _____ <i>ii.</i> Basis for designation: _____ <i>iii.</i> Designating agency and date: _____</p>	

e. Does the project site contain, or is it substantially contiguous to, a building, archaeological site, or district which is listed on, or has been nominated by the NYS Board of Historic Preservation for inclusion on, the State or National Register of Historic Places?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Nature of historic/archaeological resource: <input type="checkbox"/> Archaeological Site <input type="checkbox"/> Historic Building or District	
<i>ii.</i> Name: _____	
<i>iii.</i> Brief description of attributes on which listing is based: _____	
f. Is the project site, or any portion of it, located in or adjacent to an area designated as sensitive for archaeological sites on the NY State Historic Preservation Office (SHPO) archaeological site inventory?	<input type="checkbox"/> Yes <input type="checkbox"/> No
g. Have additional archaeological or historic site(s) or resources been identified on the project site?	
If Yes:	
<i>i.</i> Describe possible resource(s): _____	
<i>ii.</i> Basis for identification: _____	
h. Is the project site within five miles of any officially designated and publicly accessible federal, state, or local scenic or aesthetic resource?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Identify resource: _____	
<i>ii.</i> Nature of, or basis for, designation (e.g., established highway overlook, state or local park, state historic trail or scenic byway, etc.): _____	
<i>iii.</i> Distance between project and resource: _____ miles.	
i. Is the project site located within a designated river corridor under the Wild, Scenic and Recreational Rivers Program 6 NYCRR 666?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Identify the name of the river and its designation: _____	
<i>ii.</i> Is the activity consistent with development restrictions contained in 6NYCRR Part 666?	
<input type="checkbox"/> Yes <input type="checkbox"/> No	

F. Additional Information

Attach any additional information which may be needed to clarify your project.

If you have identified any adverse impacts which could be associated with your proposal, please describe those impacts plus any measures which you propose to avoid or minimize them.

G. Verification

I certify that the information provided is true to the best of my knowledge.

Applicant/Sponsor Name _____ Date _____

Signature _____ Title _____



Village of Port Chester Waterfront Commission

222 Grace Church Street
Port Chester, NY 10573
Phone 914-937-6780



LWRP COASTAL ASSESSMENT FORM

An applicant seeking an approval for a non-excluded action from any agency of the Village of Port Chester which is subject to the Village of Port Chester Local Waterfront Revitalization Program (LWRP) must complete this form. This form is intended to assist the Village Waterfront Commission in determining if the proposed action is consistent with the policies and purposes of the LWRP.

SITE IDENTIFICATION INFORMATION		
Application Name: Port North Main Street LLC	Application #:	Date Submitted: 03/03/2016
Site Address: No. 531-601 Street: North Main Street		
Property Location: <i>(Identify landmarks, distance from intersections, etc.)</i> Northeasterly side of North Main Street, parallel to the Byram River, south of the Williams James Pump Station		
Village of Port Chester Tax Map Designation: Section 136.064 Block 64.1 Lot(s) 33 and 61	Zoning Designation of Site: C4	
OWNER/APPLICANT INFORMATION		
Property Owner: Port North Main Street LLC	Phone #: 914-481-1880 Fax #: 914-206-4858	Email: ddt@tartagliaalawgroup.com
Owner Address: No. 95-25 Street: Queens Boulevard Town: Rego Park State: NY Zip: 11374		
Applicant (If different than owner):	Phone #: Fax #:	Email:
Applicant Address (If different than owner): No. Street: Town: State: Zip:		
PROJECT DESCRIPTION		
Describe the project, proposed use and operation thereof: Amend the Zoning Code to create a C4-R Gateway Retail District for the area along North Main Street from Terrace Avenue to the northern border of the District at or about Hillside Avenue and Riverside Avenue. Impose a parking restriction for retail uses within the C4-R District at one space per 300 square feet.		

CONSISTANCY WITH LWRP POLICIES

Actions to be undertaken within the Village's coastal area shall be evaluated for consistency in accordance with the following LWRP policy standards and conditions, which are derived from and further explained and described in Section III of the Village of Port Chester LWRP, a copy of which is on file in the Village Clerk's office and available for inspection during normal business hours. Agencies that undertake direct actions shall also consult with Section IV of the LWRP in making their consistency determination.

*YES indicates that the proposed project/action is consistent with the applicable policy,
NO indicates that the proposed project/action is inconsistent with the applicable policy,
NOT APPLICABLE indicates that it does not apply.*

Restore, revitalize and redevelop deteriorated and underutilized waterfront areas for commercial, industrial, cultural, recreational, public access and other compatible uses (Policies 1, 1A, 1B, 1C, 1D and 1E). Yes No
 Not Applicable

Facilitate the siting of water dependent uses on or near coastal waters (Policy 2). Yes No
 Not Applicable

Encourage the development and enhancement of those traditional uses and activities which have provided the Village a unique maritime identity (Policy 4). Yes No
 Not Applicable

Ensure that development occurs where adequate public infrastructure is available (Policy 5). Yes No
 Not Applicable

Streamline development permit procedures (Policy 6). Yes No
 Not Applicable

Protect fish and wildlife resources from hazardous wastes and other contaminating pollutants (Policy 8). Yes No
 Not Applicable

Maintain and expand the recreational use of existing fish and wildlife resources while ensuring protection of renewable fish and wildlife resources (Policy 9). Yes No
 Not Applicable

Maintain and expand commercial fishing resources to promote commercial fishing opportunities (Policy 10). Yes No
 Not Applicable

<p>Minimize flooding and erosion hazards through proper siting of buildings and structures; construction of carefully selected, long-term structural measures; and the use of appropriate nonstructural means (Policies 11, 13, 14, 15 and 17).</p>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p>Public funds shall be used for erosion protection structures only where necessary and in an appropriate manner (Policy 16).</p>	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p>Safeguard vital economic, social and environmental interests in the coastal area when major actions are undertaken (Policy 18).</p>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p>Protect, maintain, and improve public access to the shoreline and to water-related recreational resources, while protecting natural and historic resources and adjacent land uses and providing a continuous public walkway along the foreshore of the Byram River (Policies 19, 20 and 20A).</p>	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p>Encourage, facilitate and prioritize water-dependent and water-enhanced recreational resources and facilities near coastal waters, with priority given to those near access to existing public transportation and areas where the use of the shore is severely restricted by existing development (Policy 21).</p>	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p>Encourage the development of water-related recreational resources and facilities, as multiple uses, in appropriate locations within the shore zone (Policy 22).</p>	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p>Protect, restore and enhance historic resources (Policy 23).</p>	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p>Protect and enhance scenic and aesthetic resources (Policy 25).</p>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
<p>Site and construct energy facilities in a manner which will be compatible with the environment and contingent upon the need for a shorefront location and in such a manner as to avoid adverse environmental impacts when in operation (Policies 27 and 29).</p>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable

Protect coastal waters from direct and indirect discharge of pollutants (Policies 30, 31, 33, 34, 35, 36 and 37).	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
Ensure that dredging and dredge spoil disposal are undertaken in a manner protective of natural resources (Policies 15 and 35).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable
Ensure that any transportation, handling or disposal of hazardous wastes and effluent is undertaken in a manner which will not adversely affect the environment (Policy 39).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable
Protect air quality in accordance with state and federal air quality standards (Policies 41 and 42).	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Not Applicable
Preserve and protect tidal and freshwater wetlands (Policy 44).	<input type="checkbox"/> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Not Applicable

DETERMINATION OF CONSISTENCY

To be filled out by the Waterfront Commission

- This project/action is consistent with LWRP Policies and Conditions
- This project/action is NOT consistent with LWRP Policies and Conditions

The following changes to the project/action are recommended to ensure consistency with the LWRP:

Waterfront Commission Chairman

Date

Gregg Gregory, Chairman



Village of Port Chester Planning Commission

Anthony Baxter
Michael Brescio
Peter Coperine
Joseph Montesano
Christopher Summa
Michael Scarola
Constance Phillips, Secretary

Port North Main Street Zoning Petition

The Village of Port Chester's Planning Commission discussed Port North Main Street's zoning petition during the January 25, 2016 Planning Commission meeting. Below are the Commission's comments to the Board of Trustees regarding the proposed inclusion of retail and personal service uses by special exception, special exception criteria, amended dimensional regulations, and a retail parking requirement for the North Main C4 District.

- Consider traffic and circulation impacts of retail uses along this corridor. Retail uses generally generate trips on a fairly even basis throughout the day, in comparison to other land uses which generate a concentrated number of trips during particular times during the day.
- Retail should be context appropriate. Consider implementing special exception criteria to restrict the types of permitted retail based on desired character of the District.
- Utilize the waterfront and incorporate the Byram River, as stated in the Comprehensive Plan, where possible and practical. Also consider the sensitivities presented by the River and address environmental impacts to the waterfront wherever possible.
- Aesthetics, building orientations, and overall appearance should fit in with the "gateway" nature of the District.

Before making any decisions regarding the proposed zoning change, the Waterfront Commission must also provide comments to the Board of Trustees and evaluate the petition for waterfront consistency. Additional comments from Westchester County and the Town of Greenwich will be forthcoming once this petition moves to a public hearing.



VILLAGE OF
PORT CHESTER
222 Grace Church Street, Port Chester, New York 10573

AGENDA MEMO

Office of the Village Treasurer

Village BOT Meeting Date: 3/7/2016

Item Type: Resolution

Description	Yes	No	Description	Yes	No
Fiscal Impact			Public Hearing Required		x
Funding Source:			BID #		
			Strategic Plan Priority Area		
			N/A		
Agreement		x	Manager Priorities		
Strategic Plan Related		x	N/A		

Sponsor's Name: Leonie Douglas, Village Treasurer

Agenda Heading Title
(Will appear as indicated below on Agenda)

ADOPTION OF LOCAL ADJUSTMENTS FOR HOMESTEAD AND
NON-HOMESTEAD BASE PROPORTIONS FOR 2016-2017

Summary

Background:

This is the resolution to adopt the local adjustments for 2016-2017 Village taxes which establishes a Homestead and a Non-Homestead tax rate.

Homestead Base Proportions are of 54.889754%; and

Non-Homestead Base Proportions are of 45.110246%.

Proposed Action

That the Board of Trustees adopt the Resolution

Attachments

RESOLUTION

**ADOPTION OF LOCAL ADJUSTMENTS FOR HOMESTEAD AND
NON-HOMESTEAD BASE PROPORTIONS FOR 2016-2017**

On motion of TRUSTEE _____, seconded by TRUSTEE _____, the

following resolution was adopted by the Board of Trustees of the Village of Port Chester,

New York:

WHEREAS, on February 2, 2005, the Board of Trustees adopted Local Law No.2 of 2005 relating to the adoption of the provisions of Section 1903 of the Real Property Tax Law regarding Homestead Base Proportions; and

WHEREAS, the Town of Rye Assessor has prepared and adopted Homestead and Non-Homestead base proportions for the Village of Port Chester on February 16, 2016; and

WHEREAS, the Board of Trustees of the Village of Port Chester wishes to adopt local adjustments to these base proportions that will be used to establish a Homestead tax rate and a Non-Homestead tax rate for the 2016-2017 Village Budget and tax rates. Now, therefore, be it

RESOLVED, that the Board of Trustees of the Village of Port Chester hereby adopts for the 2016-2017 Village tax, Homestead Base Proportions of 54.889754%, and Non-Homestead base proportions of 45.110246%; and be it further

RESOLVED, that the Village Manager is authorized to sign all necessary documents to implement the purposes of this resolution; and be it further

RESOLVED, that the Town Assessor and Receiver of Taxes are authorized to make any filings on behalf of the Village of Port Chester, and to take any additional actions necessary to implement the purposes of this resolution.

APPROVED AS TO FORM:

Anthony M. Cerreto, Village Attorney

RESOLUTION

Resolution of the Town Board of the Town of Rye Establishing a Homestead Base Proportion for the Village of Port Chester and to Authorize the Town Assessor to File a Statement of Locally Determined Homestead Base Proportions Pursuant to and in Accordance with the Provisions of Section 1903 of the Real Property Tax Law Commonly Referred to as the "Homestead Tax Option" based on the 2015 Assessment Roll (completed, verified and filed in 2015) for the 2016 Village budget and tax rates.

On motion of Councilperson Jackson, seconded by Councilperson Villanova, the following Resolution was adopted:

WHEREAS, in 1981, the State of New York enacted Section 1903 of the Real Property Tax Law (RPTL) known as the "Homestead Tax Option"; and

WHEREAS, the Homestead Tax Option was enacted to prevent a disproportionate shift of property tax burden to residential property owners following a revaluation of real property; and

WHEREAS, the Town Board adopted a Local Law on March 23, 2004 entitled "A Local Law Adopting the Provisions of Section 1903 of the Real Property Tax Law" commonly referred to as the "Homestead Tax Option" (the "Local Law"); and

WHEREAS, pursuant to the Local Law, the Town Board desires to establish two separate property tax rates for the Village of Port Chester; a rate for residential property owners equal to a rate of 54.889754 (the "Homestead Base Proportion") and a rate for all other property owners equal to a rate of 45.110246 (the "Non-Homestead Base Proportion") and to authorize the Town Assessor to file a Statement of Locally Determined Homestead Base Proportions Pursuant to Section 1903 of the RPTL based on the 2015 Assessment Roll (completed, verified and filed in 2015) for the 2016 Village budget and tax rates; and

NOW, THEREFORE, BE IT RESOLVED BY THE TOWN BOARD AS FOLLOWS:

1. The Town Board hereby: (a) establishes two separate property tax rates for the Village of Port Chester pursuant to the provisions of Section 1903 of the RPTL **(I) the Homestead Base Proportion at a rate equal to 54.889754 and (II) the Non-Homestead Base Proportion at a rate equal to 45.110246** and (b) authorizes the Town Assessor to file a Statement of Locally Determined Homestead Base Proportions Pursuant to Section 1903 of the RPTL based on the 2015 Assessment Roll (completed, verified and filed in 2015) for the 2016 Village budget and tax rates.

ROLL CALL

AYES: Councilpersons Jackson and Villanova and Supervisor Zuckerman

NOES: None


ABSENT: Councilpersons Nardi and Baxter

Dated: February 16, 2016

STATE OF NEW YORK)
COUNTY OF WESTCHESTER) ss:
TOWN OF RYE)

I, Hope B. Vespia, Town Clerk of the Town of Rye, New York, do hereby certify that I have compared the foregoing copy with a copy of the Resolution adopted by the Town Board of the Town of Rye, at a meeting held on February 16, 2016 at which time a quorum was present, and that the same is a true and correct transcript of the whole thereof.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the corporate seal of the Town of Rye, New York, on this 17th day February, 2016.


Hope B. Vespia
Town Clerk



VILLAGE OF
PORT CHESTER
222 Grace Church Street, Port Chester, New York 10573

AGENDA MEMO

Department: Justice Court

BOT Meeting Date: 3/7/2016

Item Type: Resolution

Sponsor's Name: Regina Hill, Court Clerk

Description	Yes	No	Description	Yes	No
Fiscal Impact	X	<input type="checkbox"/>	Public Hearing Required	<input type="checkbox"/>	x
Funding Source: JCAP NYS Grant Account #: 1.1.3389			BID #		
			Strategic Plan Priority Area		
			N/A		
Agreement	<input type="checkbox"/>	x	Manager Priorities		
Strategic Plan Related	<input type="checkbox"/>	x	N/A		

Agenda Heading Title
(Will appear on the Agenda as indicated below)

JCAP Grant Resolution

Summary

Background:

Acceptance of the JCAP Grant. The Justice court was awarded \$6,855.99. The breakdown for the funds are \$5,275.99 for Security Cameras in the Criminal, Traffic and Parking offices. \$280 for a Cash Counter for Parking. \$400 for a Safe and \$900 for court enhancements.

Proposed Action

That the Board of Trustees adopt the Resolution

[AM-V20140930](#)

Attachments

Background on the Materials to be purchased with the Grant

STATE OF NEW YORK
UNIFIED COURT SYSTEM
Westchester County Courthouse
111 Dr. Martin Luther King Jr. Blvd.
White Plains, NY 10601
(914) 824-5100

REC'D DEC 29 2015
(1062)

Lawrence K. Marks
Chief Administrative Judge

Alan D. Scheinkman
Ninth District Administrative Judge

Michael V. Coccoma
Deputy Chief Administrative Judge
Courts Outside New York City

December 23, 2015

Mrs. Regina Hill
Court Clerk
Port Chester Village Court
350 North Main Street
Port Chester, NY 10573

Dear Mrs. Hill:

We are pleased to advise you the Port Chester Village Court has been awarded a grant under the 2015-2016 cycle of the Justice Court Assistance Program (JCAP).

JCAP was established by the New York State Legislature in 1999, at the request of the Unified Court System, to help provide our State's town and village courts with the resources and equipment necessary to fulfill their critical role in our justice system. Under the Court System's Action Plan for the Justice Courts, JCAP has been expanded, both in the level of funding and the scope of the projects funded.

The Port Chester Village Court is one of 353 courts receiving JCAP funding this year. Details regarding your award are set forth on the enclosed reconciliation form. Funds will be sent to your municipality via direct deposit or check and must be spent within 180 days. The reconciliation form must be returned to the Office of Justice Court Support via fax at: (518) 438-3518 or e-mail at: jcap@nycourts.gov.

Congratulations and thank you for your participation in the Justice Court Assistance Program.

Alan D. Scheinkman
Ninth District Administrative Judge

Charles Apotheker
Supervising Judge

cc: Hon. Randall Eng, Presiding Justice Appellate Division
Hon. Michael V. Coccoma, Deputy Chief Administrative Judge
Courts Outside New York City

2015-2016 Justice Court Assistance Program Award Reconciliation Form

Please fax this form to 518-438-3518 or mail to:
Office of Justice Court Support, 187 Wolf Road, Suite 103, Albany, N.Y. 12205

Application ID # 3372

In the space provided below, indicate when the money was spent on the item described below and the exact amount spent.

Funds to be spent within 180 days of receipt

Information about when you spent the grant;

Port Chester Village Court, Westchester County

District: 9

Item Category	Grant Amount Approved	Print Name: _____
Courtroom Enhancements	\$900.00	Signature: _____
<input type="checkbox"/> I affirm this was received	Date: _____	Amount actually spent: _____

Item Category	Grant Amount Approved	Print Name: _____
Office Equipment	\$400.00	Signature: _____
<input type="checkbox"/> I affirm this was received	Date: _____	Amount actually spent: _____

Item Category	Grant Amount Approved	Print Name: _____
Other	\$280.00	Signature: _____
<input type="checkbox"/> I affirm this was received	Date: _____	Amount actually spent: _____

Item Category	Grant Amount Approved	Print Name: _____
Security	\$5,275.99	Signature: _____
<input type="checkbox"/> I affirm this was received	Date: _____	Amount actually spent: _____

Total Amount of Grant	Port Chester Village Court \$6,855.99
------------------------------	--

SPECIAL NOTE REGARDING AWARD DISBURSEMENTS:

Your Town Supervisor or Village Mayor should receive a check for the amount of the grant or the grant amount will be sent via direct deposit to your municipality. All grant recipients are reminded that, as required by law, funds received hereunder may not be used for purposes other than the purchase of the item(s) set forth on the enclosed award form. Also, as stipulated in the municipal certification accompanying the application for your grant, "any goods and/or services purchased with any Justice Court Assistance Program funds shall be obtained in accordance with acceptable procurement practices established by the governing municipality including, but not limited to, competitive bidding and procurement policies and procedures."

Please remember to save your receipts for at least three (3) years for audit and review purposes. If the amount you spend purchasing an item is less than the amount awarded in the grant, and that difference is less than 10% of the award for that item, then you may use that savings toward another grant item or towards consumable office supplies. If the savings exceeds 10% of the amount awarded for that item, please contact the Office of Justice Court Support at (800)-232-0630 for approval.

FOR OJCS USE ONLY:	Application # _____	Attachments _____
Vendor ID# 1000001510	AO Date _____	Approval Date _____
Voucher# _____	DN/SP _____	Grant Amt _____
Submit Date _____	Business Unit _____	Final Approval _____

Thomas, David

From: Cerreto, Tony
Sent: Thursday, March 3, 2016 9:19 AM
To: Thomas, David
Cc: Ameigh, Chris
Subject: FW: JCAP Award

Back up to agenda memo

From: Hill, Regina (Court Clerk) [mailto:rhill@vpcpd.com]
Sent: Thursday, March 3, 2016 9:19 AM
To: Cerreto, Tony <TonyC@PortChesterNY.com>
Subject: JCAP Award

The Justice court was awarded \$6,855.99. The breakdown for the funds are \$5,275.99 for Security Cameras in the Criminal, Traffic and Parking offices. \$280 for a Cash Counter for Parking. \$400 for a Safe and \$900 for office supplies.

Regina Hill, Court Clerk
Port Chester Justice Court
350 North Main Street
Port Chester, NY 10573
Office: 914-939-8220, Cell: 914-417-0259
Fax: 914-939-3479, Email: rhill@vpcpd.com

IMPORTANT WARNING: This message is intended for the use of the person or entity to which it is addressed and may contain information that is privileged and confidential, the disclosure of which is governed by applicable law. If the reader of this message is not the intended recipient, or the employee or agent responsible to deliver it to the intended recipient, you are hereby notified that any dissemination, distribution or copying of this information is STRICTLY PROHIBITED. If you have received this message in error, please notify the sender immediately and arrange for the return or destruction of these documents.

RESOLUTION
ACCEPTANCE OF JUSTICE COURT ASSISTANCE PROGRAM GRANT
TO PURCHASE EQUIPMENT
FOR THE JUSTICE COURT

On motion of TRUSTEE _____, seconded by TRUSTEE _____, the following resolution was adopted by the Board of Trustees of the Village of Port Chester, New York:

WHEREAS, the Board of Trustees authorized the submission of an application for a New York State Office of Court Administration 2016 Justice Court Assistance Program (JCAP) grant to facilitate necessary improvements to the Port Chester Justice Court; and

WHEREAS, the Village has been awarded a \$6,855.99 grant; and

WHEREAS, the Court Clerk seeks to purchase the equipment sought through the JCAP grant so as to enhance security and operations. Now, therefore, be it

RESOLVED, that the Board of Trustees hereby accepts the 2016 JCAP grant and authorizes the Village Treasurer to modify the General Fund Budget as follows:

General Fund

Increase:

1.10389	NYS Other Public Safety Items	\$6,855.99
1.1110.200	Equipment	\$6,855.99

Approved as to Form:

Anthony M. Cerreto, Village Attorney



VILLAGE OF
PORT CHESTER

222 Grace Church Street, Port Chester, New York 10573

RES-05
 BOT 3-7 -2016

AGENDA MEMO

Office of the Village Manager

Village BOT Meeting Date: 3/7/2016

Item Type: Resolution

Description	Yes	No	Description	Yes	No
Fiscal Impact	x		Public Hearing Required		x
Funding Source:			BID # 2015-14		
Account #:5.3120.2013			Strategic Plan Priority Area		
			N/A		
Agreement	x		Manager Priorities		
Strategic Plan Related		x	N/A		

Sponsor's Name: Christopher D. Steers, Village Manager

Agenda Heading Title
(Will appear as indicated below on Agenda)

AWARDING BID FOR MASONRY REPAIRS TO POLICE HEADQUARTERS/JUSTICE COURT .

Summary

Background:

The Police Headquarters/Justice Court at 350 North Main Street requires masonry work to be done

Dolph Rotfeld Engineering, the Village's consulting engineer, prepared the necessary bid specifications for letting this work out for competitive bidding as required by State Law.

The resolution provides for the award to be made to the low (and single) bidder, but authorizes the Village Manager to enter into post-bid negotiations with the bidder.

Mr. Rotfeld's office will be attending the meeting to answer any questions..

Proposed Action

That the Board of Trustees adopt the Resolution

Attachments

Award memo from Dolph Rotfeld Engineering, P.C.

Dolph Rotfeld Engineering, P.C.

CONSULTANTS & DESIGNERS

200 White Plains Road, Tarrytown, NY 10591 • (914) 631-8600

March 3, 2016

Mr. Christopher Steers
Village Manager
222 Grace Church Street
Port Chester, N.Y. 10573

RE: 350 North Main Street Masonry Repairs
Bid No. 2015-14
Port Chester, New York

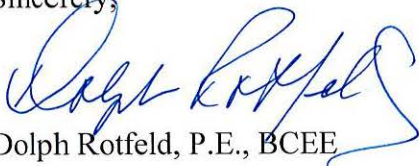
Dear Mr. Steers:

On November 23, 2015, one bid was received for the above referenced project. The bidder was RVT Construction of Paterson, NJ with a bid price of \$89,000.00.

This office has checked the references provided by RVT Construction including properties in Little Falls, NJ and the E.T. Hamilton School in Voorhees, NJ where work similar in scope to this project was completed. The references provided by RVT Construction expressed satisfaction in the Contractor's work and confidence in their ability to produce the same quality of workmanship for this project.

This office hereby recommends immediate award of the above referenced contract to RVT Construction so that the work can commence as soon as possible.

Sincerely,



Dolph Rotfeld, P.E., BCEE

C: A. Cerreto, Village Attorney
L. Douglas, Village Treasurer
D. Thomas, Village Clerk

AWARDING BID FOR MASONRY REPAIRS TO POLICE HEADQUARTERS/JUSTICE COURT

On motion of TRUSTEE _____, seconded by TRUSTEE _____, the following resolution was adopted by the Board of Trustees of the Village of Port Chester, New York:

WHEREAS, the Village of Port Chester has advertised that it was seeking bids for masonry work at the Police Headquarters/Justice Court facility at 350 North Main Street (Bid # 2015-14-); and

WHEREAS, the Village received a single bid, from RVT Construction, 85 5th Avenue, Unit 3, Patterson, New Jersey 07524 which submitted a bid of \$89,000.00; and

WHEREAS, the Village's consulting engineer, Dolph Rotfeld Engineering, P.C . recommends that this bid be accepted which is in compliance with the bid specifications. Now therefore, be it

RESOLVED, that the Board of Trustees hereby awards the bid for masonry repairs to the Police Headquarters/Justice Court (Bid #15-14) to RVY Construction, and be it further

RESOLVED, that the Village Manager is hereby authorized to enter into post-bid negotiations and enter into an agreement with the contractor provided that the amount shall not exceed \$80,000; and be it further

RESOLVED, that the funding for said work be appropriated from the Police Building Improvement Project a/c 5.3120.2013.126 in the Capital Fund.

Approved as to Form:

Anthony M. Cerreto, Village Attorney



VILLAGE OF
PORT CHESTER

222 Grace Church Street, Port Chester, New York 10573

AGENDA MEMO

Office of the Village Manager

Village BOT Meeting Date: 3/7/2016

Item Type: Resolution

Description	Yes	No	Description	Yes	No
Fiscal Impact	x		Public Hearing Required		x
Funding Source:			BID # 2016-01 Strategic Plan Priority Area N/A		
Agreement	X		Manager Priorities		
Strategic Plan Related		x	N/A		

Sponsor's Name: Christopher D. Steers, Village Manager

Agenda Heading Title
(Will appear as indicated below on Agenda)

AWARDING BID FOR DEMOLITION
OF UNSAFE STRUCTURE AT 1 BETSY BROWN ROAD

Summary

Background:

The Village of Port Chester commenced a special proceeding in Westchester County Supreme Court to abate the public nuisance created by the fire damaged structure at 1 Betsy Brown Road, by demolishing the unsafe structure.

The Village received a Decision (on 12/23/2015) and a Court Order (on 2/19/2016) authorizing it to demolish the unsafe structure by retaining a private contractor, with all expenses to constitute a lien and if unpaid to be collected and enforced in the same manner as Village taxes.

After the Village received the Court's decision but before it received the Order the Village went out to bid for this project so the contractor would be able to proceed as soon as possible.

Of the seven bids submitted for this project, State Contracting Corp. of NY d/b/a Capital Industries was the lowest bidder at \$28,000.

References were checked and the bid is in full compliance with bid specifications.

Proposed Action

That the Board of Trustees adopt the Resolution

Attachments

Resolution Bid Package #2016-01 Bid

AWARDING BID FOR DEMOLITION
OF UNSAFE STRUCTURE AT 1 BETSY BROWN ROAD

On motion of TRUSTEE _____, seconded by TRUSTEE _____,
the following resolution was adopted by the Board of Trustees of the Village of Port Chester, New
York:

WHEREAS, the Village of Port Chester commenced a special proceeding in Westchester
County Supreme Court to abate the public nuisance created by the fire-damaged structure at 1
Betsy Brown Road; and

WHEREAS, the Village received a decision on December 23, 2015 granting the Village's
petition authorizing the Village to effectuate the demolition through a private contractor and for
such expenses to constitute a lien on the property to be collected and enforced in the same manner
as village taxes and requesting an order to be prepared for the court' signature confirming same;
and

WHEREAS, in anticipation of the signed order, the Village advertised that it was seeking
bids for the demolition of 1 Betsy Brown Road (Bid # 2016-01); and

WHEREAS, the Village received seven bids for this work; and

WHEREAS, State Contracting Corp of NY d/b/a Capital Industries submitted the lowest
bid of \$28,000.00; and.

WHEREAS, on February 19, 2016 the Village received the signed order; and

WHEREAS, Village staff recommends that this bid be accepted which is in compliance
with the bid specifications. Now therefore, be it

RESOLVED, that the Board of Trustees hereby awards Bid # 2016-01 Demolition of 1
Betsy Brown Road to State Contracting Corp of NY d/b/a Capital Industries, 555 Saw Mill
River Road, Yonkers New York, 10701, and be it further

RESOLVED, that the Village Manager is hereby authorized to enter into an agreement
with the contractor; and be it further

RESOLVED, that the Board of Trustees authorizes the Village Treasurer to modify the
General Fund as following:

General Fund:

Transfers from:

1.1990.400	Contingency	\$28,000
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Transfers to:

1.3650.400	Demolition of Unsafe Buildings	\$28,000
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Approved as to Form:

Anthony M. Cerreto, Village Attorney

**VILLAGE OF PORT CHESTER
DEMOLITION 1 BETSY BROWN ROAD BID NO. 2016-1
BID PROPOSAL FORM**

To: **Village Manager - Village Hall
Port Chester, New York**

Bid Submitted by:

State Contracting Corp of NY
(Name)

555 Saw Mill River Rd. Yonkers, NY 10701
(Address)

914-963-2000
(Telephone No.)

1. I/We do hereby declare that I/We have carefully examined the Notice to Bidders, the Details, and the Specifications relating to the above entitled matter and the work.
2. I/We do hereby offer and agree to furnish all materials, to fully and faithfully construct, perform and execute all work in the above titled matter in accordance with the Details and Specifications relating thereto, and to furnish all labor, tools, implements, models, forms, transportations and materials necessary and proper for the purpose and the price/prices as given on the bid forms.
3. I/We do hereby declare that the prices so stated cover all expenses of every kind incidental to the completion of said work, and the contract therefor, including all claims that may arise through damages or any other cause whatsoever.
4. I/We do hereby agree that I/We will execute a contract therefore, containing all the terms, conditions, provisions and covenants necessary to complete the work according to the Details and Specifications therefore within 10 business days after the award of the contract and if I/We fail to execute said contract within said period of time, that the Village Board shall have the power to rescind said award and also that the said Village Board shall retain the proceeds of the certified check, or require the payment of the sum of the bid bond. The Contract execution will serve as the official notification to commence work.
5. I/We do also declare and agree I/We will be prepared to respond to any emergency work within five days after the Contract execution and will complete the work fully and in every respect on or before the time specified in said Contract and do authorize the said Board, in case of failure to complete the work within such specified time, to employ such men, equipment and materials as may be necessary for the proper completion of said work and to deduct the cost thereof from the amount due under the

Contract. I/We agree that the Village reserves the right to select any one, combination of, or all the Bid items in this proposal for the Contractor to complete without affecting any of the Bid prices.

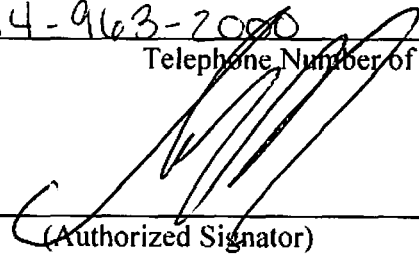
6. I/We hereby affirm that by submission of this Bid, each Bidder and each person signing on behalf of any Bidder certifies, and in the case of a joint bid each party thereto certifies, as to its own organization, under the penalty of perjury, that to the best of knowledge and belief:
 - (a) the prices in this Bid have been arrived at independently without collusion, consultation, communication, or agreement for the purpose of restricting competition, as to any matter relating to such prices with any other Bidder or with any competitor;
 - (b) unless otherwise required by law, the prices which have been quoted in this Bid have not been knowingly disclosed by the Bidder and will not knowingly be disclosed by the Bidder prior to opening, directly or indirectly, to any other Bidder or to any competitor; and
 - (c) no attempt has been made or will be made-by the Bidder to induce any other person, partnership or corporation to submit or not submit a Bid for the purpose of restricting competition.
 - (d) no member of the Village Board or and officer or employee of the Village of Port Chester, New York, or person whose salary is payable in whole or in part from the said Treasury is, shall be or become interested, directly, as contracting party, partner, stockholder, surety or otherwise, in this Bid, or in the performance of the Contract, or in the supplies, materials or equipment and work or labor to which it relates, or in any portion of the profits thereof.
8. I/We hereby further agree that this proposal is a firm Bid and shall remain in effect for a period of at least forty five (45) calendar days from the date of the opening of Bids, and that with said period of forty five (45) days, the Village of Port Chester will accept or reject this proposal, or this period may be extended by mutual agreement.
9. I/We do hereby declare that, if this is a Corporate Bid, I have been duly authorized to act as the Signator on this proposal in behalf of this Corporation.
10. I/We hereby affirm under penalty of perjury, the truth of all statements in this proposal.

BID
\$28,000.00
(written in numbers)
Twenty-Eight Thousand dollars
(written in words)

State Contracting Corp of NY DBA Capital Industries Date: 1/26/2016
(Legal Name of Bidder)

555 Saw Mill River Rd, Yonkers, NY 10701
Address of Individual, Firm or Corporation

914-963-2000
Telephone Number of Individual, Firm or Corporation

By: 
(Authorized Signator)

Corporate Seal
(if incorporated)

VILLAGE OF PORT CHESTER

BID ANALYSIS SHEET

OBID #2016-01

BID FOR:	DEMOLITION OF UNSAFE STRUCTURE AT 1 BETSY BROWN ROAD		
BID OPENING DATE:	JANUARY 27, 2016	TIME:	10:00 A.M.
BID PUBLICATION DATE:	JANUARY 15, 2016		
SPECIFICATIONS AVAILABLE:	JANUARY 19, 2016		

Please Print Name and Address

BIDDER:	<i>ConTech Construction Technology, Inc</i>		
ADDRESS:	<i>28 Lakeview Drive</i>		
ADDRESS:			
CITY:	<i>Yorktown Heights</i>	STATE:	<i>NY</i> ZIP CODE: <i>10598</i>
E-MAIL:	<i>Contech03@gmail.com</i>		
PHONE #:	<i>914-455-3100</i>	FAX #:	<i>914-962-4500</i>
AMOUNT:	<i>\$38,525.00</i>		

Please Print Name and Address

BIDDER:	<i>Force Construction</i>		
ADDRESS:	<i>P.O. 4117</i>		
ADDRESS:			
CITY:	<i>New Windsor</i>	STATE:	<i>NY</i> ZIP CODE: <i>12553</i>
E-MAIL:	<i>seanforce@hotmail.com</i>		
PHONE #:	<i>Cell: 914-474-5334</i>	FAX #:	
AMOUNT:	<i>\$39,895.00</i>		

Please Print Name and Address

BIDDER:	<i>State Contracting Group of New York DBA Capital Industries</i>		
ADDRESS:	<i>555 Saw Mill River Road</i>		
ADDRESS:			
CITY:	<i>Yonkers</i>	STATE:	<i>NY</i> ZIP CODE: <i>10701</i>
E-MAIL:			
PHONE #:	<i>(914) 963-2000</i>	FAX #:	
AMOUNT:	<i>\$28,000.00</i>		

VILLAGE OF PORT CHESTER

BID ANALYSIS SHEET

Please Print Name and Address	
BIDDER: Legacy Supply LLC	
ADDRESS: 14 Railroad Avenue	
ADDRESS:	
CITY: Valhalla	STATE: NY ZIP CODE: 10595
E-MAIL: lisa@legacysupplyllc.com	
PHONE #: 914-262-2048	FAX #: 914-946-2355
AMOUNT: \$110,000.00	

Please Print Name and Address	
BIDDER: Foremost Developers, LLC	
ADDRESS: 82 Seven Oaks Lane	
ADDRESS:	
CITY: Brewster	STATE: NY ZIP CODE: 10509
E-MAIL:	
PHONE #:	FAX #:
AMOUNT: \$96,000.00	

Please Print Name and Address	
BIDDER: RVT Construction	
ADDRESS: 85 5 th Avenue	
ADDRESS: Unit 3	
CITY: Paterson	STATE: NJ ZIP CODE: 07524
E-MAIL: RISTO@RVTCONSTRUCTION.COM	
PHONE #: 973-777-9856	FAX #: 973-777-9857
AMOUNT: \$60,000.00	

VILLAGE OF PORT CHESTER

BID ANALYSIS SHEET

Please Print Name and Address	
BIDDER: <i>Gentile Construction Corp</i>	
ADDRESS: <i>451 Old Nepperhan Avenue</i>	
ADDRESS:	
CITY: <i>Yonkers</i>	STATE: <i>NY</i> ZIP CODE: <i>10703</i>
E-MAIL:	
PHONE #:	FAX #:
AMOUNT: <i>\$28,900.00</i>	

The following were present at the opening of the bids (☑):

Village Clerk:	<input checked="" type="checkbox"/>	David Thomas
Deputy Village Clerk:	<input type="checkbox"/>	Vita Sileo
Village Attorney	<input type="checkbox"/>	Anthony (Tony) Cerreto
Department Head:	<input type="checkbox"/>	
Village Engineer:	<input type="checkbox"/>	Dolph Rotfeld
Project Engineer:	<input type="checkbox"/>	Dan Peluso
Other:	<input type="checkbox"/>	
Other:	<input type="checkbox"/>	
Other:	<input type="checkbox"/>	



VILLAGE OF
PORT CHESTER

222 Grace Church Street, Port Chester, New York 10573

AGENDA MEMO

Department: Office of the Village Manager
Department: Planning and Development Department

BOT Meeting Date: 3/7/2016

Item Type: Resolution

Sponsor's Name:	Christopher Ameigh, Assistant to the Village Manager
Sponsor's Name:	Eric Zamft, Director of Planning & Economic Development

Description	Yes	No	Description	Yes	No
Fiscal Impact		<input checked="" type="checkbox"/>	Public Hearing Required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Funding Source:			BID #		
Account #:			Strategic Plan Priority Area		
			Community Development		
	Yes	No			
Agreement	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Manager Priorities		
Strategic Plan Related	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Planning & Zoning		

Agenda Heading Title
(Will appear as indicated below on Agenda)

Designation of officers and creation of Capital Projects for 2015-16 CDBG awards

Summary

Earlier this year, the Village was awarded two Community Development Block Grants (CDBG) from New York State:

- \$106,745 to replace kitchen appliances and the bus that services the Village's Senior Community Center, which are at the end of their useful life. New appliances, equipment and bus services will allow the village to continue providing meals and transportation to approximately 1,300 seniors.

- \$750,000 to inspect and repair deteriorated sewer mains throughout the village with Cured-In-Place piping. This project is part of a multi-phase sewer rehabilitation program in the Village.

As part of receipt of such funding a number of forms must be submitted, including Form 8-1 Project Team, which identifies the officers that will administer the CDBG grants on the local level. There are currently three roles listed on Form 8-1 that need to be identified, and, per the recommendation of New York State, should be identified by resolution. The three roles are Fair Housing Officer, Section 3 Coordinator, and Labor Compliance Officer.

Also, as part of receipt of such funding bank accounts must be created and designated for direct deposit of CDBG funds. In order to create such accounts the Village Treasurer needs authorization to create Capital Project lines for the two proposed projects. Note that the Senior Center Project requires two Capital Project lines as Vehicles and Equipment are considered separate Capital Expense types in accounting guidelines. Upon creation of the Capital Project lines, bank accounts will be set up for said purpose.

Therefore, in order for the Village to proceed to utilize and receive CDBG funding, two resolutions have been prepared that appoint individuals from the Village in these roles for the purposes of the CDBG grants administration and that amends the 2015-16 Capital Project Plan to allow for receipt of award funds.

Attachments
Resolution amending the capital plan for and appointing officers to administer the CDBG Senior Center project
Resolution amending the capital plan for appointing officers to administer the CDBG Sewer Main project

RESOLUTION

AMENDING THE 2015-2016 CAPITAL PROJECT PLAN TO ESTABLISH TWO (2) PROJECTS FOR THE COMMUNITY DEVELOPMENT BLOCK GRANT (CDBG) SENIOR CENTER VEHICLES AND EQUIPMENT AND APPOINTING OFFICERS RESPONSIBLE FOR ADMINISTRATION THERETO

On the motion of TRUSTEE _____, seconded by TRUSTEE _____, the following resolution was adopted by the Board of Trustees of the Village of Port Chester, New York:

WHEREAS, the Village of Port Chester was awarded \$106,745 in Community Development Block Grant (“CDBG”) funding from the New York State Office of Community Renewal (“NYSOCR”) for replacement Vehicles and Kitchen Equipment; and

WHEREAS, as part of receipt of such funding a number of forms must be submitted; and

WHEREAS, Form 8-1 Project Team, and form I-2 Designation of Depository for Direct Deposit of NYS CDBG Funds are required forms; and

WHEREAS, NYSOCR has recommended that project team members be identified on such form and by local resolution, where necessary; and

WHEREAS, the only roles on Form 8-1 that are necessary to appoint are Fair Housing Officer, Section 3 Coordinator, and Labor Compliance Officer; and

WHEREAS, the following amendment to the 2015-2016 Capital Project Plan is necessary to properly account for the additional project

NOW, THEREFORE, be it

RESOLVED, that Christopher Steers, occupying the role as Village Manger is hereby appointed as the Village of Port Chester Fair Housing Officer for the CDBG Senior Center Project; and be it further

RESOLVED, that Christopher Steers, occupying the role as Village Manger is hereby appointed as the Village of Port Chester Section 3 Officer for the CDBG Senior Center Project; and be it further

RESOLVED, that Village Engineer is hereby appointed as the Village of Port Chester Labor Standards Compliance Officer for the CDBG Senior Center Project; and be it further

RESOLVED, that Village of Port Chester affirms the roles and responsibilities of the remaining officers designated on Form 8-1; and be it further

RESOLVED, that the 2015-2016 Capital Fund Budget be modified as follows:

1. SENIOR CENTER VEHICLES:

Revenues

5.5.4985.2015.0166	CDBG CFDA 14.228	\$93,438
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Appropriations

5.6750.203.2015.0166	Senior Center Vehicles	\$93,438
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2. SENIOR CENTER EQUIPMENT:

Revenues

5.5.4985.2015.0167	CDBG CFDA 14.228	\$13,307
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Appropriations

5.6730.200.2015.0167	Senior Center Equipment	\$13,307
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Approved as to Form:

Village Attorney
Anthony Cerreto

ROLL CALL

AYES:

NOES:

ABSENT:

DATE: March 7, 2016

RESOLUTION

**AMENDING THE 2015-2016 CAPITAL PROJECT PLAN TO
ESTABLISH A PROJECT FOR THE COMMUNITY DEVELOPMENT
BLOCK GRANT (CDBG) SEWER MAIN IMPROVEMENT PROJECT
AND APPOINTING OFFICERS RESPONSIBLE FOR
ADMINISTRATION THERETO**

On the motion of TRUSTEE _____, seconded by TRUSTEE _____,
the following resolution was adopted by the Board of Trustees of the Village of Port Chester,
New York:

WHEREAS, the Village of Port Chester was awarded \$750,000 in Community Development Block Grant (“CDBG”) funding from the New York State Office of Community Renewal (“NYSOCR”) for repairs to the Village of Port Chester’s sewer mains; and

WHEREAS, as part of receipt of such funding a number of forms must be submitted; and

WHEREAS, Form 8-1 Project Team, and form I-2 Designation of Depository for Direct Deposit of NYS CDBG Funds are required forms; and

WHEREAS, NYSOCR has recommended that project team members be identified on such form and by local resolution, where necessary; and

WHEREAS, the only roles on Form 8-1 that are necessary to appoint are Fair Housing Officer, Section 3 Coordinator, and Labor Compliance Officer; and

WHEREAS, the following amendment to the 2015-2016 Capital Project Plan is necessary to properly account for the additional project

NOW, THEREFORE, be it

RESOLVED, that Christopher Steers, occupying the role as Village Manger is hereby appointed as the Village of Port Chester Fair Housing Officer for the CDBG Senior Center Project; and be it further

RESOLVED, that Christopher Steers, occupying the role as Village Manger is hereby appointed as the Village of Port Chester Section 3 Officer for the CDBG Senior Center Project; and be it further

RESOLVED, that Village Engineer is hereby appointed as the Village of Port Chester Labor Standards Compliance Officer for the CDBG Senior Center Project; and be it further

RESOLVED, that Village of Port Chester affirms the roles and responsibilities of the remaining officers designated on Form 8-1; and be it further

RESOLVED, that the 2015-2016 Capital Fund Budget be modified as follows:

1. SEWER MAIN IMPROVEMENT

Revenue

5.5.4985.2015.0168	CDBG CFDA 14.228	\$750,000
--------------------	------------------	-----------

Appropriations

5.8120.400.2015.0168	Sewer Main Improvement	\$750,000
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Approved as to Form:

Village Attorney
Anthony Cerreto

ROLL CALL

AYES:

NOES:

ABSENT:

DATE: March 7, 2016



VILLAGE OF PORT CHESTER
DEPARTMENT OF PLANNING & ECONOMIC
DEVELOPMENT

222 Grace Church Street, Rm. 202
Port Chester, NY 10573
(P) 914.937.6780
(F) 914.939-2733

Eric Zamft, AICP, Director
Andrea Sherman, Assistant Planner
Constance Phillips, Planning Commission Secretary

MEMORANDUM

From: Department of Planning & Economic Development
To: Village Manager
Date: March 3, 2016
RE: Proposed Legislation Modifying the Westchester County Airport Terminal Use Regulations

The following are bullet points highlighting the current status of Westchester County's proposed legislation to modify the Airport Terminal Use Regulations ("TUR") and some of the issues that the Village may want to consider.

Existing Conditions

- For almost 40 years, the Airport operated under a series of capacity constraints designed primarily to limit aircraft noise in the communities surrounding the Airport. Between 1968 and 1984, air carriers were limited to 10 daily flight departures with no limitations on activity for commuter aircraft. From 1985 onward, a contractual agreement between the airlines and Westchester County (referred to as "the Stipulation Agreement") superseded the previous capacity constraints and limited passengers for both air carrier and commuter activity to 240 passengers per half hour. In 2005, the TUR was implemented, establishing similar capacity restrictions that had been in place since 1985.
- Such restrictions include 1) a maximum of 240 passengers boarding and deplaning per half hour and 2) a maximum of four aircraft may be scheduled to use the Terminal Ramp at any time.
- In order to allocate aircraft in the four "slots" a lottery system is in place whereby airlines request particular slots. The Commissioner of Public Works and Transportation has the final call on that determination.
- The Airport has a voluntary restriction on flights from approximately 11:30 p.m. to 5:30 a.m. Neither the Airport nor the County have the ability to enforce the curfew. Rather, they can collect data on complaints about flight noise during those periods and report that back to a violating airline.
- The current Airport Terminal was constructed to accommodate the passengers that result from the above variables – somewhere between 1.4 and 1.58 million passenger per year.
- Since the original restrictions were put in place and even since the re-adopted provisions, the business plan of the airline industry has changed – no longer are airlines flying smaller aircraft from many smaller airports; rather, now they are flying larger aircraft and fewer flights. Therefore, the restrictions at the Airport result in



VILLAGE OF
PORT CHESTER

222 Grace Church Street, Port Chester, New York 10573

AGENDA MEMO

Village BOT Meeting Date: 3/7/2016

Item Type: Resolution

Description	Yes	No	Description	Yes	No
Fiscal Impact	x		Public Hearing Required		x
Funding Source:			BID #		
			Strategic Plan Priority Area		
			N/A		
Agreement		x	Manager Priorities		
Strategic Plan Related		x	N/A		

Sponsor's Name: Select Sponsor's Name.

Agenda Heading Title
(Will appear as indicated below on Agenda)

ADVANCING ENHANCEMENTS AT EDGEWOOD PARK

Summary

Background:

This is a resolution that will breakdown the implentiation of the enhancements at Edgewood Park, into phases. The first phase will be the construction of the parking area.

The resolution also invtes the Park Commission to a joint meeting with the Board to coordinate further on these enhancements.

Proposed Action

That the Board of Trustees adopt the Resolution

Attachments

Resolution

ADVANCING ENHANCEMENTS TO INCREASE RECREATIONAL PROGRAMMING
AND PROMOTE GREATER OVERALL UTILIZATION OF EDGEWOOD PARK

On motion of TRUSTEE _____, seconded by TRUSTEE _____

, the following resolution was adopted by the Board of Trustees of the Village of Port
Chester, New York:

WHEREAS, at the instance of the Park Commission, the Village acquired property at 201
Grace Church Street adjacent to the park so as to provide on-site parking and thereby facilitate
increased recreational programming and promote greater overall utilization of the park; and

WHEREAS, the Board has since requested that the Park Commission to make a
determination as to the proposed location, layout and configuration of the parking lot, restroom
facilities and other enhancements at the park; and

WHEREAS, recognizing existing fiscal constraints and limited resources with the desire
to deliver tangible results, the Board seeks to phase the implementation of the desired
enhancements deeming as a first priority the construction of the parking area. Now, therefore, be
it

RESOLVED, the Board of Trustees expresses its intent that the parking area at
Edgewood Park be constructed so that it may be available for use by _____ 2016; and be it
further

RESOLVED, invites the Park Commission to a joint meeting to advance the
enhancements at the park on this timetable; and be it further

RESOLVED, that in the interim, the Village Manager work with the Village's consulting
engineer to assist the Park Commission in their deliberations to develop cost estimates, plans
and working bid documents.

Approved as to Form:

Anthony M. Cerreto, Village Attorney

sometimes only one or two slots being able to be filled since the number of passengers on the one or two flights comes close to the half-hourly maximum.

- The County and Airport are currently preparing an Airport Master Plan Update; the previous Airport Master Plan dates from 1987.
- An Airport Master Plan, as with any master plan, examines existing conditions, identifies issues, projects a future, and proposes action items to fulfill that future.
- Key to that process is the production of an aviation forecast that looks 5, 10, and, ultimately, 20 years into the future. The forecast is prepared through standard aviation industry practices, but can be adjusted for local conditions and projected changes. One condition may be the existence of restrictions on growth, i.e., a TUR. The previous forecast prepared for the Airport dates back to 1986.
- The aviation forecast is compared to the FAA's annual TAF for the airport. The FAA TAF is based upon historic growth and national trends; it does not consider local conditions. Any forecast that is outside of a difference threshold undergoes a significant review by FAA headquarters in Washington DC; a forecast within the threshold is reviewed locally – in this case by the New York District. The FAA, in one form or another, must approve the forecast.

Proposal

- Westchester County is proposing Local Law No. _ of 2016 modifying the TUR to permit 11,520 passengers per day, subject to the physical constraints of the Terminal building.
- The number of slots are proposed to remain the same.
- The County is requesting such a change to allow there to be four full flights during the peak hours when the airlines have demand, with fewer options in the off-peak hours.
- The information regarding these proposed modifications is hard to locate. The easiest place to find information is on the Village of Rye Brook's website. It does not appear as if there was direct correspondence between the County and the Village. At a minimum, the Village should be an Interested Agency under SEQRA.
- The County prepared a Short Environmental Assessment Form that comes to the conclusion that the action will not result in any significant adverse environmental impact. The analysis specifically examined air emissions, noise impacts, and vehicular traffic.
- Per the Village of Rye Brook's correspondence from February 25, 2016, the County originally proposed the legislation in October of 2015 and re-submitted it in early February 2016.
- The County Executive submitted to the County Board of Legislators a request that it be placed on the February 8th agenda.
- There was a joint meeting of the Committees on Legislation, Infrastructure and Environment & Health on Monday, February 29th that was open to the public.
- The number 11,520 was reached by extending the 240 passengers per half hour over the course of a 24-hour period. The rationale behind this should be clarified. If an 18-hour day is the preferred time for flights, even if only voluntary, then how can they support a 24-hour period?

- It is also unclear if the number of slots could ever change. The key wording is “subject to the physical constraints of the Terminal building”. If the Terminal were to be enlarged, it may allow additional capacity.
- It is unclear how this potential change impacts the Airport Master Plan Update and, specifically, the forecast. Information should be provided on the status of both and if these modifications are reflected in the analysis.
- Village services may be impacted by these modifications. The Village Fire, Police, and Emergency Services serve the Airport as part of the Mutual-Aid Agreement. There should be a discussion with the County on any additional demand that such modifications might require. Perhaps this is something that the SEQRA review should have evaluated.
- The Village of Rye Brook reached out to the other municipalities in central Westchester County, including the Village on February 25th asking that they review the related documents and potentially take a policy position and/or voice any concerns on the matter. The Village of Rye Brook adopted a resolution on February 23, 2016 opposing the proposed modifications.



National Alliance on Mental Illness of Westchester, Inc.

100 Clearbrook Road, Suite 181, Elmsford, New York 10523

Phone: 914-592-5458

Fax: 914-592-2652

www.namiwestchester.org

Email: info@namiwestchester.org



February 12, 2016

Board of Trustees
Village of Port Chester
Village Clerk
222 Grace Church Street-1st floor
Port Chester, NY 10573

Dear Board of Trustees

Attached is a description of the Ribbon Campaign that Michael Rizzuto spoke to you about. In addition, I have attached our organization's brochure.

We have been doing this campaign for about 3 years and a lot of towns/villages have joined us. The ribbons are placed on "main street" as indicated by the town board. The ribbons will go up on or before May 1 and come down soon after May 31st. We would appreciate it if you could please forward a letter or email to sharonm@namiwestchester.org with your official approval and designated areas where the ribbons can be placed.

Thank you for your support and please do not hesitate to contact us if you need more information.

Sincerely,

A handwritten signature in black ink that reads "Sharon McCarthy".

Sharon McCarthy
Program Director
NAMI Westchester



JOIN US IN HONORING MENTAL HEALTH AWARENESS MONTH



Every May NAMI Westchester displays the above ribbon throughout the county to bring community awareness to mental health. We are a self-help and support organization dedicated to improving the lives of people, and their families, living with mental health problems through support, education and advocacy.

NAMI Westchester is a grassroots organization dedicated to improving the quality of life of all individuals and families whose lives are affected by mental illness. The Ribbon Campaign was initiated in 2013 with over 12 town/villages participating. We hope this anti-stigma campaign will bring the topic of mental health to the forefront and facilitate an open and honest dialogue.

We hope you will join us in honoring this awareness campaign by allowing ribbons to be placed on "Main Street" in your town/village. On or about May volunteers will tie ribbons on trees or poles (as designated by town officials). We also hope you will allow one or two ribbons to be placed outside Town Hall.

FOR MORE INFORMATION: Contact NAMI Westchester

914-592-5458

www.namiwestchester.org



OUR MISSION

NAMI Westchester is one of over 1200 local affiliates of the National Alliance on Mental Illness. We are a grassroots organization dedicated to improving the quality of life for all individuals and families whose lives are affected by mental illness. NAMI advocates for access to services, treatment, support and research and is steadfast in its commitment to raise awareness and build a community of hope for all those in need. We offer the kind of understanding and care that only those who have lived the experience of mental illness can provide.

OUR VISION

Is to create a culture where open disclosure of a mental illness does not have negative consequences in the community, the schools and in the workplace. We continue to seek better treatment for mental illnesses and to eliminate the stigma associated with them. Our programs are a primary source of assistance to people of all cultural and economic backgrounds.

FUNDING

We **do not charge any fee** for any of our programs and services. We rely on our membership, contributions, fundraisers, and grants for financial support.

Following is a description of our programs and services, for more information or to register please contact our offices at 914-592-5458 or visit our website at www.namiwestchester.org

EDUCATION PROGRAMS



NAMI Basics is a program for parents and caregivers of children and adolescents living with mental illness. It covers the fundamentals of caring for yourself, your family and child. The class meets for six 2.5-hour sessions. It is taught by trained teachers who have also taken the course. The program gives critical information and strategies related to caregiving.



Family to Family is a 12 -week, 2.5 hours, course for family and caregivers of individuals living with a mental illness. The class is taught by trained family members. It provides critical information and strategies related to caregiving. The class incorporates presentations, discussions, problem solving and communication skills. In 2013, the program was designated an Evidence Based Practice by SAMHSA.



Peer-to-Peer is a 10-week program taught by 2 trained individuals in recovery. It provides information on brain biology, symptoms, and relapse prevention. In addition, skills will be taught on how to interact with health providers and family as well as techniques for reducing stress. The course provides a confidential place to learn from shared experiences in an environment of sincere, uncritical acceptance.

PLEASE NOTE

No specific medication or medical therapy is endorsed or recommended during any of the above programs.

SUPPORT SERVICES



NAMI Connection is a Peer Recovery Support Group for people living with a mental illness. The groups are facilitated by trained individuals who are living in recovery. Individuals are able to share experiences, coping skills, resources and hope. NAMI Westchester has several groups that meet throughout the county every month.



NAMI Westchester provides a network of support groups for families & friends of individuals suffering from a mental illness. NAMI Westchester support groups include a spousal group, adult sibling group and parents & caregivers of young adults and adolescents. Groups meet once a month and are peer run by trained facilitators.

HELP LINE

NAMI Westchester operates a help line during operating hours that provides resource information and support. The number is 914-592-5458. In addition, we provide resource information on our website at www.namiwestchester.org.



COMMUNITY OUTREACH



IOOV un masks mental illness, using speaker stories to illustrate the individual realities of living with mental illness. You, the audience, gains a better understanding of what it is like to live with mental illness and stay in recovery. This program can change attitudes, preconceived notions and stereotypes regarding mental illness. It is a 60-90 minute presentation led by two trained speakers living in recovery.

PRESENTATIONS

NAMI Westchester will provide speakers to help educate the community on mental health and the journey so many families face when dealing with mental illness. To arrange a presentation call our offices at 914-592-5458.

RIBBON CAMPAIGN



May is Mental Health Awareness month. During the month of May ribbons are on display throughout the county to bring awareness to mental illness and where to find help.

SPEAKER SERIES

This event is held once a month. We invite speakers to discuss relevant mental health topics. For more information on the monthly speaker event, please visit our website at www.namiwestchester.org and go to Events/Speaker Series.



NAMI Westchester sponsors a health walk every Spring to raise funds and awareness to end the stigma of mental illness. For more information, visit our website at www.namiwalks.org/westchester

SCHOOL & COLLEGE OUTREACH

NAMI Westchester is committed to educating Westchester's teachers, parents and students about mental health through a variety of presentations. We also provide support and information to local colleges and university counseling centers.



Ending the Silence is a 50-minute presentation given to middle and high school students. Students learn about mental illness from a family member and an individual living in recovery. Presentation includes a video, handouts and discussion.



Parents and Teachers as Allies is a 90-minute program that helps school principals, teachers and staff better understand mental illness in children and adolescents. PTAA provides information on how to link families to local resources and services. School professionals leave presentations with a better understanding of the early warning signs of mental illness, the lived experience of parents raising a child with a mental illness and the individual experience of living with a mental illness from a personal shared view.



National Alliance on Mental Illness of Westchester, Inc.

A self-help and support organization dedicated to improving the lives of people, and their families, living with mental illness through support, education and advocacy.

100 CLEARBROOK ROAD

ELMSFORD, NY 10523

914-592-5458

info@namiwestchester.org

www.namiwestchester.org

NAMI Westchester is a 501(c)(3) organization.

Emailed Mayor

COR-02
BOT 3-7-2016

Workforce Housing Coalition

c/o 75 South Broadway-Suite 340
White Plains, NY 10601
(914) 683-1010

www.workforcehousingcoalition.org

Monday, February 29, 2016

Mayor Dennis G. Pilla
Village of Port Chester
222 Grace Church Street
Port Chester, NY 10573

VILLAGE OF PORT CHESTER

MAR 02 2016

RECEIVED *M*

Re: United Hospital Site

Dear Mayor Pilla:

We are a coalition representing a diverse group of organizations promoting development that addresses the critical shortage of affordable workforce housing.

We generally support development and feel that restrictive, exclusionary zoning by local boards has created the current shortage, defeating the action of the free market in housing. However, while Port Chester certainly represents an economically and ethnically diverse community and needs market rate units, the plan for the United Hospital Site as currently presented would result in the loss of 133 units of affordable housing without their replacement.

We feel that a responsible site plan would better serve working families in the village and the county by adding a component of 20% affordable housing, which would little more than restore the units lost.

Therefore, we support Sustainable Port Chester Alliance having the opportunity to negotiate a Community Benefits Agreement that would provide 20% affordable housing, along with other benefits to the extent that they enhance the local community and still make economic sense for the developer.

Sincerely,

Workforce Housing Coalition

Members: Community Housing Innovations, Inc., Westchester Habitat for Humanity, White Plains/Greenburgh NAACP, Tri-State Transportation Campaign, Mount Vernon United Tenants and Hunger Action Network, Working Families Party, Westchester Residential Opportunities, Interfaith Clergy for Social Action, The Leviticus Fund, WESPAC, ULI of Westchester/Fairfield, Human Development Services of Westchester, IFCA Housing Network (IFCA), Erase Racism, Duchesne Center/Manhattanville College, Westchester Residential Opportunities, Richard Hyman, Charles Pateman, Timothy Lewis, Esq.



VILLAGE OF RYE BROOK

MAYOR
Paul S. Rosenberg

938 King Street, Rye Brook, N.Y. 10573
(914) 939-1121 Fax (914) 939-0242
www.ryebrook.org

ADMINISTRATOR
Christopher J. Bradbury

TRUSTEES
Susan R. Epstein
David M. Heiser
Jason A. Klein
Jeffrey B. Rednick

February 25, 2016

Dear Fellow Elected Officials and Neighbors:

RE: Proposed County Legislation Modifying the Airport Terminal Use Agreement

The Village of Rye Brook is very concerned about the impacts of proposed legislation that would significantly modify the long-standing Terminal Use Agreement (the "TUA") at the Westchester County Airport (the "Airport") and would result in dramatic changes to this regional Airport hub.

The proposed legislative changes would increase the long-standing limit on the number of commercial flight passengers allowed to enplane and deplane at the Airport from 240 passengers per half hour to 11,520 passengers per day. The result would be many more flights at the Airport, especially at peak times throughout the day, which would have a significant impact on the region not just in terms of air traffic but on other critical environmental areas of concern as well.

For some unexplained reason, this proposed legislation appears to have been placed on a recently expedited path. After initially submitting this legislation to the County Legislature in October 2015, it was re-submitted again earlier this month and will next be discussed at a joint meeting of the Committees on Legislation, Infrastructure and Environment & Health on Monday, Feb. 29th at 1pm on the 8th Floor of the Michaelian Office Building, 148 Martine Avenue, White Plains. This meeting is open to the public.

In reviewing the proposed Legislation, I ask that you please consider the following questions:

*Why would this significant change be expedited just months prior to the release of an updated Airport Master Plan? Shouldn't this type of a change be discussed in conjunction with the Master Plan? Some speculation has been raised that the County is first seeing if this change is approved which could then lead to other significant changes in the Master Plan (longer runways? additional gates?). These types of changes in intensity could potentially change the County's long-standing position of *No Expansion at the Airport*. The problem is we just do not know because the Master Plan is not yet available so we cannot consider this change in that larger context.*

Why are key environmental impacts not being considered? The Airport has already been designated by the County as a Critical Environmental Area (CEA), but so far the administration has only drafted a resolution recommending a Negative Declaration based upon a Short Environmental Assessment

Form than lacks meaningful details and analysis and determines no significant adverse environmental impacts.

Why was the Westchester County Airport Advisory Board not asked to comment on the proposed legislation? The County's Airport Advisory Board was not even initially asked to comment on this proposed legislation. After the Airport Advisory Board members inquired and started to discuss it (with a high turnout from the public), at least one member of that Board was removed and the most recent meeting was abruptly cancelled by the County administration after the meeting was already noticed. This was done without providing the Chairperson of that Board an explanation of the reason of the cancellation.

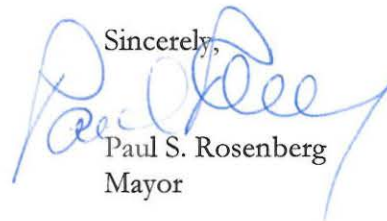
Let's consider some of the most basic facts:

- In 2014, the Airport had 1.4M passengers.
- At the proposed 11,520 passengers per day, the Airport would have the flexibility to have 4.2M passengers annually, which is several million more passengers than has ever utilized the airport at its peak of just under 2M passengers in 2010. If the County does not believe that these passenger limits will occur, why does the proposed legislation ask for this limit to be so high?
- The County's "voluntary curfew" period is 12:00 midnight to 6:30 a.m., yet the 11,520 passenger per day figure surprisingly *includes* 3,120 passengers that could potentially fly during this time period (violating the curfew) in developing the daily passenger maximum. The result of this change is expected to be more even more flights and passengers during the daily peak time periods.
- Approximately 65 days throughout the year are already considered "holiday periods" in the current TUA. These days already have no restrictions on the numbers of passengers, so this proposed legislation is NOT about added convenience for holiday travel.

To be clear, the Village of Rye Brook is not opposed to this regional Airport which serves an important purpose for the residents and business travelers and employees in the County. We agree and feel that it is reasonable to want to fly more modern, quieter, and fuel-efficient aircraft to the Airport. However, any changes to increasing the intensity of the Airport activities must also be balanced with the impacts to the residents in surrounding region. We ask that the County sit down with us to discuss a more reasonable approach.

At our regular meeting held on Tuesday, February 23, the Rye Brook Village Board approved the attached resolution expressing serious concerns with these legislative changes. I urge everyone to review our resolution and the proposed County legislation and make any concerns known to the County Executive and County Board of Legislators.

Thank you for your attention to this important matter. If you have any questions or concerns, please do not hesitate to contact me at mayor@ryebrook.org.

Sincerely,

Paul S. Rosenberg
Mayor

Encs.

On a motion made by Trustee Rednick and seconded by Trustee Heiser, the following resolution was adopted.

RESOLUTION

CONSIDERING A RESOLUTION EXPRESSING CONCERNS WITH WESTCHESTER COUNTY'S PROPOSED LEGISLATIVE CHANGES TO THE AIRPORT'S TERMINAL USE AGREEMENT

WHEREAS, prior Westchester County Executives and Boards of Legislators have historically maintained a firm policy position of *No Expansion at the Westchester County Airport* and went so far as to adopt resolutions to this effect dating back to at least 2003 to protect the County residents that are impacted by Airport activities; and

WHEREAS, despite the long-standing policy position of *No Expansion at the Westchester County Airport*, the County Executive has sent the Board of Legislators proposed legislation modifying the Terminal Use Agreement at the Westchester County Airport (the "Airport") which would drastically increase the long-standing limit on the number of commercial flight passengers allowed to enplane and deplane at the Airport from 240 passengers per half hour to 11,520 passengers per day; and

WHEREAS, the proposed legislation has been re-introduced to the Board of Legislators and it has been referred to the committees on Legislation, Infrastructure, and Environment; and

WHEREAS, this proposed legislation is of significant concern to the residents of the Village of Rye Brook and the County of Westchester who would be impacted by this airport expansion; and

WHEREAS, the net effect of this legislative change is to remove a legislatively-imposed policy of a half-hour passenger restriction and instead provide additional flexibility to the commercial airlines and the private Airport management company that contracts with the County in order to maximize and substantially increase their ability to add daily flights and increase the number of passengers as much as the market desires while utilizing the current four (4) gates within the current runway restrictions; and

WHEREAS, the County's position that 240 passengers per half hour simply equals 11,520 per day (240/half hour x 48) and impacts are minimal due to other restrictions at the Airport and terminal is seriously flawed based upon the following items:

1. The 11,520 passenger limit per day *surprisingly* includes an allocation of 3,120 passengers for the hours between 12:00 midnight and 6:30 A.M. when the County has previously instituted a *voluntary* curfew period and has made the policy decision not to encourage flights during that curfew period. This non-mandatory curfew is routinely already violated by airlines (171 times in November 2015, including 57 times by one airline);
2. Many more private planes and their passengers already use the airport. If this legislation is approved, this change will further dilute one of the few protections against additional flights and airport expansion. Commercial flights represent about 20% of the total flights at the Airport, and the Terminal Use Agreement already factors in approximately 65 "holiday" days when there are no restrictions on passenger limits. The airlines already have unrestricted limits on passengers approximately 18% of the year. The remaining "non-holiday" days simply place reasonable restrictions that spreads passengers throughout the days in order to limit the impacts on the County residents affected by the Airport traffic;
3. The Airport and terminal do not have the capacity to handle the number of passengers and flights they are seeking to add. For instance, in 2014 there were 1.4M passengers that utilized the Airport. 11,520 passengers per day would total 4.2M passengers in a year. At its peak (+/- 2008), the Airport had between 2.0- 2.5M passengers and there were serious impacts to traffic and parking;
4. The County is requesting the flexibility to have 11,520 passengers per day, yet at the same time, they state that they do not anticipate having 11,520 passengers per day because airlines prefer certain "prime" travel periods. The result is that the flexibility and control of the number of passengers and flights is placed solely in the hands of the airlines and the private airport management company that has a contract with the County in order to increase flights by

utilizing all of the gates during any travel times that the market dictates there is interest in flying. If the proposed legislation is approved, the 11,520 passenger limit may likely become a largely irrelevant restriction;

5. Over the years the County has made improvements at the Airport terminal and gates, but has stated the improvements were not Airport expansions, and the construction was for necessary security enhancements and added modern convenience for the passengers. At the time of these projects it was often stated to local municipalities that the best protection against expansion is the passenger limit of 240 per half hour, the runway design, weight restrictions, and the number of gates (4). Now, the County is reversing the argument in their favor, stating that the facility has expanded to 40,000 sf since the time the 240 passenger limit was put in place, so the provision is superfluous and outdated;
6. The only significant additional parking to handle an increase in flights that has been added to the Airport is the off-site facility at the Purchase Park-to-Fly, the long-term viability of which, to our knowledge, is not determined by Westchester County;
7. The County has stated that another significant protection against expansion is the limit of four (4) gates, but if this is a self-imposed limit there is nothing prohibiting additional gates to be considered by the County at a later date;
8. Another significant protection for the municipalities in the region is the limits on the length of the runways and the airplane weight restrictions which limit the size of the planes and the distance they can travel. The County has stated that increasing the runway length is one of the items being *considered* for potential inclusion in an upcoming Airport Master Plan update anticipated by mid-year 2016;
9. Any discussion or consideration of changes to the passenger restrictions, runways, gates or other significant items should be reviewed in the context of the updated Airport Master Plan anticipated later this year in order to avoid any segmentation concerns with regard to SEQRA, and to review each modification in the context of the long-term goals of the local residents, the County and the Airport; and

NOW THEREFORE BE IT RESOLVED, that the Village of Rye Brook firmly believes that Westchester County is not in a position to consider a Negative Declaration under SEQRA for this proposed legislation until such time that it has fully evaluated all of the environmental impacts that additional flights and passengers will bring to the Airport and surrounding region including the following items:

1. Impacts to the availability of on-site and off-site parking from the increase in passengers and flights;
2. Impacts of an increase in on-site and off-site traffic including on I-684, King Street, and Anderson Hill Road, especially during peak commuting hours;
3. Impacts on the water quality of the Blind Brook and Rye Lake/Kensico Reservoir as a result of the additional flights;
4. Impacts of additional de-icing fluid that would be utilized at the Airport and would enter the Blind Brook, which continues to be a significant problem for residents downstream from the Airport property as far south as Westchester Avenue;
5. Additional noise impacts and a reduction in air quality as a result of the additional flights;
6. Since the Airport has been designated by the County as a Critical Environmental Area (CEA), the potential impact of any Type I or Unlisted Action on the environmental characteristics of the CEA is a relevant area of environmental concern and must be evaluated in the determination of significance under SEQR;
7. Impacts to the FAA Z instrument overlay zones that radiate out from the airport as a result of any increase in flights, noise and/or types of aircraft at the Airport as a result of this legislation. These zones also affect development in the surrounding municipalities, in that they limit the type of development that may be located within these zones;
8. Other impacts of the additional flights, especially considering that in 2014 there were 1.4M passengers at the Airport and at the proposed limit of 11,520 passengers per day there would be

4.2M passengers per year not including the potential for additional passengers on the unrestricted +/- 65 holiday day periods. The potential for these passenger totals to far exceed any actual prior peak years ever experienced at the Airport is high, and as such, these impacts need to be fully evaluated;

9. Whether any of the identified considerations and impacts would trigger the need for an Environmental Impact Statement to be prepared to fully address any significant impacts of the proposed legislation; and

BE IT FURTHER RESOLVED, that while the Board of Trustees acknowledges that the Westchester County Airport serves a very important purpose for the residents and corporations in Westchester County and the surrounding region who depend upon the convenience of this regional airport, they are equally concerned about the impacts of the additional flights and number of passengers that would occur as a result of the proposed legislative changes to the Terminal Use Agreement; and

BE IT FURTHER RESOLVED, that the proposed legislation has far reaching impacts that require further environmental study and evaluation that should not be expedited and should instead be reviewed in the context of the update to the Airport Master Plan anticipated later in 2016; and

BE IT FURTHER RESOLVED, that the proposed legislative changes provide far too much flexibility for the airlines and the private airport management company that contracts with the County and have the potential to seriously impact the residents in the surrounding region; and

BE IT FURTHER RESOLVED, that prior to making any changes to the Terminal Use Agreement, the County should seek a written legal opinion from their counsel regarding whether making significant changes to the Terminal Use Agreement impacts any existing grandfathered protections included in this Agreement; and

BE IT FURTHER RESOLVED, that the Village Board is eager to continue to work with the County Executive, the Board of Legislators, the airline representatives and any other impacted municipalities to seek alternative ways of addressing problems currently experienced by the airlines and the airport management staff in a way that would seek to both support the Airport and limit impacts to the residents in the region; and

BE IT FURTHER RESOLVED, that a copy of this resolution be provided to the County Executive, the Board of Legislators, the Westchester County Airport Advisory Board, and other municipalities in the region urging them to consider the impacts of this proposed legislation on the residents and property owners within their jurisdictions.

TRUSTEE EPSTEIN	AYE
TRUSTEE HEISER	AYE
TRUSTEE KLEIN	AYE
TRUSTEE REDNICK	AYE
MAYOR ROSENBERG	AYE

State of New York }
County of Westchester } ss:
Village of Rye Brook }

I hereby certify that this is the Resolution adopted by the Board of Trustees of the Village of Rye Brook which was duly passed by said Board on February 23, 2016

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Seal of the Village of Rye Brook, this 25th day of February, 2016.



Village Clerk



COR-04

BOT 3-7-2016

February 22, 2016

Hon. Dennis Pilla
Mayor and Trustees
Village of Port Chester
222 Grace Church Street
Port Chester, NY 10573

Re: Parking Management – Walgreens lot

Dear Trustees:

Pursuant to our recent discussions on implementation of the parking management plans contemplated by Article 16 of the Land Acquisition and Disposition Agreement, kindly consider this letter to be the formal request by G&S Port Chester Unit 2C LLC for the imposition of a one half hour time limitation for use of the Walgreens parking area. This restriction would be enforced by private contractors pursuant to the provisions of the new Village law on private property parking management.

Walgreens' business has been substantially and adversely affected by abuse of this parking area by commuters and others who do not patronize the Walgreens store. Walgreens has already informally imposed this restriction in order to protect its investment. We look forward to your prompt confirmation as to the imposition of the time limit.

Yours Truly,
G&S Investors